




SYDNEY METRO CITY & SOUTH WEST
CONSTRUCTION TRAFFIC MANAGEMENT PLAN
CROWS NEST SITES
 APRIL 2017



DEVELOPMENT	VERIFICATION	VALIDATION
Name: S Lewis	Name: E Nuberg	Name: B Shum
Signature :  RMS Card # 0022818927 Exp 12/06/18	Signature : 	Signature : 

Document Approval

REV	DATE	DESCRIPTION	PREPARED	REVIEWED	APPROVED
A-00	27 Feb 2017	Draft for Review	S. Lewis		
B-00	4 March 2017	For formal issue to TfNSW	S Lewis	B Shum	
B-01	9 April 2017	Issue to Road safety auditor	S Lewis		
C-00	12 April 2017	Issue for Approval	S Lewis	E Nuberg	B Shum

Glossary

REQUIREMENT	REFERENCE
CTMF	Construction Traffic Management Framework
CTMP	Construction Traffic Management Plan
NSC	North Sydney Council
RMS	Roads and Maritime Service
ROL	Road Occupancy License
SCO	Sydney Coordination Office
TCP	Traffic Control Plan
TfNSW	Transport for New South Wales

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1. Introduction

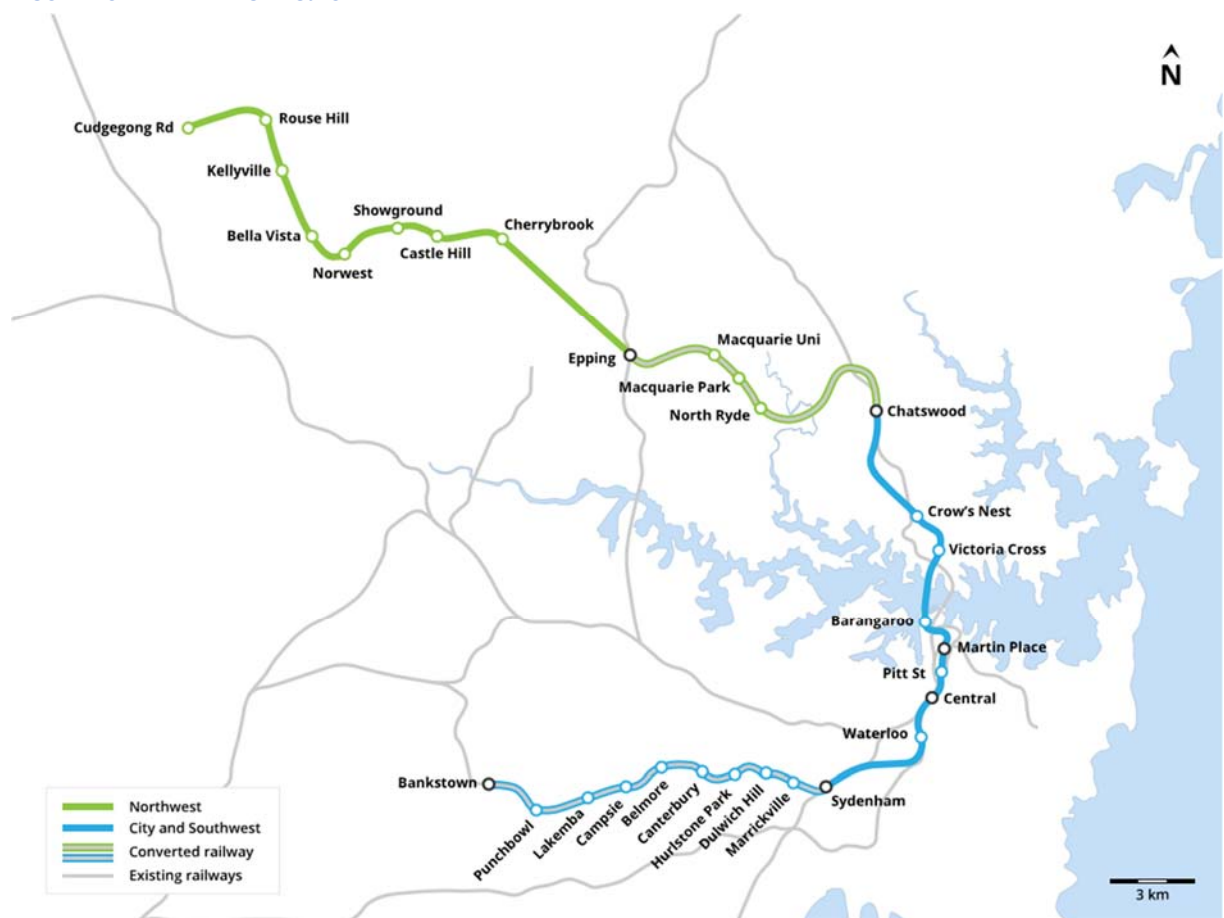
1.1. Project Overview

The Sydney Metro is Australia's biggest public transport project. The metro project currently consists of:

- Stage one is the Sydney Metro Northwest, formerly the North West Rail Link, connecting the north western suburbs of Sydney, through to Chatswood.
- Stage two is the City & South West project linking Chatswood through to Sydenham.
- Stage three includes the upgrade of the existing rail line between Sydenham and Bankstown.

Figure 1 shows the proposed Sydney Metro project and stages.

FIGURE 1 SYDNEY METRO PROJECT



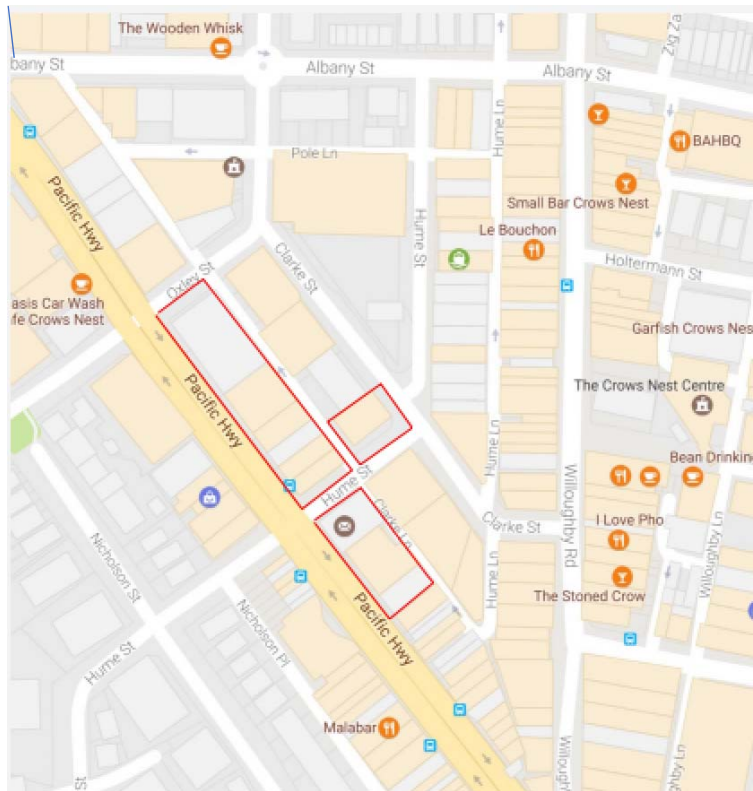
1.2. Purpose of this Plan

The purpose of this *Construction Traffic Management Plan (CTMP)*: CROWS NEST (STATION EXCAVATION) SITES is to detail the site operations for the demolition phase of the Sydney Metro City & South West project. This plan will set out the traffic management initiative that will be deployed to minimise disruption to , and ensure the safety of the wide range of stakeholders potentially affected by the works, including but not limited to: motorists, pedestrians; cyclists; public transport users, local residents and property owners; business owners; and workers/staff engaged on the Project.

2. Locality and existing conditions

The Crows Nest (Station Excavation) sites are three distinct sites. The northern site is bounded by the Pacific Highway to the west, Oxley Street to the north, Clarke Lane to the east and Hume Street to the south. The southern site is bounded by Hume Street to the north, Clarke Lane to the west, 473 Pacific Highway to the south and the Pacific Highway to the west. The last site (Beaurepaires) bounded by Clarke Lane to the west, 20 Clarke Street to the north, Clarke Street to the east and Hume Street to the south. The three sites are shown in Figure 2 below in red. The existing parking restrictions at the site are detailed below and are also included in Appendix 1.

FIGURE 2 CROWS NEST SITES



3. Crows Nest Sites

3.1.1. Pacific Highway

Pacific Highway is classified as a state road which is under the control of RMS. It commences in North Sydney and continues north of the city. The speed limit on the Pacific Highway is 60km/hr.

½ P metered parking exists along most the frontage of the site, No Stopping restrictions exist at each signalised intersection. A bus zone is located immediately north of the Hume Street intersection and a mail zone is located south of the Hume Street intersection. Further south of the site, 1P metered parking exists. A transit lane (T3) is in operation on the southbound carriageway between 6AM-10AM.

A safety camera is in place at the intersection with Hume Street.

The bus stop services the following routes 143, 144, 252, 254, 257, 265, 286, 287, 290, 291 and M20.

The effective width of the footpath is 2.2m (property boundary to face of kerb is 3.7m with 1.9m allocated to street furniture including a bus seating, trees and parking meters). There are no cycling facilities on this section of the Pacific Highway.

3.1.2. Oxley Street

Oxley Street is a local road which connects Lawson Lane to Lithgow Street. Oxley Street is signalised at the intersection of the Pacific Highway. 2P metered parking is available on the northern side of Oxley Street with No Stopping restrictions on the southern side between Clarke Lane and the Pacific Highway. There are 2 lanes available for westbound traffic at the intersection with the Pacific Highway. There are right turn bans from the Pacific Highway on both approaches. The southbound restriction exempts buses. The speed limit on Oxley Street is 50km/hr.

The effective width of the footpath is 1.2m (property boundary to face of kerb is 3.6m with overgrown vegetation impacting on the footpath operation). There are no cycling facilities on the section of the Oxley Street between Clarke Street and Pacific Highway. It is noted that cycling facilities exist on Oxley Street between Clarke Street and Albany Street.

3.1.3. Hume Street

Hume Street is a local road which connects Albany Street to River Road. Hume Street is signalised at the intersection of the Pacific Highway. Metered parking exists on the northern side of Hume Street between the Pacific Highway and Clarke Street. The southern side of Hume Street is signposted as No Stopping between Clarke Lane and the Pacific Highway. A bus zone exists on the southern side of Hume Street between Clarke Lane and Clarke Street, which is used as a layover for Sydney Buses. No service information is provided at this bus stop.

There are 2 lanes available for westbound traffic at the intersection with the Pacific Highway. There are right turn bans from Hume Street on the eastern approach onto the Pacific Highway, and from both approaches on the Pacific Highway. The speed limit on Hume Street is 50km/hr.

The effective width of the footpath is 2.0m (property boundary to face of kerb is 3.8m with 1.8m allocated to street furniture including parking meters and trees). There are no cycling facilities on this section of the Hume Street.

3.1.4. Clarke Lane

Clarke Lane is a local road which connects Clarke Street to Albany Street. it crosses Hume and Oxley Street with stop signs installed at both these cross streets. Clarke Lane is one way northbound between Clarke Street and Oxley Street, north of Oxley Street, Clarke Lane is two way. No Parking restrictions exist on both sides of Clarke Lane between Clarke Street and Oxley Street.

Clarke Lane provides rear lane access to a number of businesses located on Clarke Street, Willoughby Road and the Pacific Highway.

Clarke Lane has varying footpath widths between 0.6m to 2.2.m. There are no cycling facilities on this section of the Clarke Lane.

3.1.5. Clarke Street

Clarke Street is a local road which connects Willoughby Road to Oxley Street. it crosses Hume Street with give way signs installed on Hume Street. No Stopping restrictions are in place across the frontage of the

Beaurepaires site. Metered parking is in place on the western side of Clarke Street. 10 minute parking and bus zones are located on the eastern side of Clarke Street.

The effective width of the footpath is 2.0m (property boundary to face of kerb is 3.8m with 1.8m allocated to street furniture including parking meters and trees). An on road cycle lane is marked in the southbound direction. The northbound direction has cycle logos but a dedicated lane is not marked.

3.1. Preliminary Works

Timing: From 12th June 2017 to Site Operations Commencement

3.1.1. Works and Operations

Prior to the commencement of demolition works within the site, Delta Group will need to disconnect/ isolate existing utilities and undertake HAZMAT identification and removal. Fencing installation will occur where required. These works will be undertaken within the site using existing driveways. Only light vehicles will be required for these works.

3.2. Site operations

Duration: 5 months

Timing: July 2017 to November 2017

3.2.1. Works Required

Prior to the commencement of demolition works within the site, Delta Group will need to construct a driveway on Clarke Lane to allow access into the vacant site adjacent to the Australia post building. Access into the vacant block is proposed from Hume Lane Other vehicles will access Clarke Lane from Pacific Highway. Egress will be onto Clarke Lane.

Access into the site between Hume Street and Oxley Street will be via Clarke Lane. Egress from this site is proposed via the existing driveway on Oxley Street. The Beaurepaires site will be used as office accommodation. Demolition of this building will occur post completion of other works at this location. The existing driveway access will be used.

Class B hoarding will be installed on site – refer to Appendix 2 for details on the proposed arrangements.

Demolition of the existing buildings within the site, will occur during normal construction hours of 0700-1800 Monday to Friday and 0800-1300 on Saturday.

3.2.2. Operating Conditions

All traffic will enter and exit the sites in a forward direction.

3.2.2.1. Scaffolding and hoarding placement

Scaffolding will generally be within the site at the project boundary, however, Class B hoarding will be installed around the frontages of Pacific Highway, Hume Street and Clarke Street. A typical scaffolding arrangement is included within Appendix 2. The hoarding will have lighting to provide a safe walkway for pedestrians and will have available a minimum of 2m clearance as a pedestrian thoroughfare.

During the installation of the scaffolding/ hoarding in Clarke Lane, DELTA will close the footpath and detour pedestrians to allow the installation of an engineered demolition screen post crane access. This screen will then

allow the demolition of the walls adjacent to site. Once these walls have been demolished, Class A hoarding will be installed

3.2.3. Impact on Traffic Flows

The operation of the site will have minimal impact on existing traffic conditions as the traffic generation is typically low and vehicles will be restricted to left in/ left out. Post the arrival of machinery on site, traffic generation at this site will typically be in the order of 50 trucks per month, with an average of 2-3 trucks per day. There will be minimal truck numbers during the peak periods (AM and PM).

3.2.4. Impact on Public Transport

There is no impact on public transport as the existing bus stop on the Pacific Highway and Hume Street between Clarke Lane and Clarke Street will be retained. There may be some short-term relocations/ closures during lane closures for the awning removals etc on the Pacific Highway. This will be discussed with bus operators prior to implementation.

3.2.5. Impact on Pedestrians and Cyclists

There will be minimal impact on pedestrian and cyclist movements during the bulk of the works. During the contra flow operations, pedestrians and cyclists will be managed on Hume Street.

3.2.6. Impacts on commercial and residential access

There is no impact on existing commercial properties as all properties on site will be vacant prior to the commencement of works. Clarke Street, Willoughby Road and Pacific Highway properties access – both commercial and residential, will be retained.

3.2.7. Staff parking and transportation to site

During the demolition works, it is anticipated that there will be 10-12 personnel on site. There will be existing car parking spaces on site in the Beaurepaires site. The workforce for the actual demolition will be encouraged to arrive by site using public transport, with most likely to use the St Leonards Interchange, a 9 minute walk (750m) from site. Ample bus services are also available, close to the site.

3.2.8. Cumulative impacts

North Sydney Council have advised they are currently pursuing the closure of Hume Street between Clarke Street and Pole Lane, for an extension of the existing parkland.

3.2.9. Special Events

Crows Nest Fair is held typically in October on a Sunday between 10AM and 5PM. The fair will not be impacted by any of the demolition works.

In reviewing North Sydney Council's website for special events near the site, there are no other special events listed, which would be impacted by the works.

3.2.10. Traffic Control Plan (TCP) / Road Occupancy License (ROL) Identified Works

Works that have been identified as requiring a Traffic Control Plan are detailed below. Appendix 3 provides details on expected durations, timing for implementation, the works being undertaken and the expected traffic impacts.

1. Construction of driveway on Clarke Lane
2. Installation of hoarding will require pedestrian management

3.2.11. North Sydney Council required approvals

Works that have been identified as requiring Council approval include:

1. Vehicular crossing on Clarke Lane
2. Hoarding and scaffolding installation
3. Permit to stand plant/ crane
4. Over size/ over mass vehicles

4. Fleet Management

Trucks to be used on the project will be compliant with NSW legislation and standards including the Heavy Vehicle National Legislation. A combination of truck types will be used during the demolition works. These trucks may be single unit trucks, bin trucks and semi-trailers (out of hours only). Table 1 provides an estimation of materials to be removed over the life of the works, whilst **Error! Reference source not found.** provides a comparison of the EIS and DELTA traffic movements at the site.

TABLE 1 MATERIAL (TONNES) ESTIMATE FOR CROWS NEST SITES

BRICK	CONCRETE	STEEL	DEMOLITION WASTE
1450	3700	90	520

TABLE 2: AVERAGE VEHICLE MOVEMENTS PER HOUR

TIME	EIS LIGHT VEHICLES	DELTA LIGHT VEHICLES	EIS HEAVY VEHICLES	DELTA HEAVY VEHICLES
0700-1000	2	2	5	2
1000-1200	9	1	10	1
1200-1400	9	1	10	1
1400-1600	9	1	10	1
1600-1800	2	1	5	1
1800-0700	0-9	0	0-5	0

4.1. Logistics Routes

Generally, the haulage routes will be via arterial roads/ freeway or Tollway. Where possible the routes have taken into account the requirements of the Environmental Impact Statement (EIS). Where the routes differ from that in the EIS, the routes chosen are based on the availability of the arterial road system and these are detailed in this CTMP.

Materials will be removed from site using a variety of trucks including 5t tippers, bin trucks and single unit trucks (5.5m to 12.5m in length) and taken to authorised disposal sites around Sydney. Refer to Appendix 4 for details on the proposed routes.

4.2. Permits/ Over Dimensional Vehicles

Permit issue for vehicles greater than 4.5 tonnes is through the National Heavy Vehicle Regulator (NHVR). This applies to particular special purpose vehicles (SPV) such as mobile cranes and other oversize/overmass vehicles (OSOM). At present, RMS is currently undertaking this permit issue.

For over dimensional vehicles, generally vehicles that are greater than 25m length or 3.5m width require a pilot(s). Extremely long or wide vehicles will require an escort (fee payable). Permits will be applied for by the transport operator.

There will be a requirement for over mass/ over dimension vehicles during these works. We anticipate that there will be 3 float trucks required for delivery and removal of machinery. These movements will be undertaken in accordance with RMS and North Sydney Council permit conditions.

4.3. Environmental Considerations

All vehicles used on the Project will comply with national and state regulations for exhaust emissions and noise and:

- Switch off their engines when idle, particularly in residential areas
- Avoid the use of their engine brakes in residential areas at night
- Have their load covered prior to leaving site to reduce dust
- Be fitted with broadband reverse beepers.

Refer to our Construction Environmental Management Plan (CEMP) for further details

4.4. Drivers and Operators

Operator selection will be based on safety performance criteria. Operators and drivers will be required to have general construction industry induction cards and will be required to attend ongoing general project and site specific inductions.

All operators will be comprehensively trained with regard to community expectations and impacts from haulage operations. The induction will have a particular focus on operator behaviour. Operator competency and standards of behaviour will be continually assessed and discipline procedures will be put in place to maintain compliance.

Queuing of trucks in local streets around the demolition sites will not occur as there will be sufficient designated area within site for trucks to wait. It is noted that there are minimal truck movements required during the demolition phase. Trucks will be provided post the site start times.

The primary access point into the Crows Nest sites will be via Clarke Lane.

5. Other Considerations

5.1. Road Safety Audits

Road safety audit has been undertaken during the development of the CTMP. Refer to Appendix 5.

5.2. Police and Emergency Services

The Police and relevant Emergency Services will be informed in a timely manner of relevant activities proposed within this CTMP. The initial communication to these stakeholders will be via the Traffic and Transport Liaison Group (TTLG). Regular updates will be provided to emergency services, including changes to road network configurations, changes to road conditions and worksite access locations, through emails and face to face discussions.

5.3. Communications and the community

TfNSW will be responsible for the dissemination of information to the community including affected residents, relevant Councils, businesses and the public. Refer to Sydney Metro Community Consultation Strategy – Early Works.

Any enquiries, complaints and/ or compliments should be directed to the TfNSW’s Sydney Metro Project Information line - **1800 171 386** or via mail to: sydneymetro@transport.nsw.gov.au or Sydney Metro City & Southwest, PO Box K659, Haymarket, NSW 1240 or [Sydney Metro City & Southwest Website](#).

5.3.1. Travelling public

Public transport interruptions will be communicated via on site signage. The motoring public will be forewarned of any changes, including road closures, road changes and lane closures well in advance using appropriate signage, including variable message signs (VMS).

5.3.2. Stakeholders consulted

TABLE 3 identifies the stakeholders consulted during the development of this CTMP:

TABLE 3 STAKEHOLDER CONSULTATION

STAKEHOLDER	DATE	CONSULTATION	MAIN CONTACT PEOPLE
North Sydney Council	1 st Feb 2017	Meeting	Michaela Kemp
North Sydney Council	16 th Feb 2017	Meeting	Michaela Kemp
North Sydney Council	14 th Mar 2017	Issue of CTMP	Michaela Kemp
Roads and Maritime Service	22 nd March 2017	Issue of CTMP	Peta Smith
Sydney Coordination Office	14 th March 2017	Issue of CTMP	Giovanny Ramirez
TfNSW	14 th March 2017	Issue of CTMP	Alan McNamara
TCG	21 st Feb 2017- 11 th April 2017	All members	SCO/ TfNSW/ WC/ RMS
North Sydney Council	12 April 2017	Reissue of CTMP	Michaela Kemp
Sydney Coordination Office	12 April 2017	Reissue of CTMP	Giovanny Ramirez
Roads and Maritime Service	12 April 2017	Reissue of CTMP	Peta Smith

5.4. Inspections and monitoring

The site will be monitored by the site supervisor. Any changes as noted in this CTMP, to signs/ lines that impact on the public will be monitored daily during site operating hours.

Traffic control used for pedestrian management, lane closures etc will need to provide records of the traffic control implemented. Any changes required to the traffic control set up will be authorised by a RMS “Prepare a Work Zone Traffic Management Plan” or equivalent.

5.5. Environmental Maintenance

All works associated with the temporary traffic arrangements proposed within this plan will conform to the Construction EMP.

5.6. On Site Contacts

TABLE 4 ON SITE CONTACTS

NAME	POSITION	CONTACT #
Ben Shum	Project Director	0423 796 946
Gary Waterhouse	Project Manager	0455 130 219
Jesse Holmes	Site Supervisor	0467 206 829
Kevan Zulu	Safety Manager	0411 092 898
Matthew Stephenson	Environment and Sustainability Manager	0418 284 226

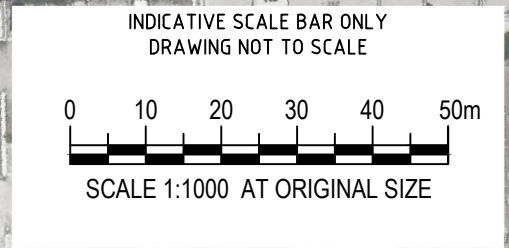
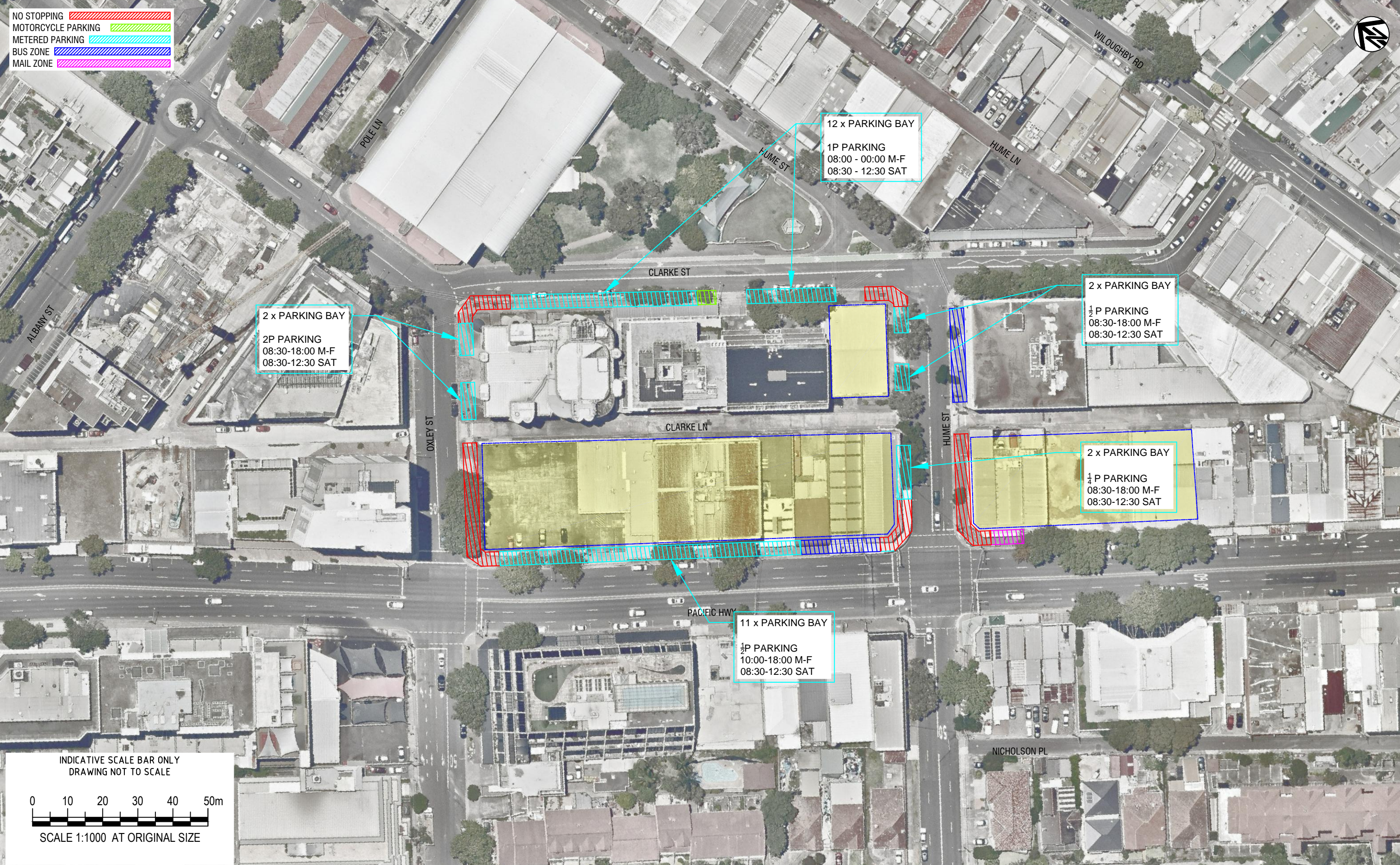
5.7. References

The following documents were used in the development of this CTMP:

- Construction Traffic Management Framework – TfNSW, v.4.4 provided by TfNSW 4th March 2017
- Principal’s General Specification G10 Traffic and Transport Management, v2.0
- Roads and Maritime Service Traffic Control at Worksites Manual v 4.0
- Relevant AustRoads Guides and RMS Supplements
- Sydney Metro Principal Contractor Health and Safety Standard

Appendix 1 Existing parking restrictions

DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0003-01	Existing parking restrictions



DRAWN BY:		LH	
DRW CHECK:		SL	
APPROVED:		SL	
IND REVIEW:		N/A	
A	SS	26/02/2017	INITIAL DRAFT
REV	BY	DATE	DESCRIPTION
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:

PROJECT CLIENT

SYDNEY METRO

**DELTA GROUP AUSTRALIA WIDE
EXISTING PARKING RESTRICTIONS
CROWS NEST**

DRAWING No:		DD-SYDM-DWG-0003-01	
SHEET	1	OF	1
REVISION	A		

Appendix 2 Proposed site operations including hoarding arrangements

DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0003-02	Proposed Access/ egress arrangements
DD-SYDM-DWG-0003-03	Site Establishment layout including hoarding arrangements
DD-SYDM-DWG-0003-04	Single unit truck swept path into Clarke Lane from Clarke Street and onto Hume Street
DD-SYDM-DWG-0003-05	Single unit truck swept path into Clarke Lane from Hume Street
DD-SYDM-DWG-0003-06	Single unit truck swept path Clarke Lane onto Oxley Street
DD-SYDM-TYP-0000-01	Typical Hoarding arrangement over footpath



LEGEND	
ACCESS	
EGRESS	
STAGE 1	
STAGE 2	

REV	BY	DATE	DESCRIPTION	APPD.
C-00	LS	05/04/2017	UPDATED ACCESS / EGRESS	
A-01	LS	15/03/2017	UPDATED ACCESS AS PER DELTA COMMENTS 15/03/17	
A-00	LS	05/03/2017	INITIAL DRAFT	SL

DRAWN BY:	LS
DRW CHECK:	SL
APPROVED:	SL
IND REVIEW:	N/A

PROJECT

CLIENT

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
SITE ACCESS & EGRESS
CROWS NEST

DRAWING No:	DD-SYDM-DWG-0003-02	
SHEET	1	OF 1
REVISION	C-00	

Crows Nest – Site Establishment

Establish Site office and amenities at 14 Clarke St.

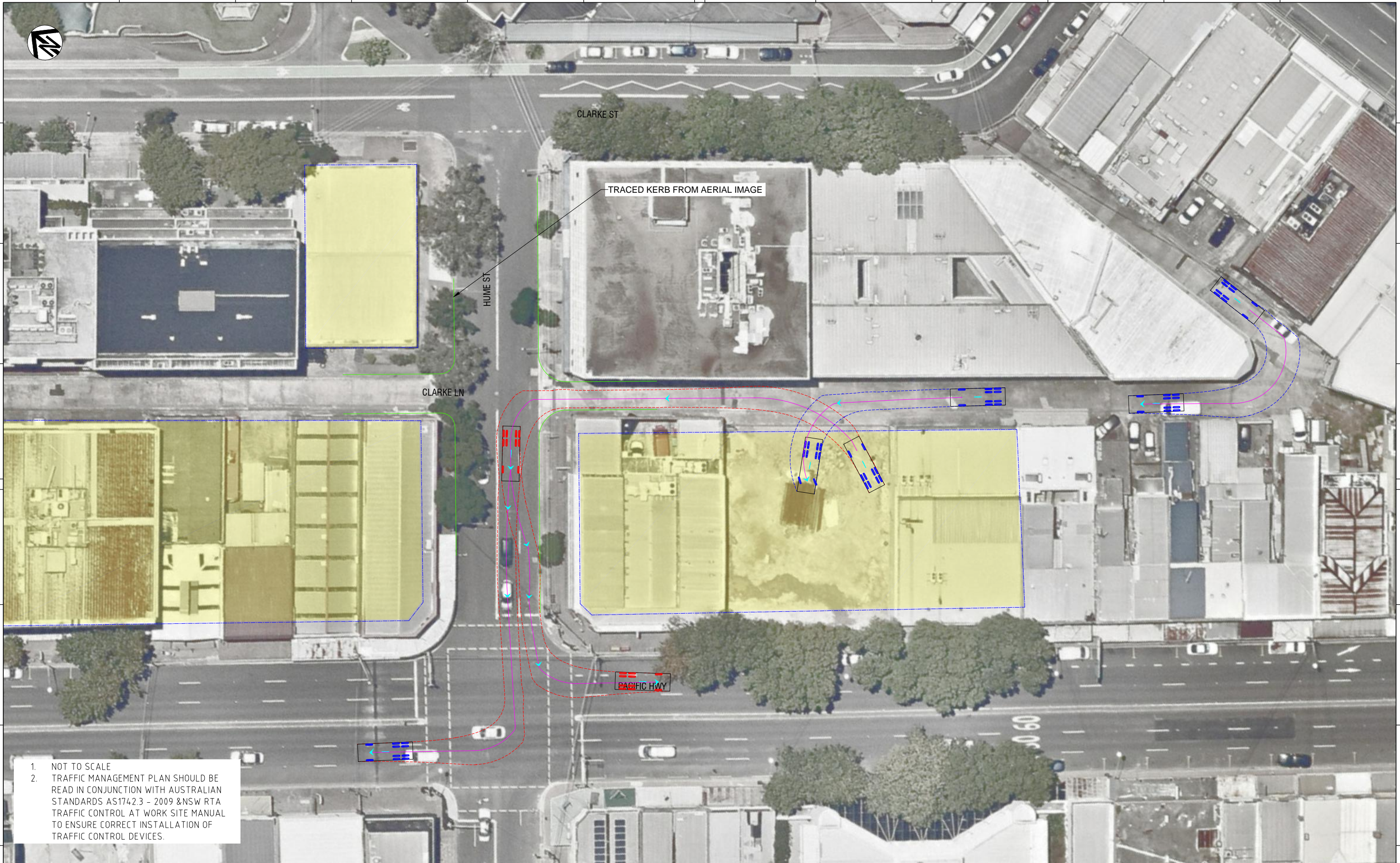
Establish A-Class and B-Class Hoardings.

Install scaffold off ground, A or B Class Hoarding as shown.

Setup silt fences/sand bags/ geofab around site perimeter as required.



DD-SYDM-DWG-0003-03



1. NOT TO SCALE
2. TRAFFIC MANAGEMENT PLAN SHOULD BE READ IN CONJUNCTION WITH AUSTRALIAN STANDARDS AS1742.3 - 2009 & NSW RTA TRAFFIC CONTROL AT WORK SITE MANUAL TO ENSURE CORRECT INSTALLATION OF TRAFFIC CONTROL DEVICES.

DRAWN BY:		LS		
DRW CHECK:		SS		
APPROVED:		SL		
IND REVIEW:		N/A		
REV	BY	DATE	DESCRIPTION	APPD.
C-00	LS	05/04/2017	AMENDED DESIGN VEHICLE	
B-00	LS	29/03/2017	AMENDED SWEEP PATHS	
A-00	LS	26/02/2017	INITIAL DRAFT	SL
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:	

PROJECT

CLIENT

SYDNEY METRO

**DELTA GROUP AUSTRALIA WIDE
PROPOSED SITE ACCESS/EGRESS
CLARK LN SOUTH - CROWS NEST**

DRAWING No:		DD-SYDM-DWG-0003-04	
SHEET	1	OF	1
REVISION	C-00		



C-00	LS	05/04/2017	UPDATED DESIGN VEHICLE		
A-00	LS	26/02/2017	INITIAL DRAFT	SL	
REV	BY	DATE	DESCRIPTION	APPD.	
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:		

DRAWN BY:	SS
DRW CHECK:	SL
APPROVED:	SL
IND REVIEW:	N/A

PROJECT



CLIENT



SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
 PROPOSED SITE ACCESS
 CLARK LN NORTH - CROWS NEST

DRAWING No:	DD-SYDM-DWG-0003-05		
SHEET	1	OF	1
REVISION	C-00		



1. NOT TO SCALE
2. TRAFFIC MANAGEMENT PLAN SHOULD BE READ IN CONJUNCTION WITH AUSTRALIAN STANDARDS AS1742.3 - 2009 & NSW RTA TRAFFIC CONTROL AT WORK SITE MANUAL TO ENSURE CORRECT INSTALLATION OF TRAFFIC CONTROL DEVICES.

C-00	LS	05/04/2017	AMENDED DESIGN VEHICLE		
A-00	LS	26/02/2017	INITIAL DRAFT	SL	
REV	BY	DATE	DESCRIPTION	APPD.	
COORDINATE SYSTEM:		HEIGHT DATUM:		SCALE:	

DRAWN BY:	LS
DRW CHECK:	SL
APPROVED:	SL
IND REVIEW:	N/A

PROJECT



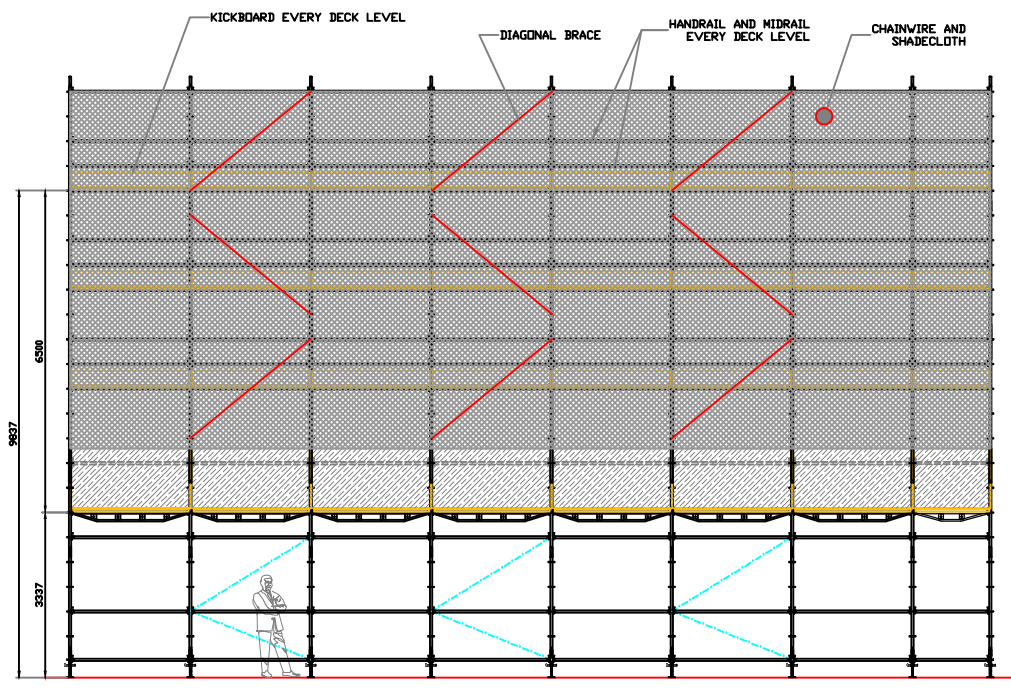
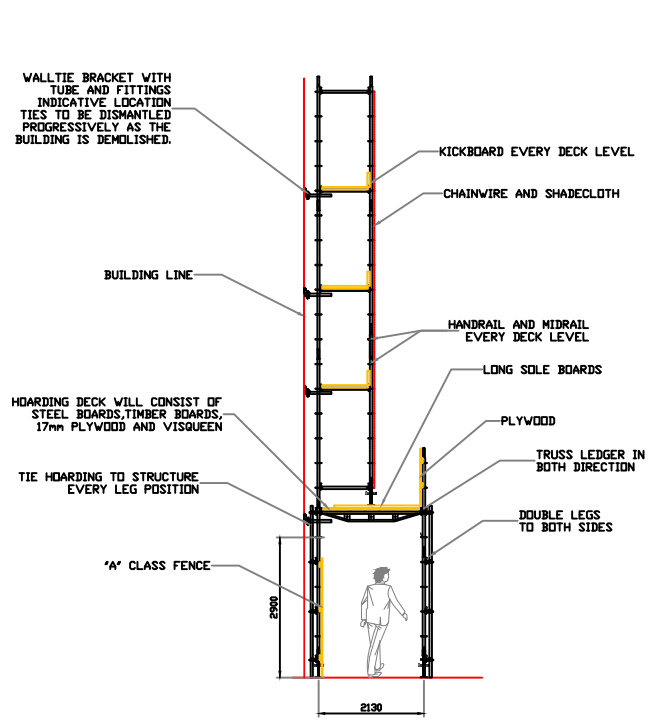
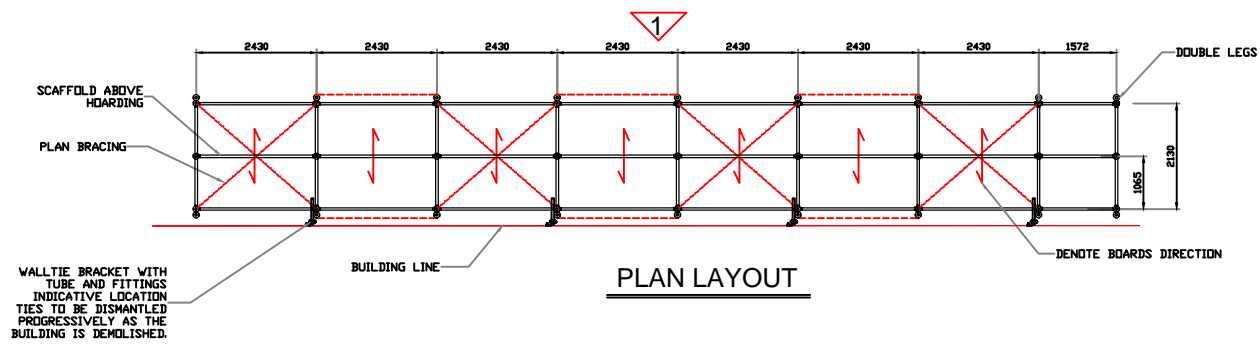
CLIENT



SYDNEY METRO

**DELTA GROUP AUSTRALIA WIDE
PROPOSED SITE EGRESS
OXLEY ST - CROWS NEST**

DRAWING No:	DD-SYDM-DWG-0003-06		
SHEET	1	OF	1
REVISION	C-00		



GENERAL NOTES

THIS SCAFFOLD DESIGN RELATES SPECIFICALLY TO AT-RING RING LOCK SYSTEM COMPONENTRY SUPPLIED BY DB GLOBAL GROUP.

SCAFFOLD BOARD LOCATIONS ARE SHOWN FOR ILLUSTRATION PURPOSES ONLY

ALL DIMENSIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO ACTUAL SITE CONDITIONS DO NOT OBTAIN DIMENSIONS BY SCALING FROM THE DRAWINGS

IT IS THE HIRER'S RESPONSIBILITY TO ENSURE THAT THE SCAFFOLD IS NOT ALTERED (INCLUDING THE REMOVAL OF TIES) WITHOUT PERMISSION FROM DB GLOBAL GROUP.

A) SCAFFOLD TIES
ALL TIES ON THE SCAFFOLDING TO SATISFY THE REQUIREMENTS OF BS11874 (ALL PARTS). ALL BRICKWORK, MASONRY, CONCRETE SLAB EDGES OR ANY OTHER SURFACE / STRUCTURE USED AS SUPPORT FOR TIES, MUST SUSTAIN A MINIMUM OF 6.0kn PULL OUT OR PUSH IN LOAD.

ALL BRICKWORK, MASONRY, CONCRETE SLAB EDGES OR ANY OTHER SURFACE / STRUCTURE USED AS SUPPORT FOR TIES, MUST SUSTAIN A MINIMUM OF 6.0kn PULL OUT OR PUSH IN LOAD.

MAXIMUM VERTICAL SPACING OF TIES TO BE 4.0m.
MAXIMUM OF 2.0m OF CHAMFER & SHADECLOTH IS ALLOWED ABOVE THE LAST ROW OF TIES.

WHERE CHEMICAL OR MECHANICAL MASONRY ANCHORS ARE USED IT IS RECOMMENDED

B) ALL ANCHORS USED TO SECURE CHAINWIRE BRACE TO BE PROOF LOADED IN TENSION TO THE MANUFACTURERS RECOMMENDED WORKING LOAD.

C) 10% OF ALL ANCHORS (20 RANDOM) USED TO SECURE TIES AND ACTING IN TENSION ONLY TO BE PROOF LOADED AS IN PARAGRAPH (C) ABOVE.

D) SCAFFOLD BRACING
D) BRACING TO THE OUTSIDE FACE OF THE SCAFFOLD SHOULD BE INSTALLED TO AT LEAST ONE BAY OF EVERY THREE.

E) BRACING SHOULD BE INSTALLED TO ALL END BAYS.

F) SCAFFOLD LOADING
F) THE SCAFFOLDING IS RATED FOR A HEAVY DUTY LOAD OF 675 KG PER BAY WITH A MAXIMUM BAY LENGTH OF 3.00m. A MAXIMUM OF 2 WORKING DECKS (DATED HEAVY DUTY) AT ANY ONE TIME IS ALLOWED UNLESS SPECIFIC DESIGN IS PROVIDED AND EXEED ON THE DRAWING REFER TO BS11874.1 FOR DUTY LOADINGS

G) MAXIMUM VERTICAL LIFT HEIGHT OF 2.0m

H) MAXIMUM NUMBER OF DECKS AND SCAFFOLD HEIGHT IS SPECIFIED ON THE DRAWING.

I) SOLEBOARDS / FOUNDATION
I) SOLEBOARDS TO BE A MINIMUM OF 225mm x 350mm x 38mm ASSUMING SOIL BEARING CAPACITY OF 100kPa. WHERE PRACTICAL, THE USE OF CONTINUOUS 225mm x 180mm x 38mm SOLEBOARDS TO BE USED.

J) THE HIRER SHALL ENSURE THAT ALL FOUNDATIONS FOR THE SCAFFOLD OR ITS RELEVANT SUPPORT SYSTEM ARE ADEQUATE TO SUPPORT THE IMPOSED LOADS DUE TO THE SCAFFOLD'S DEAD LOAD AS ERECTED AND THE ASSOCIATED LIVE LOADS.

FOR CONSTRUCTION

REV No.	REVISION NOTES	SIGN	DATE
A	C/W DELETED, REDUCE THE OF HOARDING	DJ	14/07/2016



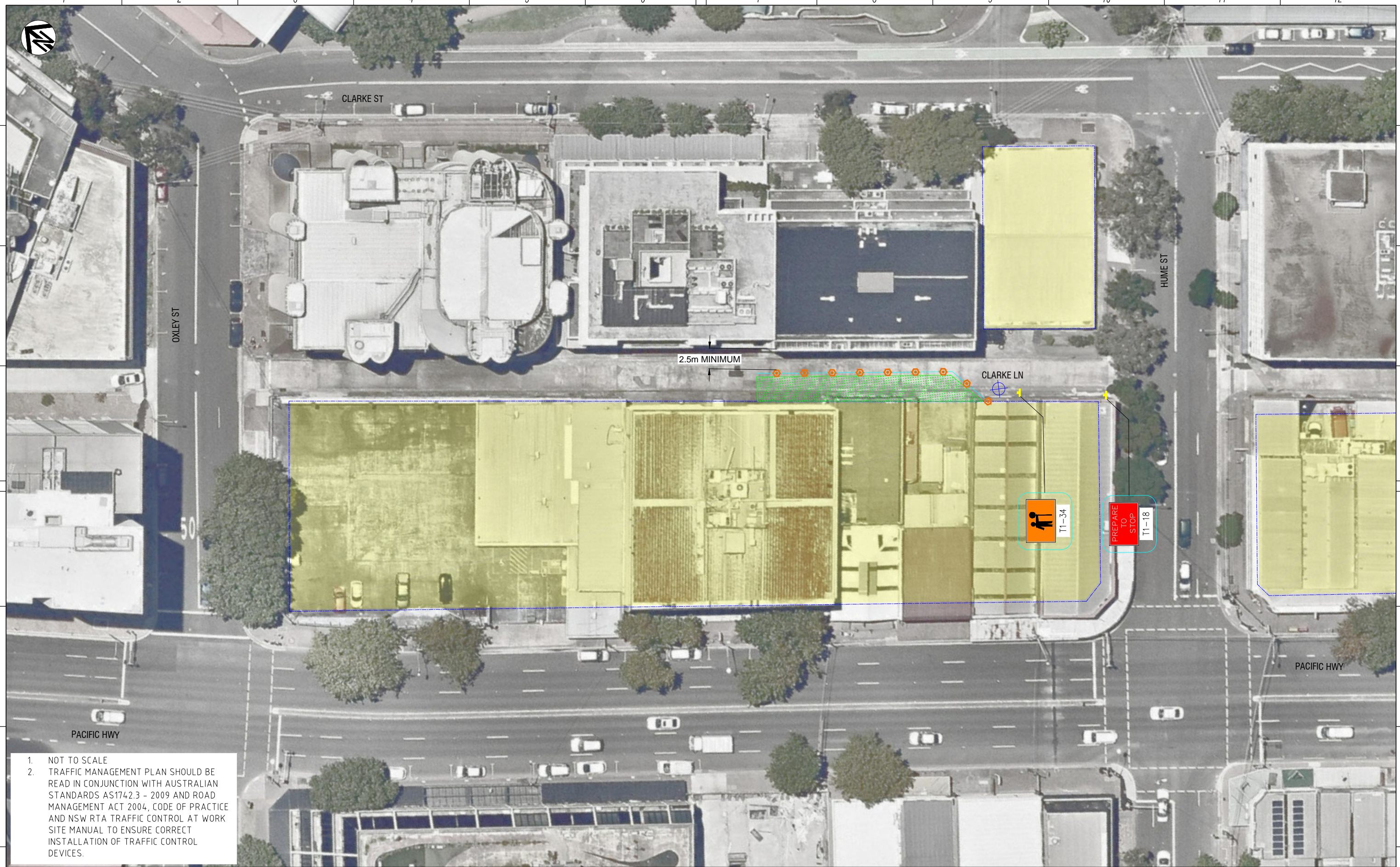
Customer: DELTA DEMOLITION GROUP

Project: TYPICAL HOARDING DETAIL

Paper Size: A1	Scale: 1:50	Drawn by: DJ-C
Date: 17/05/2016	Checked by:	
DD-SYDM-TYP-0000-01	Rev No. A	
	Sheet no. 1 of 1	

Appendix 3 Traffic control plans

TCP #	LOCATION	FROM	TO	TIMING	TRAFFIC CONTROL	WORKS	IMPACTS
0003-07	Clarke Lane	Hume Street	Oxley Street	ALL	Stop/ Slow	<ul style="list-style-type: none"> Awning removal Scaffold installation 	Minimal impacts to traffic as Clarke Lane only services local access at this location No impact to parking as this area is No Parking/ No Stopping
0003-08	Oxley Street	Clarke Lane	Pacific Highway	Nights	Lane closure	<ul style="list-style-type: none"> Awning removal Scaffold installation Plant removal 	Outside of peak periods with lane closure adjacent to traffic signals at night and/ or weekend
0003-10	Pacific Highway	Oxley Street	South of site	Nights	Lane closure	<ul style="list-style-type: none"> Awning removal Scaffold installation Aerial bundling works 	Lane closures at night and/ or outside of peak periods
0003-11	Hume Street	Pacific Highway	Clarke Street	Nights	Lane closure	<ul style="list-style-type: none"> Awning removal Scaffold installation Aerial bundling works 	Lane closures at night and/ or outside of peak periods
0000-02	Site access	Site egress	All	All	Pedestrians	<ul style="list-style-type: none"> During site access/ egress 	Only used during site access/ egress times



1. NOT TO SCALE
2. TRAFFIC MANAGEMENT PLAN SHOULD BE READ IN CONJUNCTION WITH AUSTRALIAN STANDARDS AS1742.3 - 2009 AND ROAD MANAGEMENT ACT 2004, CODE OF PRACTICE AND NSW RTA TRAFFIC CONTROL AT WORK SITE MANUAL TO ENSURE CORRECT INSTALLATION OF TRAFFIC CONTROL DEVICES.

DRAWN BY:		LS		
DRW CHECK:		SL		
APPROVED:		SL		
IND REVIEW:		N/A		
A-00	LS	05/03/2017	INITIAL DRAFT	SL
REV	BY	DATE	DESCRIPTION	APPD.
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:	

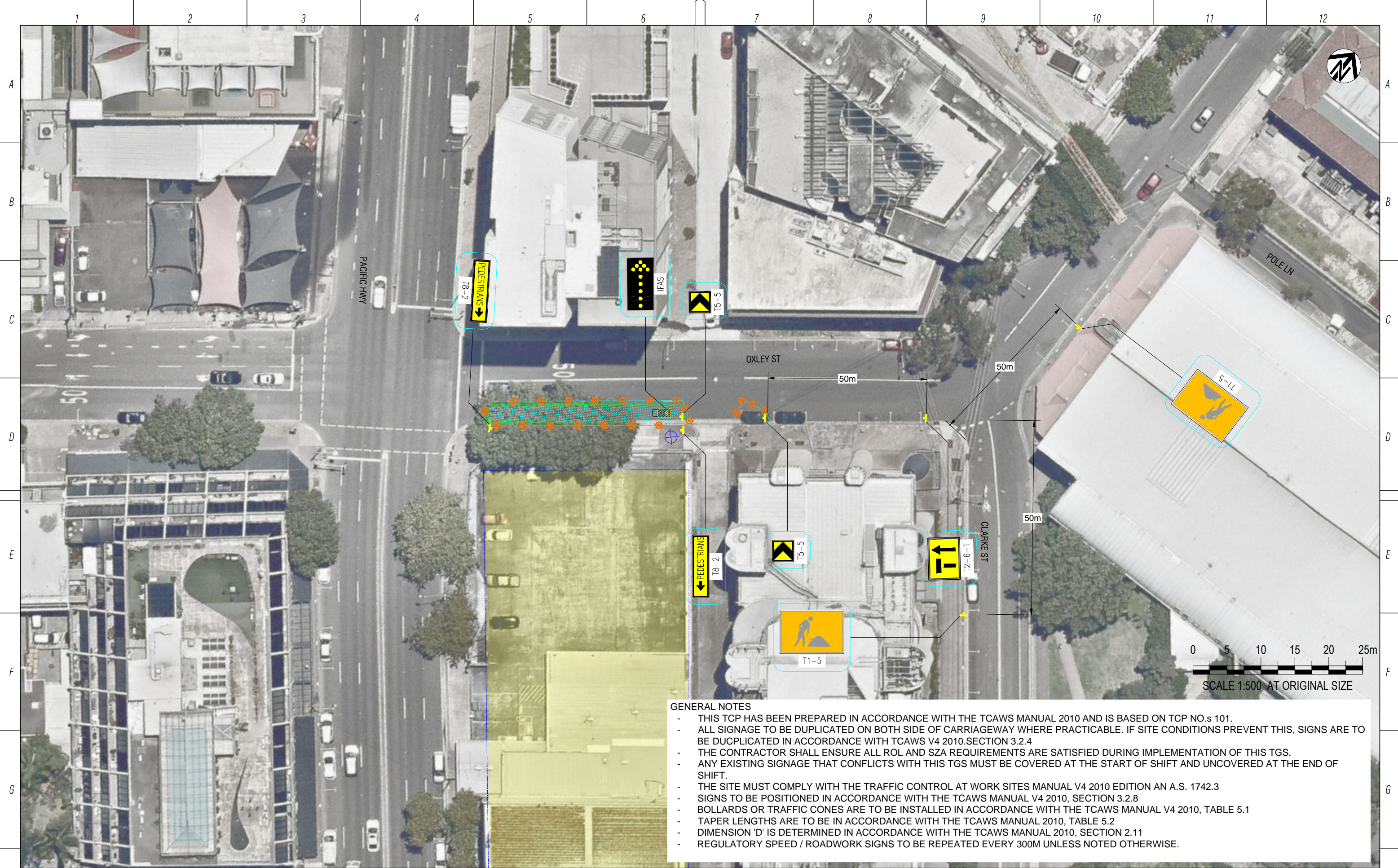
PROJECT

CLIENT

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE STOP/SLOW ON CLARKE LN CROWS NEST

DRAWING No:		DD-SYDM-DWG-0003-07	
SHEET	1	OF	1
REVISION	A-00		



- GENERAL NOTES**
- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TCP NO.s 101.
 - ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUCPLICATED IN ACCORDANCE WITH TCAWS V4 2010,SECTION 3.2.4
 - THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TGS.
 - ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TGS MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
 - THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V4 2010 EDITION AN A.S. 1742.3
 - SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, SECTION 3.2.8
 - BOLLARDS OR TRAFFIC CONES ARE TO BE INSTALLED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, TABLE 5.1
 - TAPER LENGTHS ARE TO BE IN ACCORDANCE WITH THE TCAWS MANUAL 2010, TABLE 5.2
 - DIMENSION 'D' IS DETERMINED IN ACCORDANCE WITH THE TCAWS MANUAL 2010, SECTION 2.11
 - REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

A-00	LS	05/03/2017	INITIAL DRAFT	SL
REV	BY	DATE	DESCRIPTION	APPD.
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:	

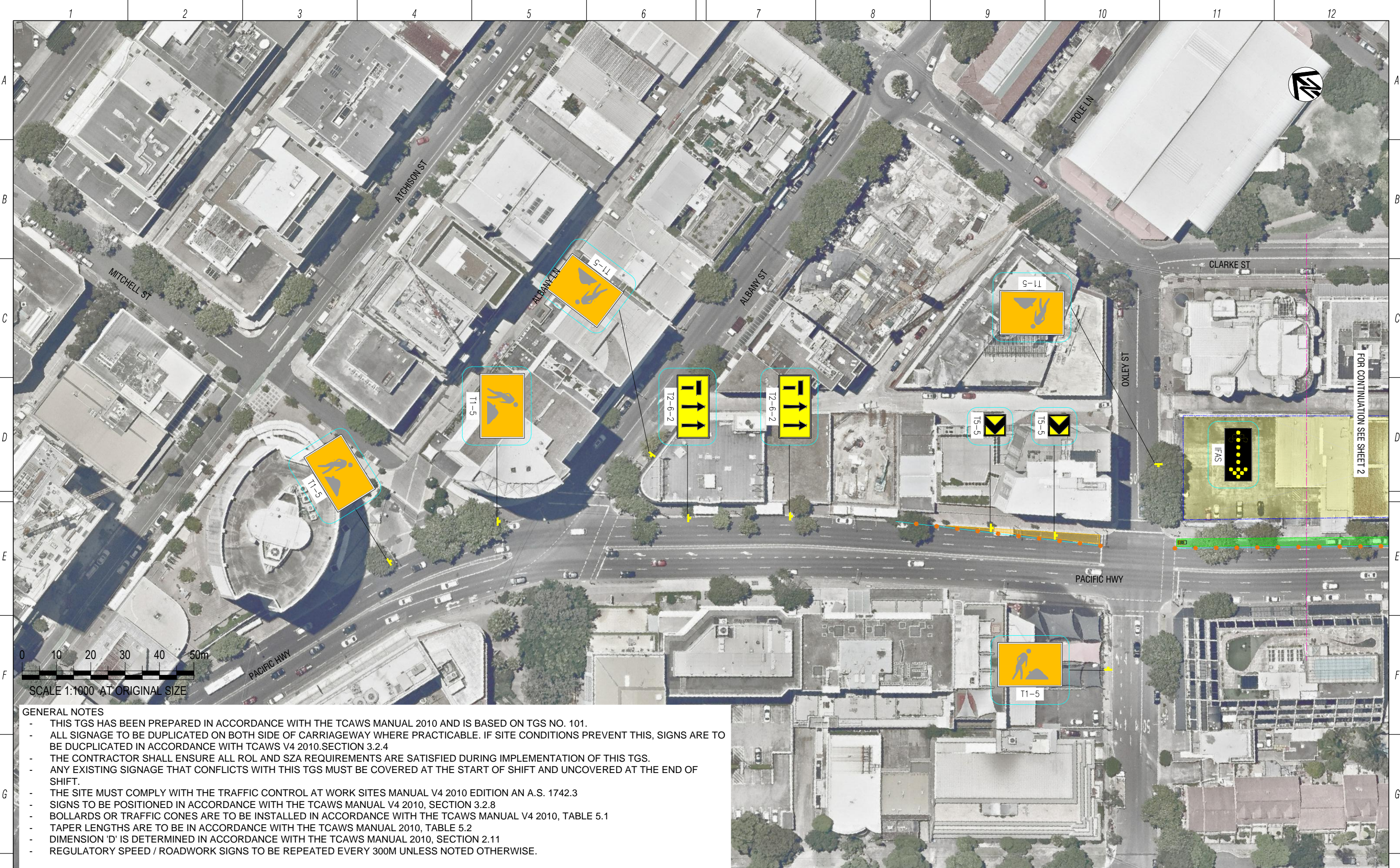
DRAWN BY: LS
 DRW CHECK: SL
 APPROVED: SL
 IND REVIEW: N/A

PROJECT

CLIENT

SYDNEY METRO
 DELTA GROUP AUSTRALIA WIDE
 OXLEY ST SLOW LANE CLOSURE
 CROWS NEST

DRAWING No:	DD-SYDM-DWG-0003-08		
SHEET	1	OF	1
REVISION	A-00		



GENERAL NOTES

- THIS TGS HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TGS NO. 101.
- ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUCPLICATED IN ACCORDANCE WITH TCAWS V4 2010,SECTION 3.2.4
- THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TGS.
- ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TGS MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
- THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V4 2010 EDITION AN A.S. 1742.3
- SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, SECTION 3.2.8
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- DIMENSION 'D' IS DETERMINED IN ACCORDANCE WITH THE TCAWS MANUAL 2010, SECTION 2.11
- REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

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		SCALE:

PROJECT CLIENT




SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
PACIFIC HWY SLOW LANE CLOSURE
CROWS NEST

DRAWING No:	DD-SYDM-DWG-0003-10	
SHEET	1	OF 2
REVISION	A-00	



GENERAL NOTES

- THIS TGS HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TGS NO. 101.
- ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUPLICATED IN ACCORDANCE WITH TCAWS V4 2010, SECTION 3.2.4
- THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TGS.
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- DIMENSION 'D' IS DETERMINED IN ACCORDANCE WITH THE TCAWS MANUAL 2010, SECTION 2.11
- REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

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APPROVED:		SL	
IND REVIEW:		N/A	
A-00	LS	05/03/2017	INITIAL DRAFT
REV	BY	DATE	DESCRIPTION
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:

PROJECT

CLIENT

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE

PACIFIC HWY SLOW LANE CLOSURE

CROWS NEST

DRAWING No:		DD-SYDM-DWG-0003-10	
SHEET	2	OF	2
REVISION	A-00		



- GENERAL NOTES**
- THIS TGS HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TGS NO. 101.
 - ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUCPLICATED IN ACCORDANCE WITH TCAWS V4 2010,SECTION 3.2.4
 - THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TGS.
 - ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TGS MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
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 - DIMENSION 'D' IS DETERMINED IN ACCORDANCE WITH THE TCAWS MANUAL 2010, SECTION 2.11
 - REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

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APPROVED:		SL	
IND REVIEW:		N/A	
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COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:

PROJECT CLIENT



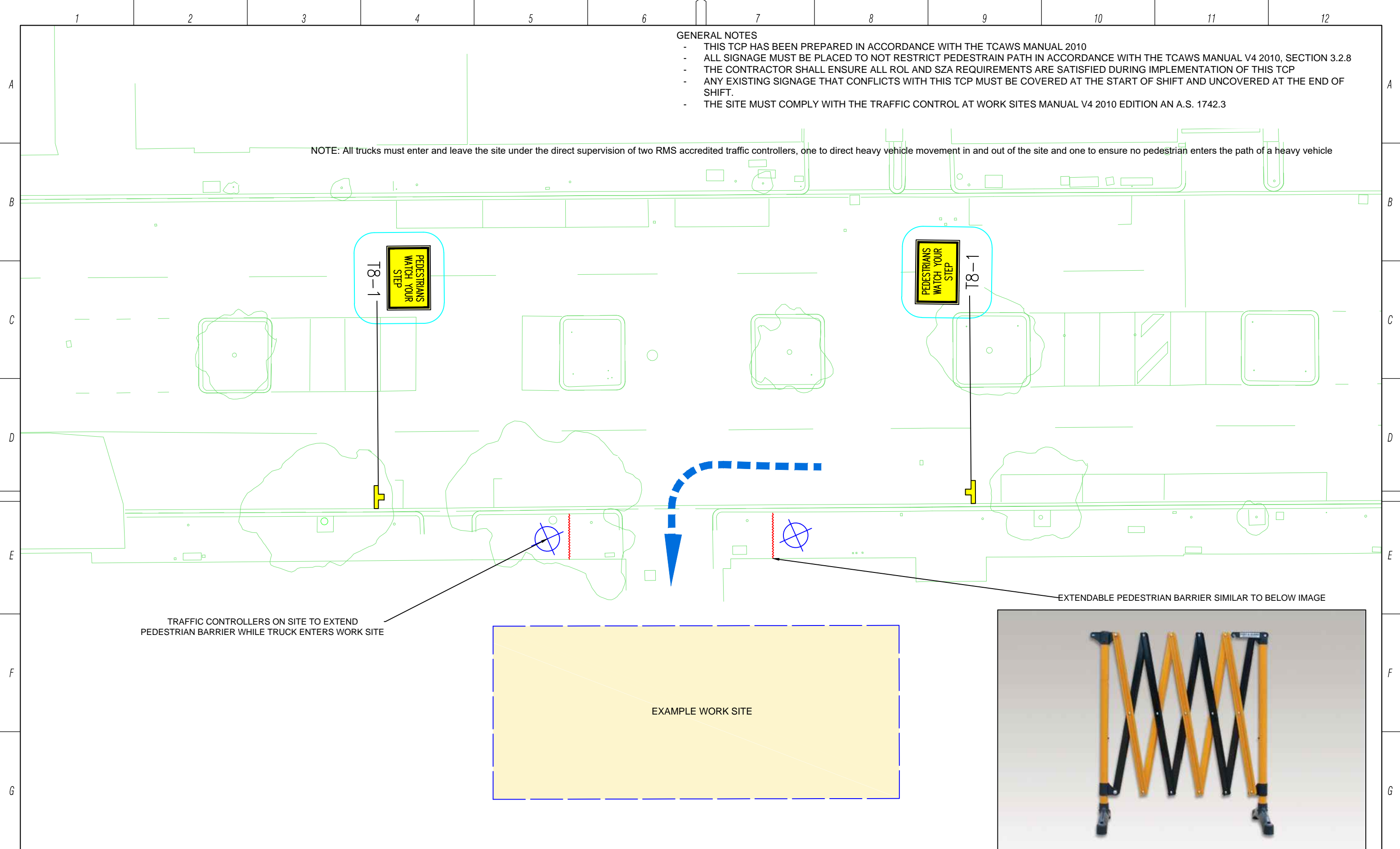

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
HUME ST SLOW LANE CLOSURE
CROWS NEST

DRAWING No:		DD-SYDM-DWG-0003-11	
SHEET	1	OF	1
REVISION	A-00		

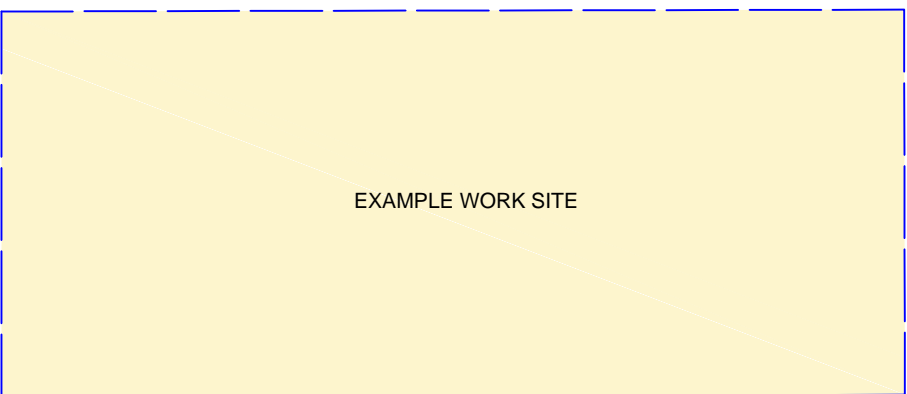
- GENERAL NOTES**
- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010
 - ALL SIGNAGE MUST BE PLACED TO NOT RESTRICT PEDESTRAIN PATH IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, SECTION 3.2.8
 - THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TCP
 - ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TCP MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
 - THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V4 2010 EDITION AN A.S. 1742.3

NOTE: All trucks must enter and leave the site under the direct supervision of two RMS accredited traffic controllers, one to direct heavy vehicle movement in and out of the site and one to ensure no pedestrian enters the path of a heavy vehicle



TRAFFIC CONTROLLERS ON SITE TO EXTEND PEDESTRIAN BARRIER WHILE TRUCK ENTERS WORK SITE

EXTENDABLE PEDESTRIAN BARRIER SIMILAR TO BELOW IMAGE



A-00	LS	19/02/2017	INITIAL DRAFT	SL
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APPROVED:	SL
IND REVIEW:	N/A



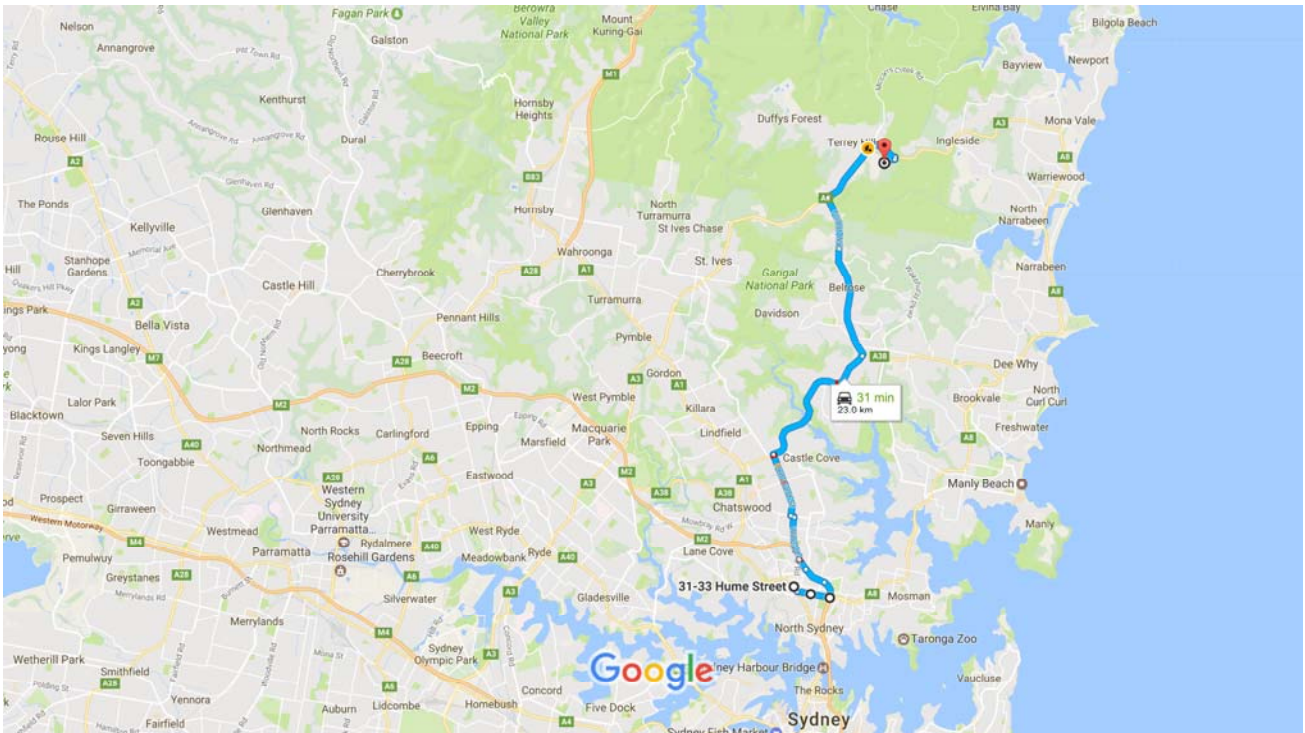
SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
TYPICAL PEDESTRIAN MANAGEMENT
SHORT TERM STOP ON FOOTPATH

DRAWING No:	DD-SYDM-TYP-0000-02	
SHEET	1	OF 1
REVISION	A-00	

Appendix 4 Haulage routes

DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0003-12	Logistics route –Crows Nest to Terrey Hills
DD-SYDM-DWG-0003-13	Logistics route – Terrey Hills to Crows Nest
DD-SYDM-DWG-0003-14	Logistics route – Crows Nest to Eastern Creek
DD-SYDM-DWG-0003-15	Logistics route – Eastern Creek to Crows Nest
DD-SYDM-DWG-0003-16	Logistics route – Crows Nest to Terrey Hills (Oxley Street)
DD-SYDM-DWG-0003-17	Logistics route – Crows Nest to Eastern Creek (Oxley Street)



Map data ©2017 Google 2 km

31-33 Hume St
Crows Nest NSW 2065

↑ 1. Head south-west on Hume St towards Pacific Hwy
4 s (13 m)

Take Penshurst St, A38, Forest Way and Mona Vale Rd/A3 to Kimbriki Rd in Ingleside
32 min (22.5 km)

↶ 2. Turn left at the 1st cross street onto Pacific Hwy
230 m

↶ 3. Turn left at the 1st cross street onto Falcon St
⚠ Partial toll road
1.2 km

⬆ 4. Turn left onto the M1/A ramp to Chatswood/Epping
⚠ Toll road
650 m

⬆ 5. Use the left lane to merge onto M1
850 m

↘ 6. Take the Willoughby Rd exit towards Roseville
400 m

↘ 7. Use the right lane to continue towards Willoughby Rd
40 m

- 8. Turn right onto Willoughby Rd
1.6 km
- ↶ 9. Use the left 2 lanes to turn left onto Mowbray Rd
140 m
- 10. Use the right 2 lanes to turn right onto Penshurst St
2.3 km
- 11. Turn right onto Boundary St/A38
ⓘ Continue to follow A38
5.6 km
- ↶ 12. Use the left 2 lanes to turn left onto Forest Way
4.1 km
- ↑ 13. Continue straight to stay on Forest Way
1.9 km
- 14. Use any lane to turn right onto Mona Vale Rd/A3
3.5 km

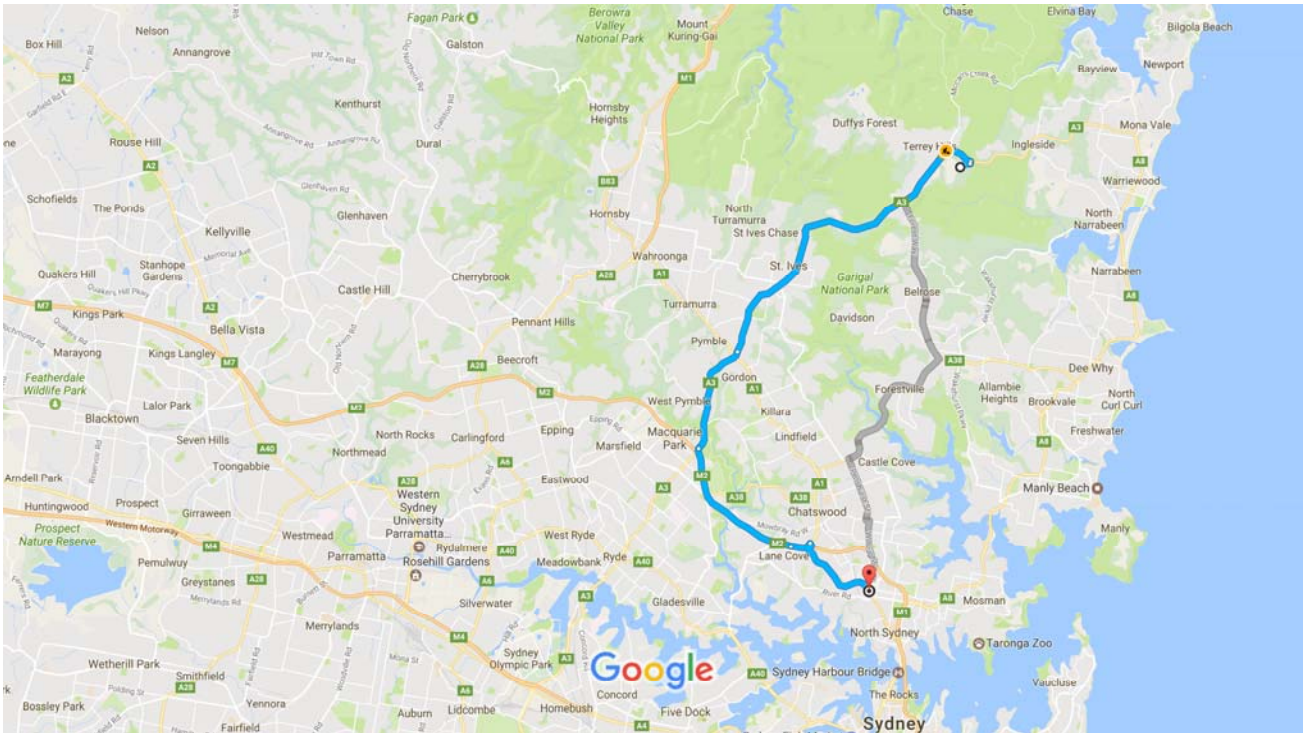
Drive to your destination

- 15. Turn right onto Kimbriki Rd
2 min (550 m)
- 16. Slight right
82 m
- ↶ 17. Turn left
ⓘ Destination will be on the left
400 m
- 30 m

Kimbriki Resource Recovery Centre

Kimbriki Road, Off Mona Vale Road, Terrey Hills NSW 2084

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 2 km

Kimbriki Resource Recovery Centre

Kimbriki Road, Off Mona Vale Road, Terrey Hills NSW 2084

Continue to Mona Vale Rd/A3

- 1 min (550 m)
- ↑ 1. Head north
- 30 m
- ↻ 2. Turn right towards Kimbriki Rd
- 400 m
- ↶ 3. Slight left onto Kimbriki Rd
- 82 m

Follow A3 and M2 to Pacific Hwy in Artarmon. Take the Pacific Hwy exit from M2

- 23 min (23.1 km)
- ↶ 4. Turn left onto Mona Vale Rd/A3
- 12.8 km
- ↻ 5. Keep right to continue on Ryde Rd/A3
i Continue to follow A3
- 4.1 km
- ⬆ 6. Use the left lane to merge onto M2 via the slip road to Sydney
⚠ Toll road
- 5.3 km

↩ 7. Take the Pacific Hwy exit

⚠ Toll road

900 m

Follow Pacific Hwy to Hume St in Crows Nest

6 min (3.3 km)

↩ 8. Use any lane to turn left onto Pacific Hwy

3.2 km

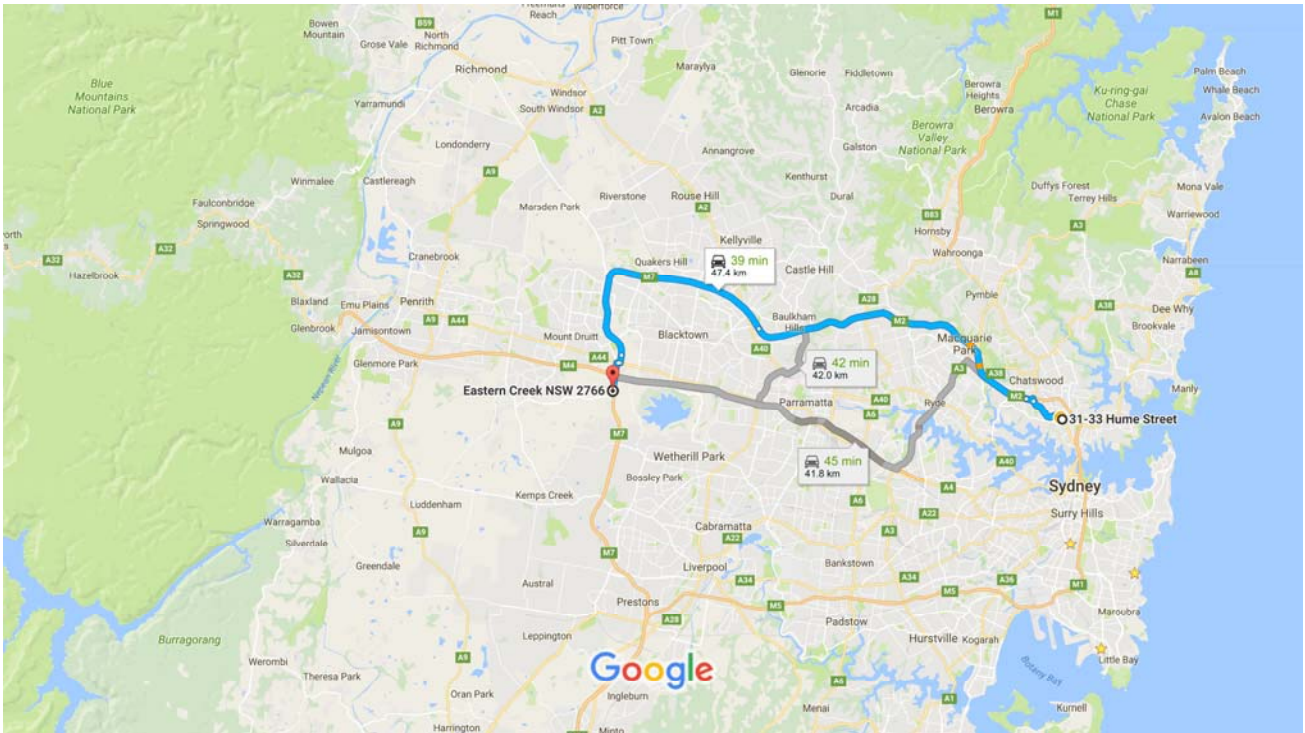
↩ 9. Turn left onto Hume St

13 m

31-33 Hume St

Crows Nest NSW 2065

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 5 km

31-33 Hume St
Crows Nest NSW 2065

Get on M2

- ↑ 1. Head south-west on Hume St towards Pacific Hwy 6 min (3.7 km)

- ➡ 2. Turn right at the 1st cross street onto Pacific Hwy 13 m

- ⬆️ 3. Use the right lane to take the Ln Cove Tunnel/M2 slip road to Epping/Windsor 2.8 km
⚠️ Toll road


- 900 m

Follow M2 and M7 to Great Western Hwy/A44 in Bungaribee. Take the Gt Western Hwy/A44 exit from M7

- ⬆️ 4. Merge onto M2 29 min (41.5 km)
⚠️ Toll road

- ↑ 5. Continue onto M7 22.6 km
⚠️ Toll road

- 18.3 km


 6. Take the Gt Western Hwy/A44 exit towards Eastern Creek/St Marys

 Toll road


550 m

Drive to Wallgrove Rd in Eastern Creek

4 min (2.2 km)

 7. Use the 2nd from the left lane to turn right onto Great Western Hwy/A44

150 m

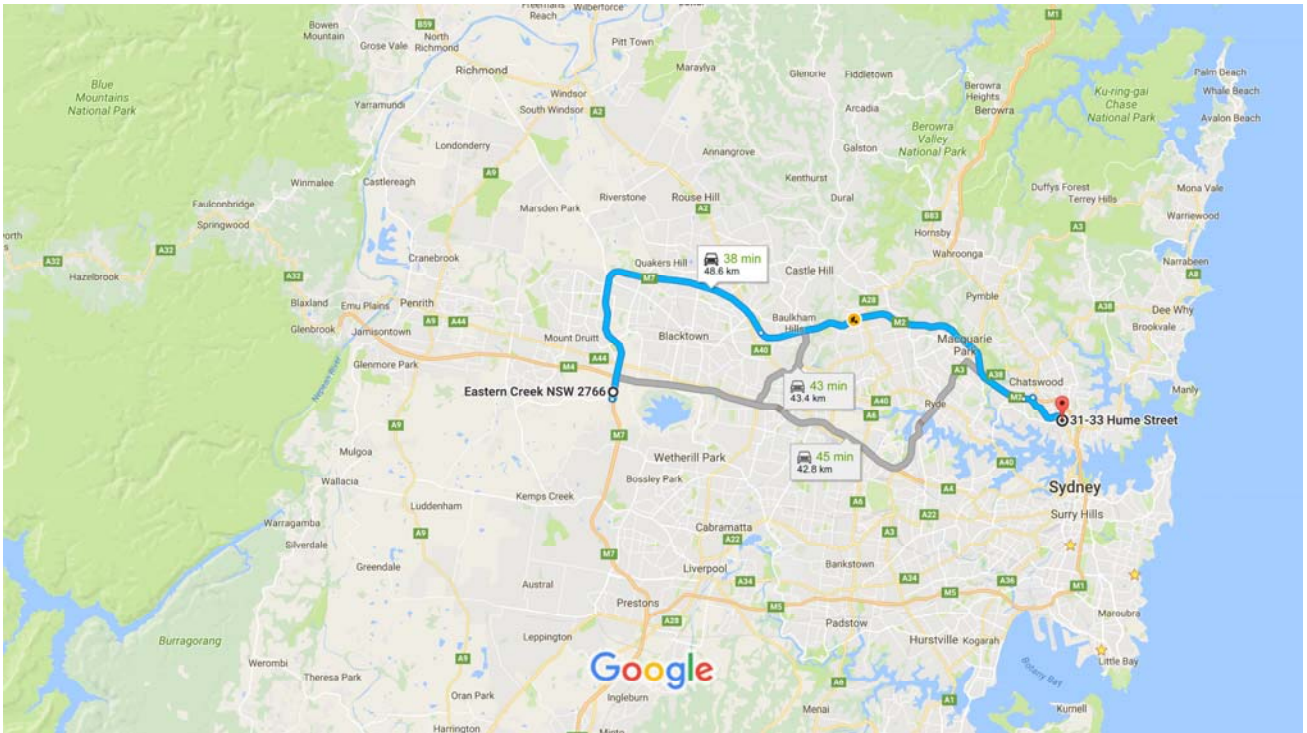
 8. Turn left onto Wallgrove Rd

2.1 km

Eastern Creek

New South Wales 2766

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 5 km

Eastern Creek

New South Wales 2766

- ↑ 1. Head south on Wallgrove Rd

550 m
- ↗ 2. Turn left to merge onto M7 towards Blacktown/NewCastle

⚠ Toll road

21.8 km
- ↑ 3. Continue onto M2

⚠ Toll road

22.1 km
- ↘ 4. Take the Pacific Hwy exit

⚠ Toll road

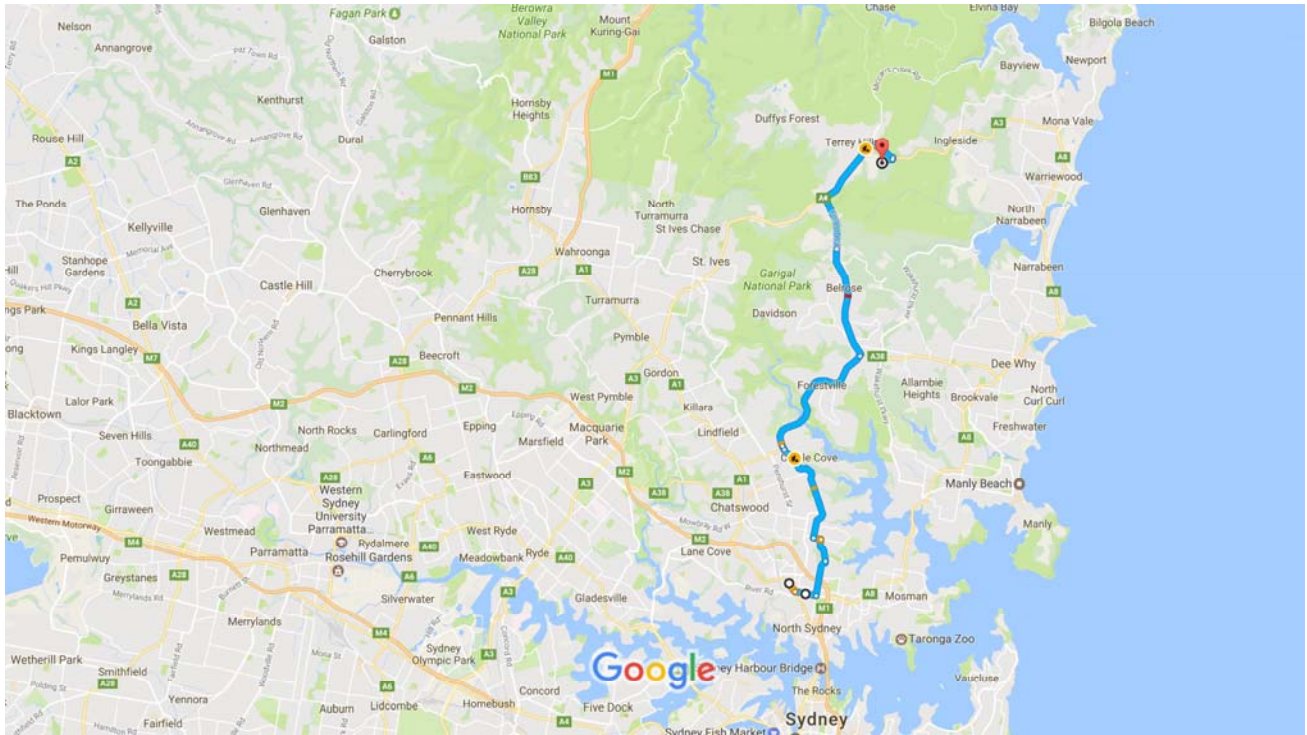
900 m
- ↙ 5. Use any lane to turn left onto Pacific Hwy

3.2 km
- ↙ 6. Turn left onto Hume St

13 m

31-33 Hume St

Crows Nest NSW 2065



Map data ©2017 Google 2 km

38 Oxley St

St Leonards NSW 2065

- ↑ 1. Head south-west on Oxley St towards Pacific Hwy

6 s (20 m)

Take Eastern Valley Way, Warringah Rd/A38, Forest Way and Mona Vale Rd/A3 to Kimbriki Rd in Ingleside

32 min (22.3 km)

- ↶ 2. Turn left onto Pacific Hwy

350 m

- ↶ 3. Turn left onto Falcon St

750 m

- ↶ 4. Turn left onto Miller St

1.3 km

- ↑ 5. Continue onto Strathallen Ave

850 m

- ↶ 6. Use the left 2 lanes to turn left onto Sailors Bay Rd

270 m

- ↷ 7. Use any lane to turn right onto Eastern Valley Way

3.9 km

- ↑ 8. Continue onto Clive St

140 m

- 9. Use any lane to turn right onto Warringah Rd/A38
5.1 km
- ↶ 10. Use the left 2 lanes to turn left onto Forest Way
4.1 km
- ↑ 11. Continue straight to stay on Forest Way
1.9 km
- 12. Use any lane to turn right onto Mona Vale Rd/A3
3.5 km

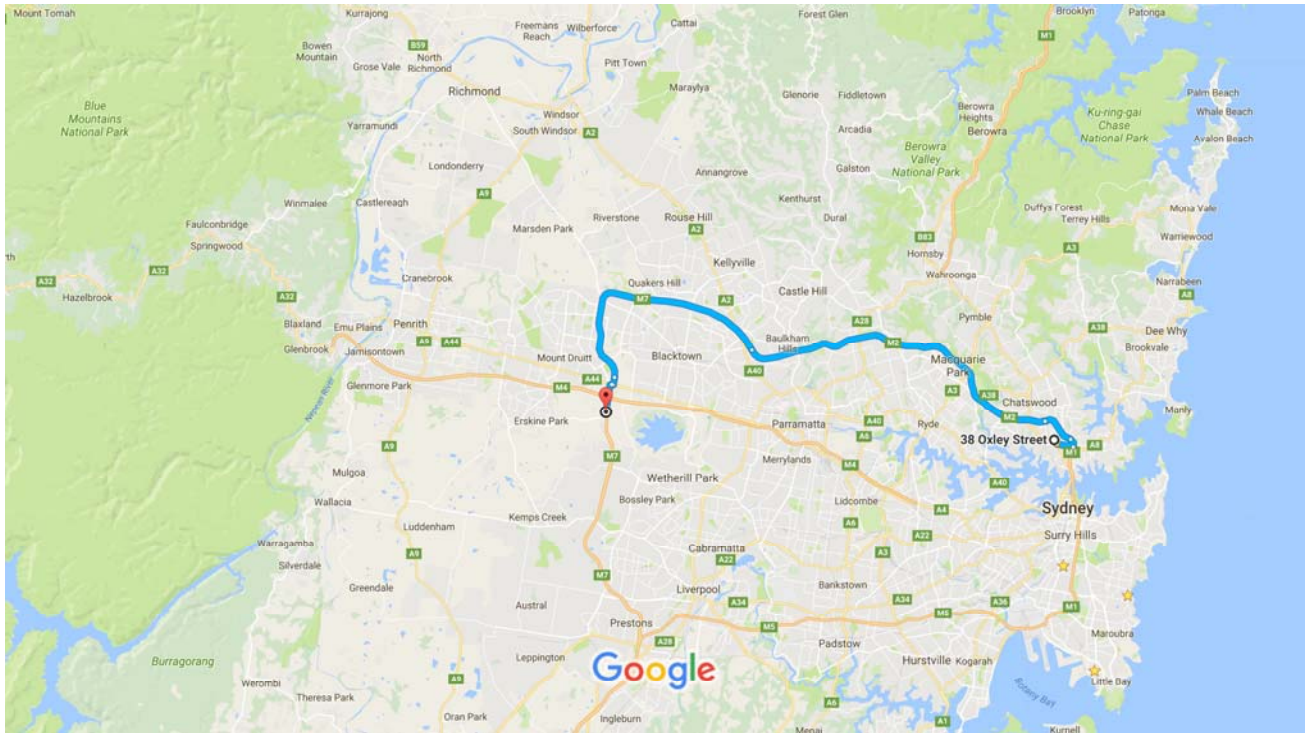
Drive to your destination

- 13. Turn right onto Kimbriki Rd
2 min (550 m)
- 14. Slight right
82 m
- 14. Slight right
400 m
- ↶ 15. Turn left
📘 Destination will be on the left
30 m

Kimbriki Resource Recovery Centre

Kimbriki Road, Off Mona Vale Road, Terrey Hills NSW 2084

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 5 km

38 Oxley St

St Leonards NSW 2065

Get on M1 in Cammeray from Falcon St

- ↑ 1. Head south-west on Oxley St towards Pacific Hwy 5 min (2.2 km)

- ↶ 2. Turn left onto Pacific Hwy 20 m

- ↶ 3. Turn left onto Falcon St 350 m
⚠ Partial toll road

- ⤴ 4. Turn left onto the M1/A ramp to Chatswood/Epping 1.2 km
⚠ Toll road

- ⤴ 650 m

Follow M2 and M7 to Great Western Hwy/A44 in Bungaribee. Take the Gt Western Hwy/A44 exit from M7

- ⤴ 5. Merge onto M1 32 min (45.8 km)

- 2.5 km

- 6. Keep right to continue on M2
⚠ Toll road
24.5 km
- ↑ 7. Continue onto M7
⚠ Toll road
18.3 km
- ↙ 8. Take the Gt Western Hwy/A44 exit towards Eastern Creek/St Marys
⚠ Toll road
550 m

Drive to Wallgrove Rd in Eastern Creek

- 9. Use the 2nd from the left lane to turn right onto Great Western Hwy/A44
4 min (2.2 km)
150 m
- ↙ 10. Turn left onto Wallgrove Rd
2.1 km

Eastern Creek

New South Wales 2766

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.

Appendix 5 Road safety audit

ROAD SAFETY AUDIT FINDINGS AND RECOMMENDATIONS

Audit Stage: Desktop Traffic Management

Title of package: Crows Nest Site (Sydney Metro Project)

Description of works: Building Construction

Plans / documents: CTCP 0003 April 2017 Rev B-01

Client: Delta Group (Sue Lewis)


Audit company: Road Safety Audits P/L


Senior auditor/s: Peter Harris (Level 3 SRSA NSW)

Report date: 11/04/07

RSA P/L reference: RSA-05547

Notes: -

Point No.	Issue	Suggestion	Delta Group (Sue Lewis) Response	
			Accept / Reject	Comment / Status
1.	Clarke Lane appears to have significant pedestrian and vehicle access points.	Review whether this has been carefully considered in relation to the changed traffic flow and access points.	Accept	Onsite inspections have shown little pedestrian movements on Clarke Lane between Hume Lane and Oxley Street
				

Point No.	Issue	Suggestion	Delta Group (Sue Lewis) Response	
			Accept / Reject	Comment / Status
2.	Similar to the previous point, egress from Clarke Lane into Hume Street appears to abut a café and have a blind corner.	Review whether this has been carefully considered in relation to the increased traffic flow.	Reject	This laneway services all of the buildings that currently front the Pacific Highway and Clarke Street. the traffic generated by these developments will be significantly more than generated for the demolition of two buildings at this location
				
3.	Given the alleyways are narrow and that the kerb lines are traced from aerial images, it would be prudent to take the design vehicle onto site to demonstrate that it can manoeuvre as intended.	Consider.	Accept	Current vehicles that service the businesses include large garbage vehicles (dumpster pick ups) and grease trap removalists which are likely to be in the same order of magnitude. We will undertake a trial prior to commencement
4.	Appendix 3: Plan 0003-10 sheet 1: the flashing arrow is (presumably) on the departure side of the intersection. It's considered that this will be more effective on the approach side. This also applies to sheet 0003-11	Consider relocating.	Reject	The flashing arrow is also used to forewarn motorists turning out from the side streets of the lane closures in place
5.	Appendix 3: Plan 0003-10 sheet 1: It's unclear where the flashing arrow is to be located and which way it's facing, and why.	Clarify.	Reject	Refer to Item 4 response

Notes: -

Appendix 6 Review comments

CTMP Review Comments Sheet

Project Title
Sydney Metro City & south west – Demolition Contract

CTMP Title: Crows Nest Site

Document Number:

DD SYDM TMP 0003 B 00

Reviewer: P Smith

Responder: S Lewis

Date: 24 March 2017

 Date: 26th March 2017

Item	Section/ Page	Comments	Response
1	3.2.2	Contraflow on Hume St or stop slow?	Document amended
2	DWG-04	<p>Drawing DD-SYDM-DWG-0003-04 indicates movement into site should be done under stop/slow (albeit contraflow for the truck movement). The swept path for trucks entering and leaving Clarke Lane shows it mount the kerb and utilises footpath.</p> <p>The swept path for trucks exiting the site commences from within Clarke Lane. Can the vehicles exit to this location from the site?</p> <p>The swept path shows trucks in lane 2 on Hume Street – lane 1 traffic can proceed straight ahead hence there is a safety conflict here</p>	Drawings have been amended
3	DWG-05	Drawing DD-SYDM-DWG-0003-05 shows vehicles tracking over the kerb and gutter entering and exiting Clarke Lane	Document and drawing amended

Item	Section/ Page	Comments	Response
		<p>What are the proposed turning movements from the Pacific Highway?</p> <p>Can the vehicles make the left turn or right turn and still access Clarke Lane?</p> <p>Swept path needs to show that trucks can make the turn from Clarke Street to Hume Street to Clarke Lane</p>	Left in from Pacific Highway, left out from both Hume Street and Oxley Street, right turn out from Hume Street
4	DWG-06	Drawing DD-SYDM-DWG-0003-06 shows vehicles tracking over the kerb and gutter and into the footpath space. The swept path shows trucks in lane 2 on Oxley Street, lane 1 traffic can proceed straight ahead hence there is a safety conflict here	Document and drawings amended
5	DWG-08	Drawing DD-SYDM-DWG-0003-08 shows traffic control set up on Oxley Street (impact on signals at highway). Appears traffic control doesn't allow for truck swept paths in and out of the lane?	That's correct as this lane closure is to be implemented at night – document amended refer to Appendix 3
6	General TCP	General TCP comments – no indication of when likely to be used (ie: every day or only during large movements?)	Refer to Appendix 3
7	DWG-10	Drawing DD-SYDM-DWG-0003-10 no indication of what this TCP is required for. Will only be permissible during night time hours?	Refer to Appendix 3
8	Review	The CTMP review sheet is for the Chatswood site TMP	Noted – it was included to show the development of the CTMP by changes that were made to all documents based on the original review by TfNSW

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
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CTMP Title: Crows Nest Site

Document Number:

DD SYDM TMP 0003 B 00

Reviewer: M Kemp

Responder: S Lewis

Date: 24 March 2017

Date: 26th March 2017

Item	Section/ Page	Comments	Response
1	General	Please find Council's preferred Conditions of Approval attached	Noted
2	3.1.2	Oxley Street cycling facilities. There is an existing south-bound separated cycleway in Oxley Street between Albany Street and Clarke Street, and at the intersection of Clarke Street. The northbound lane is a mixed traffic arrangement with bike logos marked.	Document amended
3	3.1.3	Hume Street Bus Zone The Bus Zone on the southern side of Hume Street between Clarke Lane and Clarke Street is used as a layover for STA buses.	Document amended
4	3.1.4	Clarke Lane between Pole Lane and Oxley Street is two way	Document amended
5	3.1.5	Clarke Street parking restrictions. Parking restrictions on the eastern side of Clarke Street between Oxley Street and Hume Street are 10 minute parking/ Bus Zone (CTMP states 10P).	Document amended
6	3.2.1	Approval must be obtained from Council for all temporary construction vehicular crossings. Application form is available on Council's website at https://www.northsydney.nsw.gov.au/Building_Development/Building_DA_Forms/General_Forms#9	Noted and document amended

Item	Section/ Page	Comments	Response
7	3.2.2.1 & App 2 DWG 0003-03	<p>The legend shows a green area representing "Scaffold off ground, A or B Class". It is not clear how a B Class hoarding would work in Clarke Lane due to the very narrow footpaths. North of Hume Street the footpaths are wider on the eastern side of Clarke Lane and south of Hume Street the footpaths are wider on the eastern side of Clarke Lane. Will pedestrian access be able to be maintained on the wider footpaths with the hoarding/scaffolding?</p> <p>Approval must be obtained from Council for all temporary construction hoardings. Application form is available on Council's website at https://www.northsydney.nsw.gov.au/Building_Development/Building_DA_Forms/Construction_Forms#4</p>	Document amended.
8	4 & App 2 DWG 0003-04	Swept path diagrams in and out of Clarke Lane at Hume Street show truck and dog trailers will over-run the footpaths. This truck type is therefore unsuitable and a smaller truck must be used. No truck and dog trailers are to be used for this site.	Document and drawings amended
9	4 & App 2 DWG 0003-05	Swept path diagrams in and out of Clarke Lane at Hume Street show truck and dog trailers will over-run the footpaths. This truck type is therefore unsuitable and a smaller truck must be used. No truck and dog trailers are to be used for this site	Document and drawings amended
10	4 & Appendix 2 Drawing DD-SYDM- DWG-0003-06	Swept path diagrams out of Clarke Lane at Oxley Street show truck and dog trailers will over-run the footpath. This truck type is therefore unsuitable and a smaller truck must be used. No truck and dog trailers are to be used for this site	Document and drawings amended
11	Appendix 2 Drawing DD-SYDM-TYP- 0000-01	Site specific hoarding plans are required to understand the impacts on pedestrian access, street trees and street furniture	The site-specific hoarding designs are due shortly and will be submitted with the hoarding application directly to Council.
12	Appendix 3 Drawing DD-SYDM-DWG- 0003-08	The schedule notes this plan is for a slow lane closure on Oxley Street for driveway construction. There is an existing driveway in Oxley Street which is proposed to be used for construction egress, however no modifications to the driveway are proposed elsewhere in the CTMP. Clarification needed as to what activity this TCP is for.	Document amended

Item	Section/ Page	Comments	Response
13	Appendix 3 Drawing DD-SYDM-DWG-0003-09	An additional traffic controller should be added to the TCP on Clarke Street south of Clarke Lane to ensure no pedestrians enter the path of the heavy vehicle.	Drawing now removed
14	Appendix 3 Drawing DD-SYDM-DWG-0003-10	Slow lane closure on Pacific Highway. What activities is this TCP required for? Is this only for hoarding installation?	Document amended
15	Appendix 3 Drawing DD-SYDM-DWG-0003-11	Slow lane closure on Hume Street. What activities is this TCP required for? Is this only for hoarding installation?	Document amended
16	Appendix 3 Drawing DD-SYDM-TYP-0000-02	All trucks must enter and leave the site under the direct supervision of two RMS accredited traffic controllers, one to direct heavy vehicle movement in and out of the site and one to ensure no pedestrian enters the path of a heavy vehicle.	Noted and drawing amended with note
17	Appendix 4 Haulage routes	<p>The haulage routes refer to access and egress on Clarke Street. There is no need to use Clarke Street between Oxley Street and Hume Street.</p> <p>Access routes refer to left turn onto Oxley Street from Pacific Highway and right turn onto Clarke Street. This is unnecessary. Use of local roads should be avoided as far as possible. All trucks should approach southbound on Pacific Highway and turn left onto Hume Street then either left into Clarke Lane; or right into Clarke Lane (under contra-flow traffic control); or right into Clarke Street and right into Hume Lane.</p> <p>Swept path diagrams for the largest suitable vehicle (ie single rigid truck) at Clarke Street/ Hume Lane intersection, at the hairpin bend at Clarke Lane/ Hume Lane south of Clarke Street, and at the driveway entries/exits needs to be submitted to ensure trucks can safely access the southern site via the Clarke Street, Hume Lane and Clarke Lane route.</p> <p>All egress routes should be via left turn into Hume Street or Oxley Street from Clarke Lane and directly onto the Pacific Highway.</p>	Document amended

Item	Section/ Page	Comments	Response
18	Appendix 6	Comments appear to relate to Chatswood site	They were included as these comments guided the development of the CTMP
19	General – street trees	Any impacts on street trees must be discussed in advance with Council's Tree Management Officer – Phone 9936 8100	No impacts on street trees proposed
20	General – parking signs and parking meters	Any impacts on existing parking signs or parking meters must be discussed in advance (allow 6 weeks) with Council's Traffic & Transport Operations Manager, Ms Michaela Kemp – Phone 9936 8100 to arrange necessary approvals and removal or relocation.	No impacts on parking meters or signs proposed
21	NSC Condition Item 6	Heavy vehicles must not use Clarke Street between Oxley Street and Hume Street to access or depart the site	Noted and document amended
22	NSC Condition Item 7	Trucks may only enter and leave the construction site in a forward direction	Noted and document amended
23	NSC Condition Item 9	The contra-flow arrangement on Clarke Lane may only be implemented for access to the southern site for trucks that are unable to travel in the conventional direction via Hume Lane and only under the management of a minimum of 3 RMS accredited traffic controllers – 2 at the intersection of Clarke Lane/Hume Street to ensure no vehicle or pedestrian enters the path of the heavy vehicle, and one to the south of the site to ensure no vehicle enters the path of the heavy vehicle.	Noted – document amended
24	NSC Condition Item 10	Accesses to adjoining properties are to be maintained at all times.	Suggested amendment – unless agreed with resident and Council
25	NSC Condition Item 11	The adjoining/ affected residents/occupiers are to be updated on a regular basis and at key demolition stages, particularly in relation to construction vehicle movements, and be provided with a phone number to contact the site manager, and in accordance with the Sydney Metro Community Consultation Strategy – Early Works	This is part of TfNSW's communications works. Residents/ occupiers will likely be provided with TfNSW 1800 number and not the site manager– either way communications and consultation with the public is through TfNSW/
26	NSC Condition Item 12	At no time shall Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway or any other road or Council property be blocked by any	Suggested amendment, unless agreed otherwise with SCO/ NSC

Item	Section/ Page	Comments	Response
		vehicle or works associated with the construction of the proposed development. Through traffic on Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway and any other road is to be maintained at all times.	

CONDITIONS OF APPROVAL

It is recommended that the traffic aspects of the Construction Traffic Management Plan be approved, subject to the following conditions:

1. All works on Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway and any public road are to be undertaken in accordance with AS 1742.3
2. Approval must be obtained from Council for all temporary construction vehicular crossings. Application form is available on Council's website at https://www.northsydney.nsw.gov.au/Building_Development/Building_DA_Forms/General_Forms#9
3. If required, the applicant must contact Council's Parking Meter Manager on 9936 8100 a minimum of two weeks in advance to arrange relocation of any parking meter infrastructure, including in-ground sensors.
4. Approval must be obtained from Council for all temporary construction hoardings. Application form is available on Council's website at https://www.northsydney.nsw.gov.au/Building_Development/Building_DA_Forms/Construction_Forms#4
5. Installation of a Work Zone for Metro Crows Nest site is subject to the applicant paying all appropriate Council advertised fees and charges, including the cost of the signage.
6. Heavy vehicles must not use Clarke Street between Oxley Street and Hume Street to access or depart from the site.
7. Trucks may only enter and leave the construction site in a forwards direction.
8. All trucks must enter and leave the site under the direct supervision of two RMS accredited traffic controllers, one to direct heavy vehicle movement in and out of the site and one to ensure no pedestrian enters the path of a heavy vehicle.
9. The contra-flow arrangement on Clarke Lane may only be implemented for access to the southern site for trucks that are unable to travel in the conventional direction via Hume Lane and only under the management of a minimum of 3 RMS accredited traffic controllers – 2 at the intersection of Clarke Lane/Hume Street to ensure no vehicle or pedestrian enters the path of the heavy vehicle, and one to the south of the site to ensure no vehicle enters the path of the heavy vehicle.
10. Accesses to adjoining properties are to be maintained at all times.
11. The adjoining/ affected residents/occupiers are to be updated on a regular basis and at key demolition stages, particularly in relation to construction vehicle movements, and be provided with a phone number to contact the site manager, and in accordance with the *Sydney Metro Community Consultation Strategy – Early Works*
12. At no time shall Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway or any other road or Council property be blocked by any vehicle or works associated with the construction of the proposed development. Through traffic on Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway and any other road is to be maintained at all times.
13. Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway, or any other road are not to be used as a waiting area for trucks delivering to or awaiting pick up of materials etc. from the proposed development.
14. Construction vehicles are not to queue on Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway or any other road.
15. Any approved Works Zone is to be used only to pick up and drop off materials and equipment. Construction vehicles are not to wait or park in the Works Zone.

16. Materials are to be stored on-site. At no time, are materials to be stored on Hume Street, Oxley Street, Clarke Lane, Hume Lane, Clarke Street, Pacific Highway or any other road or any Council property.
17. Mobile cranes shall be located wholly on-site or with an approved Stand Plant Permit
18. Concrete pumps shall be located wholly on-site or with an approved Stand Plant Permit, unless the pump and trucks can stand wholly within an approved and signposted Work Zone.
19. Any changes required to existing parking or meter infrastructure, including in-ground sensors must be approved through Council's Traffic & Transport Operations section. Council's standard fees and charges will apply. Phone 9936 8100.
20. All pump lines crossing Council footpaths must be ramped over to allow safe pedestrian/wheelchair traffic at all times.
21. Pedestrian access and the diversion of pedestrians shall be carried out in accordance with Australian Standard 1742.3 and 1742.10.
22. If pedestrians are diverted, pram ramps must be provided in accordance with Australian Standard 1428.1.
23. If a Permit to Stand Plant or Temporary Road Closure is required, application must be made to NSW Police, Harbourside Local Area command.
24. If a Permit to Stand Plant is required, application must be made to Council a minimum of two weeks prior to any proposed works.
25. If a Temporary Road Closure is required, application must be made to Council a minimum of three weeks prior to any proposed works.
26. Should any street trees be impacted by these works then Council's Tree Management Officer must be consulted at least 2 weeks prior to any works proceeding. Phone 9936 8100.
27. A traffic route map and conditions are to be made available to truck drivers engaged for this development.
28. A list of truck drivers' names with their licences and vehicle plate numbers and conditions are to be kept on-site by the applicant at all times, and be made available for inspection by Council Officers, Police Officers and Council Rangers.
29. Repeated failure to comply with these conditions will result in removal of any Work Zone under notice.
30. All Traffic Management Plans and Traffic Control Plans must be endorsed with the name of the person preparing the plan along with their level of certified qualification and certificate number. Only persons with current "Select/Modify Traffic Control Plans" or "Design Audit Traffic Control Plans" tickets are qualified to develop and endorse Traffic Management Plans and Traffic Control Plans

Please note the construction management program is a condition that forms part of the development application for the site. Therefore any non-compliance with these construction management program conditions of approval constitutes a breach of the conditions of approval for the development application.

COMMENTS REGISTER

Report Name:	Construction Traffic Management Plan Crows Nest Sites		
Author:	Sydney Metro Chatswood to Sydenham		
Version:	Revision B-00		
Date Received :	14/03/17		
Section	Issue	Stakeholder Comment	DELTA D&C Response
3.1.1	Pacific Highway Safety Camera	Please clarify whether the safety camera at the Hume St and Pacific Highway is intended to be affected at some stage. If it is then there is need to approach the RMS to advise of the impacts. If removal or otherwise is required there is need for ample leading times before it occurs.	No impact during the demolition works
3.1.3	Hume street changes & Impacts to Bus Stop	Any lane width changes, lane/road closures or similar affecting the Hume Street and Pacific Highway intersection will trigger the revision of the Traffic Control Signal plan (TCS plan). A new plan is to be drawn and submitted to RMS for approval with timing issues adherent to the process. To approve a new TCS Plan it may take around three months. Please advise if impacts are expected on the Bus Stop along Hume Street between Clarke Lane and Clarke Street. Any changes to bus stops are to be undertaken through the bus operators with the Coordination Office overseen and facilitating if required.	No change is proposed to the operation of the TCS. The existing bus stop on Pacific Highway and Hume Street are unaffected by the works.
3.2.4	Impact on public transport	If response to above-mentioned issue is positive please include impacts in this section as well..	NA
3.2.8	Cumulative impacts	Please advise if the proposed closure would affect your works. This may be complimentary to the proposed closure by Sydney Metro.	No impact on our works as this is one way street eastbound
3.2.10	TCP and ROL identified works	A number of these activities will require additional approval from council. The second item refers to oversized / over dimensional vehicles. Please advise what this is for, is the intention to have an over mass or oversize vehicle into the site ? If this is the case there is need to include in the body of the TMP.	Document amended
4.	Table 1	This table appears to be for Victoria Cross Sites as its name implies. Please amend or include the appropriate table.	Document amended
4.2	Permits Over Dimensional	Please advise what over dimensional vehicles are intended for the site.	Document amended

	vehicles		
5.1	Road safety audits	There is need to include a road safety audits in the draft TMPs, please include. This may result in further comments being submitted.	Noted to be provided in new CTMP
5.2	Emergency Services	Please include in this section how the company intends to advise the emergency services of their activities.	Document amended
5.3.1	Public transport interruptions	Section 3.2.4 advises there are no impacts on public transport. This section refers to Public Transport interruptions. Please advise what interruptions and modify TMP sections accordingly.	Document amended
5.4	External Events	The company is to facilitate special events. TMC would not issue ROLs that may impact on during special events. TMC is not aware of Special Events around this location.	Section removed
5.7	On Site contacts	This section is currently blank. There is need to have 24 hour contacts should an emergency arise.	Document amended
Appendix 3	TCPs	The company will require TCPs when applying for ROLs.	Noted
Appendix 4	Haulage Routes	Why is the route going back to North Sydney to get onto the Warringah Freeway, would not it be better to take Falcon St and then the on ramp onto the Warringah freeway ?	Document amended
Appendix 5	Road Safety Audit	Please provide road safety audits in draft TMP submissions. Once provided this may generate further comments.	Noted

Armstrong, Ben

From: McNamara, Alan <ALAN.MCNAMARA2@transport.nsw.gov.au>
Sent: Tuesday, 23 May 2017 2:38 PM
To: Alan McNamara
Subject: FW: RE: CTMP for Crows Nest Site

From: RAMIREZ Giovanni [<mailto:Giovanny.RAMIREZ@tmc.transport.nsw.gov.au>]
Sent: Friday, 12 May 2017 1:59 PM
To: Ben Shum; Sue; O'Leary, Peter
Cc: SMITH Peta A; ISSA Steven C
Subject: RE: CTMP for Crows Nest Site

Ben / Sue / Peter

In accordance with clause 2.3 (c) of the General Conditions, Schedule E3-Project Planning Approval and Conditions & the SMR E – Sydney Metro City and Southwest Demolition Contract the Roads and Maritime Service of NSW and the Sydney Coordination Office approve the Sydney Metro City & South West Construction Traffic Management Plan Crows Nest Sites April 2017 Rev C-00 for demolition and related removal works for the Crows Nest Sites subject to the following requirements:

- Obtaining Road Occupancy Licences from the Transport Management Centre as required
- Undertake lane closures only as per approved ROLs issued by TMC
- North Sydney Council Metro Crows Nest Site conditions are met
- Addressing any issues raised by Council, STA, residents, businesses and/or Emergency Services that may arise in the TMP approval process
- The Construction Traffic Management Plan being updated as required prior to construction commencing to ensure consistency with the final Construction Traffic Management Framework when it is approved
- Promptly addressing any CBD Taskforce and/or TMC and/or RMS issue that eventuates during the works

Regards

Giovanny Ramirez
Principal Manager Taskforce Operations
CBD Transport Taskforce | Sydney Coordination Office
Transport for NSW
T 02 8396 1460 M 0403 098 060
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Mr Stuart Hodgson
Principal Manager,
Program Sustainability Environment & Planning
Sydney Metro
Transport for NSW
PO Box 588
NORTH RYDE BC NSW 1670

27 June 2017

Ref: CTMP-Crows Nest

Dear Stuart

RE: Endorsement of Construction Traffic Management Plan – Crows Nest Sites Demolition Site

Further to my letter dated 26 May 2017, thank you for providing the following document for Environmental Representative (ER) review as required by the Condition of Approval A24(i) of the Sydney Metro City & Southwest project (SSI – 15_7400 January 9 2017).

- Chatswood Pitt Crows Nest Haul Road Consistency Assessment (undated and unsigned)

As an approved ER for the Sydney Metro City & Southwest project, I have reviewed the above document. This review has been limited to the requirements of Condition of Approval A24(i) to “provide advice on any additional mitigation measures required to minimise the impact of the work”. Comments regarding additional mitigation measures for traffic and community management have been raised with Sydney Metro and incorporated into the Consistency Assessment as relevant.

This Consistency Assessment provides assessment and confirmation by Sydney Metro that use of Clarke Lane south of Hume Street is consistent with the approved project. It is considered that this therefore meets the requirements of my conditional endorsement of the *Construction Traffic Management Plan – Crows Nest Sites Demolition Site*, dated 26 May 2017.

It is noted that the Roads and Maritime Service of NSW and the Sydney Coordination Office conditions of approval for the *Construction Traffic Management Plan – Crows Nest Sites Demolition Site* remain as outlined in my earlier letter.



HBI

Healthy Buildings International Pty Ltd

Yours sincerely

Michael Woolley

Environmental Representative – Sydney Metro – City and South West