



**SYDNEY METRO CITY & SOUTH WEST
 CONSTRUCTION TRAFFIC MANAGEMENT PLAN
 CHATSWOOD DIVE SITE**

MAY 2017



DEVELOPMENT	VERIFICATION	VALIDATION
Name: S Lewis	Name: E Nuberg	Name: B Shum
Signature : 	Signature : 	Signature : 
RMS Card # 0022818927 Exp 12/06/18		

Document Approval

REV	DATE	DESCRIPTION	PREPARED	REVIEWED	APPROVED
A-00	7 Feb 2017	Draft for Review	S. Lewis		
A-01	8 Feb 2017	TCP works updated	S Lewis		
A-02	13 Feb 2017	Incorporate Delta comments	S Lewis		
A-03	16 Feb 2017	Willoughby Council meeting comments	S Lewis	B Shum	
B-00	4 Mar 2017	TfNSW comments incorporated	S Lewis	B Shum	
B-01	1 May 2017	RMS/ TfNSW/ SCO/ WC comments incorporated and CTMP sent to road safety auditor	S Lewis		
C-00	8 May 2017	Road safety auditor comments incorporated and CTMP sent for endorsement and approval	S Lewis	E Nuberg	B Shum
C-01	18 May 2017	CTMP amended as per Council comments dated 15 May and comments made at TCG on 16 May	S Lewis	E Nuberg	B Shum

Glossary

REQUIREMENT	REFERENCE
CTMF	Construction Traffic Management Framework
CTMP	Construction Traffic Management Plan
RMS	Roads and Maritime Service
ROL	Road Occupancy License
SCO	Sydney Coordination Office
TCP	Traffic Control Plan
TfNSW	Transport for New South Wales
WC	Willoughby Council

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1. Introduction

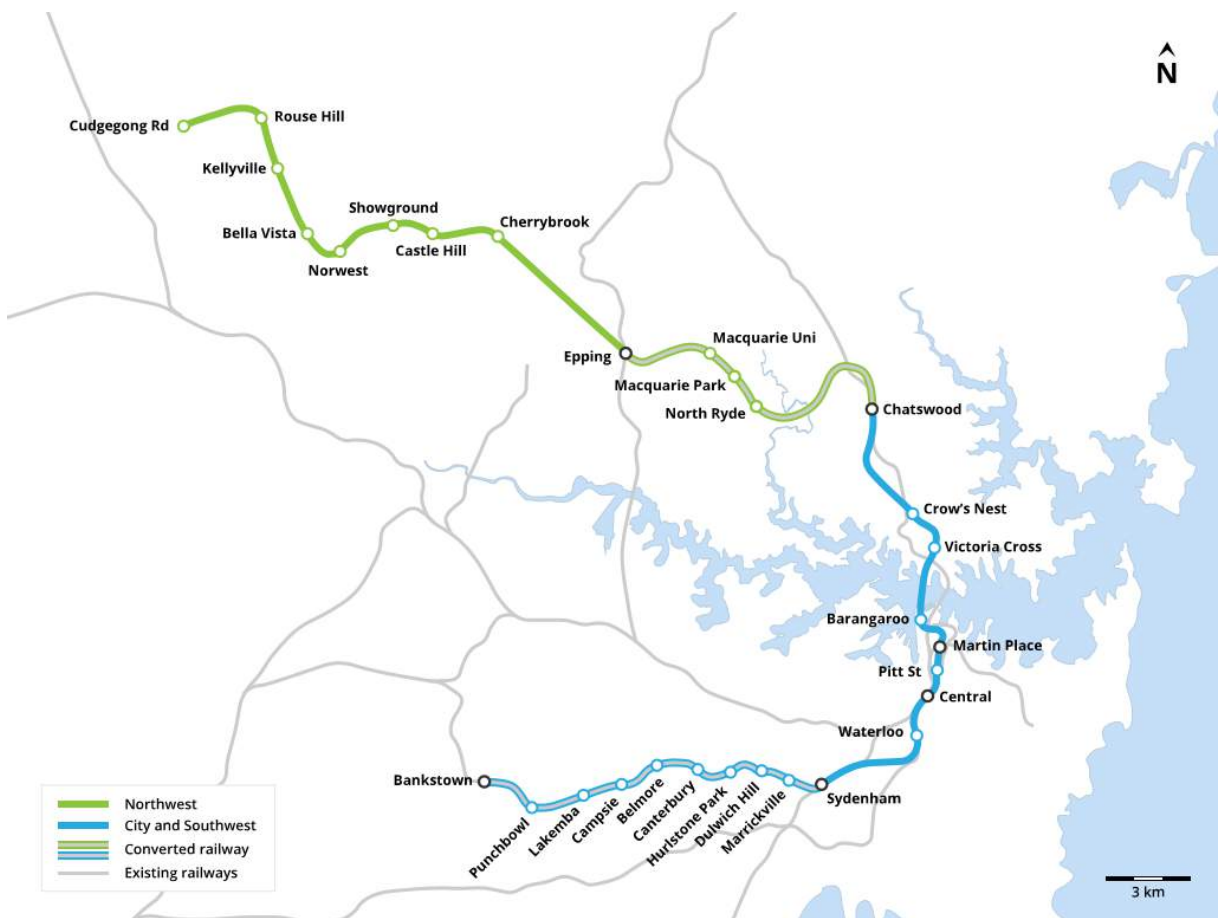
1.1. Project Overview

The Sydney Metro is Australia’s biggest public transport project. The metro project currently consists of:

- Stage one is the Sydney Metro Northwest, formerly the North West Rail Link, connecting the north western suburbs of Sydney, through to Chatswood.
- Stage two is the City & South West project linking Chatswood through to Sydenham.
- Stage three includes the upgrade of the existing rail line between Sydenham and Bankstown.

Figure 1 shows the proposed Sydney Metro project and stages.

FIGURE 1 SYDNEY METRO PROJECT



1.2. Purpose of this Plan

The purpose of this *Construction Traffic Management Plan (CTMP)*: CHATSWOOD is to detail the site operations for the demolition phase of the Sydney Metro City & South West project. This plan will set out the traffic management initiative that will be deployed to minimise disruption to , and ensure the safety of the wide range of stakeholders potentially affected by the works, including but not limited to: motorists, pedestrians; cyclists; public transport users, local residents and property owners; business owners; and workers/staff engaged on the Project.

2. Locality and existing conditions

The Chatswood dive site is bounded by the Pacific Highway, Mowbray Road, the Chatswood rail lines and Nelson Street, Chatswood and is shown in Figure 2 below. The existing parking restrictions at the site are detailed below and are included in Appendix 1.

FIGURE 2 CHATSWOOD DIVE SITE



2.1.1. Pacific Highway

The Pacific Highway is classified as a state road which is under the control of RMS. The highway commences at North Sydney and continues to the north of Sydney. The speed limit on the highway is 60km/hr. A median separates the two carriageways.

A bus stop exists on the southbound carriageway of the Pacific Highway, to the north of Mowbray Road. The bus stop services routes 143, 144, 200, 261, 536, 627 and 628.

“No Parking restrictions” currently exist along the highway frontage of the site. “No Stopping” restrictions are in place near the intersections of Mowbray Road, Nelson Street and Bryson Street.

AM Clearway (6-10AM) exists on the southbound carriageway with a PM clearway in operation on the northbound carriageway (3-7PM).

The existing footpaths are 1.5 to 1.6m wide between Bryson Street and Nelson Street, with a 2m verge between the footpath and road. Between Bryson Street and Mowbray Road the effective width of the footpath is 1.8m (property boundary to face of kerb is 3.7m with 1.9m allocated to street furniture including a bus shelter). There are no cycling facilities on this section of the Pacific Highway.

2.1.2. Mowbray Road

Mowbray Road is classified as a regional road. Regional roads typically fall under Council care with control of the road exercised between Council and RMS, with RMS agreement required for changes. Mowbray Road commences at Epping Road to the west and ceases east of Willoughby Road to the east. The speed limit on Mowbray Road is 50km/hr. A small amount of median exists at the Hampden Road intersection.

A bus stop exists on the westbound carriageway, near the intersection with the Pacific Highway.

A mail zone is located on the eastbound carriageway approximately 20m east of the highway intersection. From the mail zone to west of Mowbray House, “No Stopping” restrictions are in place. East of these No Stopping restrictions, No Parking restrictions exist. These No Parking restrictions end at the Mowbray Road rail bridge.

AM Clearway (6-10AM) exists on the eastbound carriageway only.

The existing footpath is 2m wide between the Pacific Highway and the Mowbray Road bridge. There is a 1.8m verge between the footpath and road. There are no cycling facilities on this section of the Mowbray Road.

2.1.3. Pacific Highway/ Mowbray Road intersection

On the southern approach of the intersection, there are two dedicated right turn lanes onto Mowbray Road to allow movements from south to east. There are three through northbound lanes. On the northern approach to the intersection, vehicles can only travel straight through or turn left into Mowbray Road to head east. The lane arrangements at this location are three through lanes with the kerbside lane (Lane 1) allowing both through and left turn movements.

On the western approach, Mowbray Road has two through lanes and a dedicated left turn lane onto the highway northbound. There is a right turn ban restriction for general traffic (buses excepted). On the eastern approach on Mowbray Road, there is a dedicated right turn bay onto the highway northbound, one dedicated through lane and a shared through and left lane.

There are signalised crossings on all legs of the intersection. There are no dedicated cycle facilities on the highway.

A red light/ speed camera is in place at the intersection.

2.1.4. Bryson Street

Bryson Street is a local road which connects to the Pacific Highway and services the current business of Nick Scali. NSW’s default speed limit of 50km/hr is applicable as there is no posted limit.

“No Stopping” is located on the southern side of Bryson Street, and No Parking 8.30AM-6PM Monday to Friday on the northern kerb. Bryson Street will cease to exist once the works commence.

2.1.5. Nelson Street

Nelson Street is a local road connecting Orchard Road to the Pacific Highway. Signage on the highway promotes Nelson Street as part of an elongated G turn to gain access to Mowbray Road west (due to the turn bans at the intersection of Pacific Highway and Mowbray Road). The default speed limit is 50km/hr.

At its intersection with the highway, movements are limited to left in/ left out under Give way control.

“No Stopping” restrictions exist at the intersection with the Pacific Highway and cover the driveways to the commercial premise on the northern side of Nelson Street and the Ausgrid site on the southern side. Parking is restricted on the northern side of Nelson Street between the hours of 8:30AM to 6PM Monday to Friday to the east of the commercial premises until the Nelson Street bridge.

Unrestricted parking is available along the southern side of Nelson Street between the highway and the bridge. There are approximately six spaces which are on road, whilst the remainder of the parking is within the indented parking bay outside of the Ausgrid premises.

The existing footpaths are 1.5m wide with a 2.5m verge between the Nelson Street bridge and the end of the indented parking bay. From the indented parking bay to Pacific Highway, the footpath maintains the 1.5m width but the verge widens to 3.4m. There are no cycling facilities on this section of Nelson Street.

2.2. Preliminary Works

Timing: From 1st April 2017 to Site Operations Commencement

2.2.1. Works and Operations

Prior to the commencement of demolition works within the site, Delta Group will need to disconnect/ isolate existing utilities and undertake HAZMAT identification and removal. Fencing installation will occur where required. These works will be undertaken within the site using existing driveways. Only light vehicles will be required for these works.

2.3. Site operations

Duration: 5 months

Timing: April 2017 to August 2017

2.3.1. Works Required

Prior to the commencement of demolition works within the site, Delta Group will install fencing around Mowbray House to ensure separation between the work site and this heritage item, refer to Security Management Plan.

Shade cloth will be provided on the existing fencing around the site. There are only small sections where new fencing will be required. Refer to Appendix 2 for details on current and proposed fencing arrangements.

The awning removal at the intersection of Mowbray Road and Pacific Highway, will be undertaken at night due to the proximity to the existing traffic signals. It is anticipated that awning removal works will take approximately 2 nights. The bus stop on the Pacific Highway will be temporarily relocated north to facilitate the lane closure required for the works. Scaffolding will be installed at this location post awning removal. The scaffolding will provide cover similar to the existing awning and will straddle the existing bus shelter on the southbound carriageway of the Pacific Highway, immediately north of Mowbray Road. Refer to Appendix 2 for typical details on the hoarding arrangements.

Demolition of the existing buildings within the site, will occur during normal construction hours of 0700-1800 Monday to Friday and 0800-1300 on Saturday.

2.3.2. Operating Conditions

The main access/ egress is proposed on Mowbray Road at the existing driveway into Ausgrid, located at the eastern end of the site. This access/ egress will be used for the majority of heavy vehicle movements. Semi-trailer access will be via the existing driveway located immediately east of the Pacific Highway intersection. Any semi-trailer deliveries will be planned outside of the AM and PM peak periods. Other existing driveways on Mowbray Road will be used for light vehicle movements. Refer to Appendix 1 and Appendix 2 for detail on existing parking restrictions and proposed site operations for the site.

2.3.2.1. Scaffolding and hoarding placement

Scaffolding will generally be within the site at the project boundary, however, Class B hoarding will be installed around the intersection of Pacific Highway and Mowbray Road. A typical scaffolding arrangement is included within Appendix 2. The hoarding will have lighting to provide a safe walkway for pedestrians and will have available a minimum of 2m clearance as a pedestrian thoroughfare.

2.3.3. Impact on Traffic Flows

The operation of the site will have minimal impact on existing traffic conditions as it is proposed to use existing driveways. Post the arrival of machinery on site, traffic generation at this site will typically be in the order of 50 truck movements per month, with an average of 2-3 per day. There will be minimal truck numbers during the peak periods (AM and PM).

2.3.4. Impact on Public Transport

There will be no impact on public transport for the bulk of the works. There may be a minor disruption to bus operations during the lane closures on the Pacific Highway. This relocation has been discussed and agreed with Sydney Buses – refer to Appendix 6.

2.3.5. Impact on Pedestrians and Cyclists

Other than the works on the awnings identified above, the existing facilities are retained during the works.

2.3.6. Impacts on commercial and residential access

There is no impact on existing commercial properties as all properties on site will be vacant prior to the commencement of works. Nelson Street properties access – both commercial and residential, will be retained.

2.3.7. Staff parking and transportation to site

During the demolition works, it is anticipated that there will be 10-12 personnel on site. There will be 3-4 allocated car parking spaces, typically for managerial/ engineering staff. The workforce for the actual demolition will be encouraged to arrive by site using public transport, with most likely to use the Chatswood Interchange, a 10 minute walk from site via Frank Channon shared path.

2.3.8. Cumulative impacts

There are no work sites adjacent or within the immediate area which would impact on the current traffic and transport network.

2.3.9. Special Events

In reviewing Willoughby Council's website for special events near the site, there are no special events listed, which would be impacted by the works.

2.3.10. Traffic Control Plan (TCP) / Road Occupancy License (ROL) Identified Works

Works that have been identified as requiring a Traffic Control Plan are detailed below. Appendix 3 provides details on expected durations, timing for implementation, the works being undertaken and the expected traffic impacts.

1. Removal of awning on the corner of Pacific Highway and Mowbray Road
2. Installation of fencing may require pedestrian management

2.3.11. Willoughby Council required approvals

Works that have been identified as requiring Council approval include:

1. Hoarding and scaffolding installation
2. Modifications of existing driveways on Mowbray Road
3. Oversize/ over mass vehicles

3. Fleet Management

Trucks to be used on the project will be compliant with NSW legislation and standards including the Heavy Vehicle National Legislation. A combination of truck types will be used during the demolition works. These trucks may be Truck and Dogs, bin trucks and semi-trailers. Table 1. provides an estimation of materials to be removed over the life of the works, whilst Table 2 provides a comparison of the EIS and DELTA traffic movements at the site.

TABLE 1 MATERIAL (TONNES) ESTIMATES

BRICK	CONCRETE	STEEL	DEMOLITION WASTE
2360	2920	60	520

TABLE 2 AVERAGE VEHICLE MOVEMENTS PER HOUR

TIME	EIS LIGHT VEHICLES	DELTA LIGHT VEHICLES	EIS HEAVY VEHICLES	DELTA HEAVY VEHICLES
0700-1000	2	2	6	2
1000-1200	10	1	12	1
1200-1400	10	1	12	1
1400-1600	6	1	8	1
1600-1800	2	1	6	1
1800-0700	0-9	0		0

3.1. Logistics Routes

Generally, the haulage routes will be via arterial roads/ freeway or Tollway. Where possible the routes have taken into account the requirements of the Environmental Impact Statement (EIS). Where the routes differ from that in the EIS, the routes chosen are based on the availability of the arterial road system and these are detailed in this CTMP. It should be noted that the current access/ egress arrangements do not allow for a right turn out of the Mowbray Road driveways as this was intended to occur with the installation of traffic signals at the driveway location. The movement to the northern disposal site, therefore is via Penshurst Street, due to the left only movement out of site.

Spoil will be removed from site using truck and dogs and taken to authorised disposal sites around Sydney. Refer to Appendix 4 for details on the proposed routes.

3.2. Permits/ Over Dimensional Vehicles

Permit issue for vehicles greater than 4.5 tonnes is through the National Heavy Vehicle Regulator (NHVR). This applies to particular special purpose vehicles (SPV) such as mobile cranes and other oversize/over mass vehicles (OSOM). At present, RMS is currently undertaking this permit issue.

For over dimensional vehicles, generally vehicles that are greater than 25m length or 3.5m width require a pilot(s). Extremely long or wide vehicles will require an escort (fee payable). Permits will be applied for by the transport operator.

There will be approximately 6 oversize/ over mass deliveries in total for the site, which will need to use the Pacific Highway as entry into the site. These vehicles will be limited to night time deliveries only.

3.3. Environmental Considerations

All vehicles used on the Project will comply with national and state regulations for exhaust emissions and noise and:

- Switch off their engines when idle, particularly in residential areas
- Avoid the use of their engine brakes in residential areas at night
- Have their load covered prior to leaving site to reduce dust
- Be fitted with broadband reverse beepers.

Refer to our Construction Environmental Management Plan (CEMP) for further details

3.4. Drivers and Operators

Operator selection will be based on safety performance criteria. Operators and drivers will be required to have general construction industry induction cards and will be required to attend ongoing general project and site specific inductions.

All operators will be comprehensively trained with regard to community expectations and impacts from haulage operations. The induction will have a particular focus on operator behaviour. Operator competency and standards of behaviour will be continually assessed and discipline procedures will be put in place to maintain compliance.

Queuing of trucks in local streets around the demolition sites will not occur as there will be sufficient designated area within site for trucks to wait. It is noted that there are minimal truck movements required during the demolition phase. Trucks will be provided post the site start times.

The primary access point into the Chatswood site will be via the existing Ausgrid driveway on Mowbray Road at the eastern end of the site.

4. Other Considerations

4.1. Road Safety Audits

Road safety audits will be undertaken during the development of the CTMP.

4.2. Police and Emergency Services

The Police and relevant Emergency Services will be informed in a timely manner of relevant activities proposed within this CTMP. The initial communication to these stakeholders will be via the Traffic and Transport Liaison Group (TTLG). Regular updates will be provided to emergency services, including changes to road network configurations, changes to road conditions and worksite access locations, through emails and face to face discussions.

4.3. Communications and the community

TfNSW will be responsible for the dissemination of information to the community including affected residents, relevant Councils, businesses and the public. Refer to Sydney Metro Community Consultation Strategy – Early Works.

Any enquiries, complaints and/ or compliments should be directed to the TfNSW's Sydney Metro Project Information line - **1800 171 386** or via mail to: sydneymetro@transport.nsw.gov.au or Sydney Metro City & Southwest, PO Box K659, Haymarket, NSW 1240 or [Sydney Metro City & Southwest Website](#).

4.3.1. Travelling public

Public transport interruptions will be communicated via on site signage. The motoring public will be forewarned of any changes, including road closures, road changes and lane closures well in advance using appropriate signage, including variable message signs (VMS).

4.3.2. Stakeholders consulted

Table 3 identifies the stakeholders consulted during the development of this CTMP:

TABLE 3 STAKEHOLDER CONSULTATION

STAKEHOLDER	DATE	CONSULTATION	MAIN CONTACT PEOPLE
Willoughby City Council	1 st Feb 2017	Meeting	Neil Strickland/ David Sung
Willoughby City Council	16 th Feb 2017	Meeting	Neil Strickland
Willoughby City Council	16 th Mar 2017	Issue of CTMP	Neil Strickland
Roads and Maritime Service	10 th March 2017	Issue of CTMP	Peta Smith/ Anthony McMahon
Sydney Coordination Office	10 th March 2017	Issue of CTMP	Giovanny Ramirez
TCG	21 st Feb 2017 through to 16 th May 2017	All members	SCO/ TfNSW/ WC/ RMS
Willoughby City Council	8 May 2017	Reissue of CTMP	Neil Strickland
Sydney Coordination Office	8 May 2017	Reissue of CTMP	Giovanny Ramirez
Roads and Maritime Service	8 May 2017	Reissue of CTMP	Peta Smith
Willoughby Council	18 May 2017	Reissue of CTMP	Gordon Farrelly

STAKEHOLDER	DATE	CONSULTATION	MAIN CONTACT PEOPLE
Sydney Coordination Office	18 May 2017	Reissue of CTMP	Giovanny Ramirez
Roads and Maritime Service	18 May 2017	Reissue of CTMP	Peta Smith

4.4. Inspections and monitoring

The site will be monitored by the site supervisor. Any changes as noted in this CTMP, to signs/ lines that impact on the public will be monitored daily during site operating hours.

Traffic control used for pedestrian management, lane closures etc will need to provide records of the traffic control implemented. Any changes required to the traffic control set up will be authorised by a RMS “Prepare a Work Zone Traffic Management Plan” or equivalent.

4.5. Environmental Maintenance

All works associated with the temporary traffic arrangements proposed within this plan will conform to the Construction EMP.

4.6. On Site Contacts

TABLE 4 ON SITE CONTACTS

NAME	POSITION	CONTACT #
Ben Shum	Project Director	0423 7906 946
Elliot Nuberg	Project Manager	0437 833 005
Joe Gardinia	Site Supervisor	0411 706 553
Kevan Zulu	Safety Manager	0411 092 898
Matthew Stephenson	Environment and Sustainability Manager	0418 284 226

4.7. References

The following documents were used in the development of this CTMP:

- Construction Traffic Management Framework – TfNSW, v.4.4 provided by TfNSW 4th March 2017
- Principal’s General Specification G10 Traffic and Transport Management, v2.0
- Roads and Maritime Service Traffic Control at Worksites Manual v 4.0
- Relevant AustRoads Guides and RMS Supplements
- Sydney Metro Principal Contractor Health and Safety Standard

Appendix 1 Existing parking restrictions

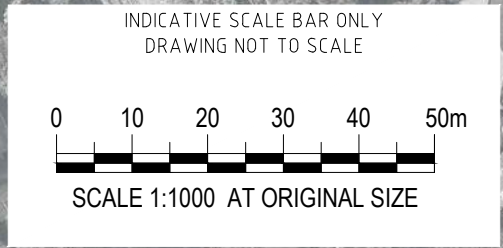
DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0001-01	Existing Parking Restrictions

- UNRESTRICTED PARKING
- NO PARKING
- NO STOPPING
- BUS ZONE
- MAIL ZONE
- CLEARWAY

NORTHBOUND CLEARWAY
15:00 - 19:00 MON - FRI

SOUTHBOUND CLEARWAY
06:00 - 10:00 MON - FRI

EASTBOUND CLEARWAY
06:00 - 10:00 MON - FRI



REV	BY	DATE	DESCRIPTION	APPD.
B-00	LS	04/03/2017	TINSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL
A-00	SS	2/6/2017	INITIAL DRAFT	SL

DRAWN BY: LS
 DRW CHECK: SL
 APPROVED: SL
 IND REVIEW: N/A

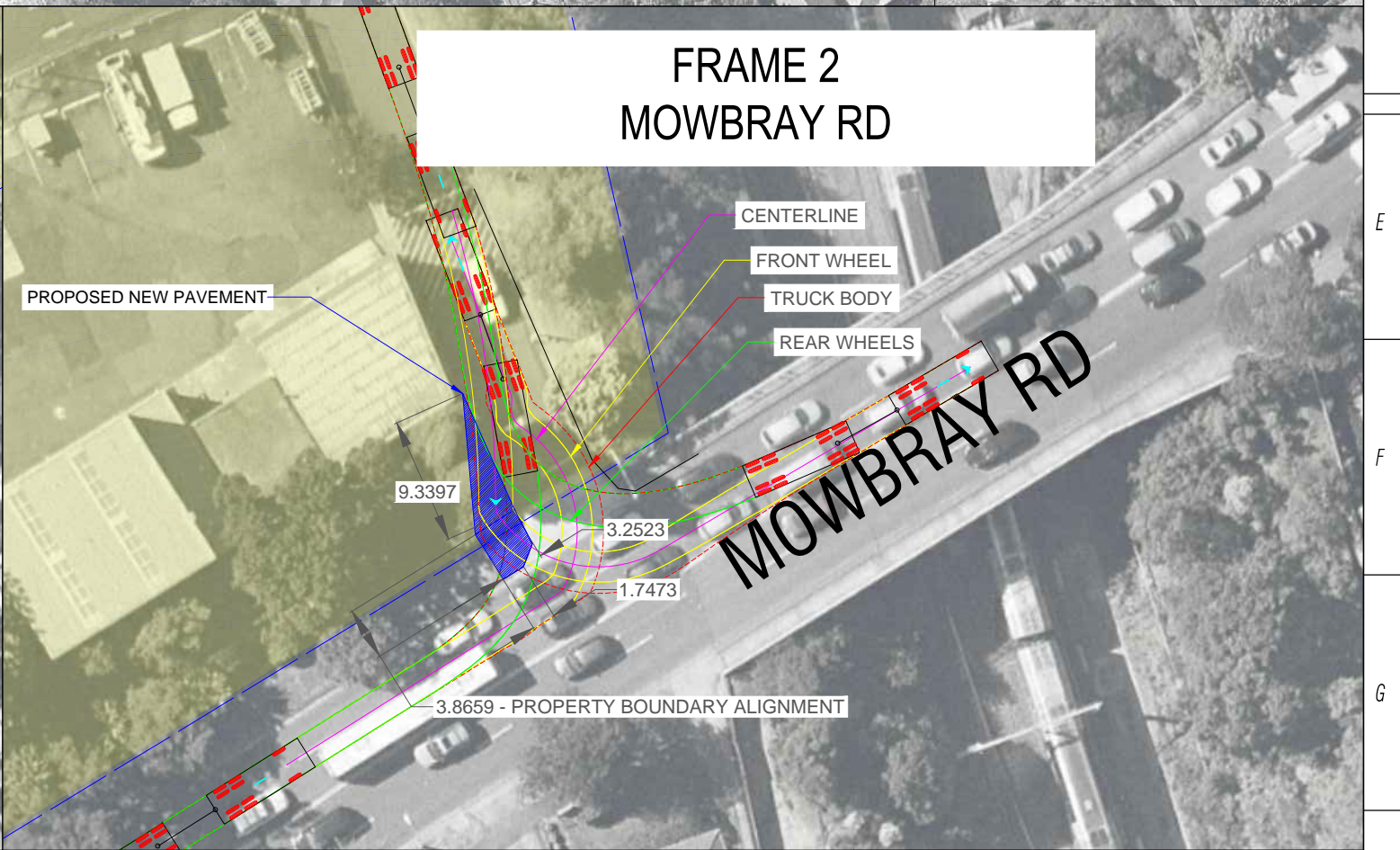
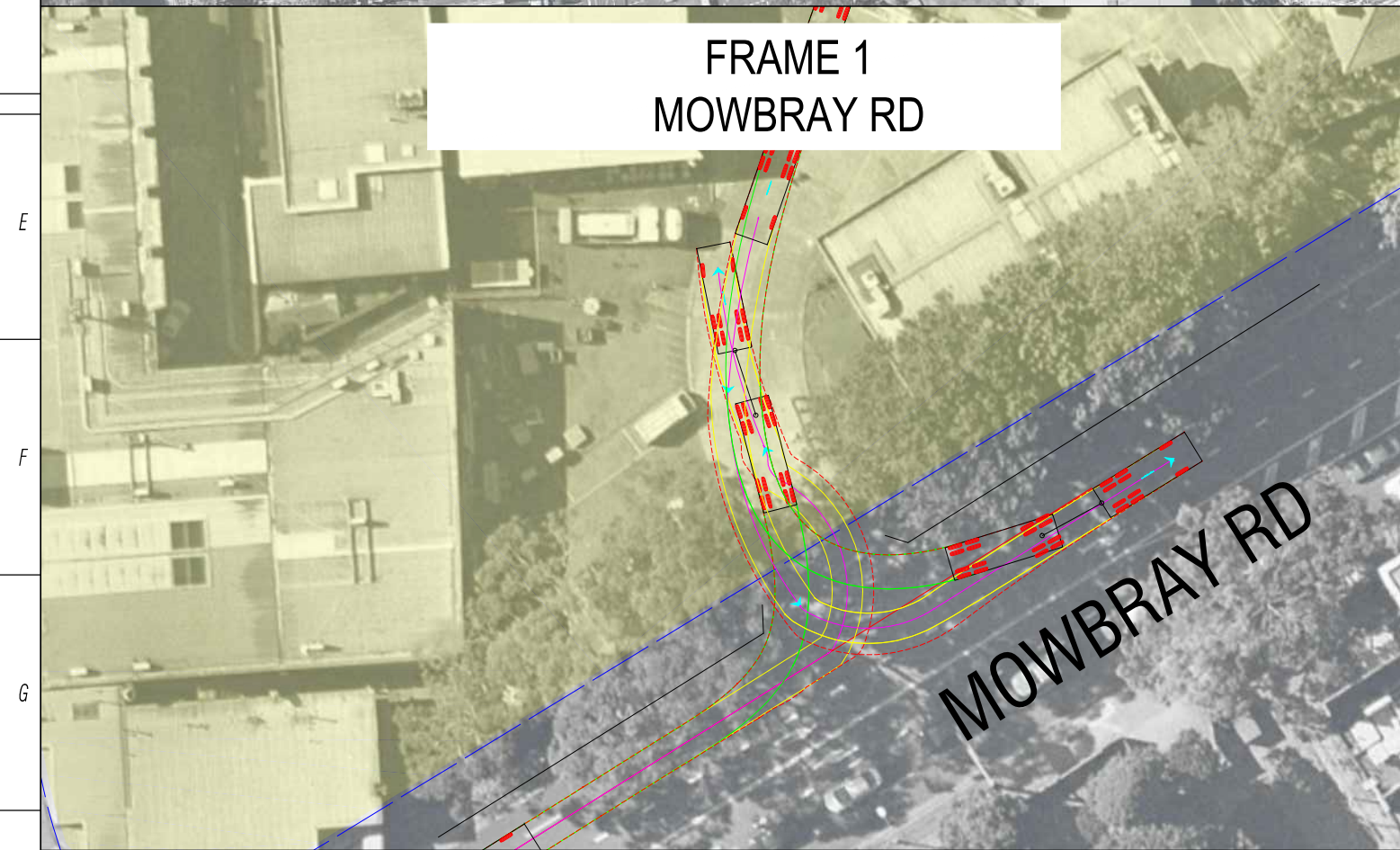
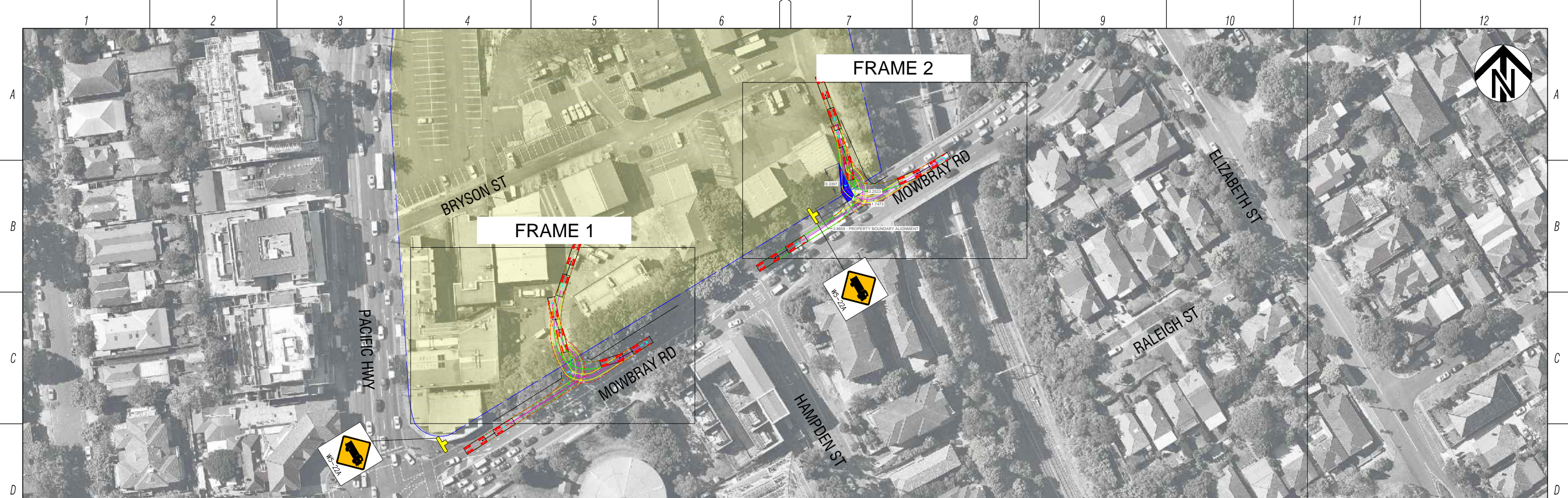


SYDNEY METRO
 DELTA GROUP AUSTRALIA WIDE
 EXISTING PARKING RESTRICTIONS
 CHATSWOOD

DRAWING No: DD-SYDM-DWG-0001-01
 SHEET 1 OF 1
 REVISION B-00

Appendix 2 Proposed site operations including hoarding arrangements

DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0001-02	Proposed Access/ egress arrangements
DD-SYDM-DWG-0001-03	Site Establishment layout including hoarding arrangements
DD-SYDM-DWG-0001-04	Semi trailer swept path into site
DD-SYDM-TYP-0000-01	Typical Hoarding arrangement over footpath



REV	BY	DATE	DESCRIPTION	APPD.
D-00	LS	02/05/2017	AMENDED SITE ACCESS	SL
C-00	LS	20/04/2017	AMENDED SWEEP PATHS	SL
B-00	LS	04/03/2017	TNSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL
A-00	SS	06/02/2017	INITIAL DRAFT	SL

DRAWN BY: LS
 DRW CHECK: SL
 APPROVED: SL
 IND REVIEW: N/A

PROJECT CLIENT

SYDNEY METRO
 DELTA GROUP AUSTRALIA WIDE
 PROPOSED SITE ACCESS/EGRESS
 CHATSWOOD

DRAWING No:	DD-SYDM-DWG-0001-02	
SHEET	1	OF 1
REVISION	E-00	

Chatswood – Site Establishment

Establish Site office and amenities at the existing building 339 Mowbray Road.

Establish A-Class and B-Class Hoardings prior to the demolition works.

Locate the Sydney Water sewer main and the main sewer vent pipe and erect A-Class hoarding to protect them.

Establish A-Class, painted plywood hoarding to the perimeter of Mowbray House with access gates.

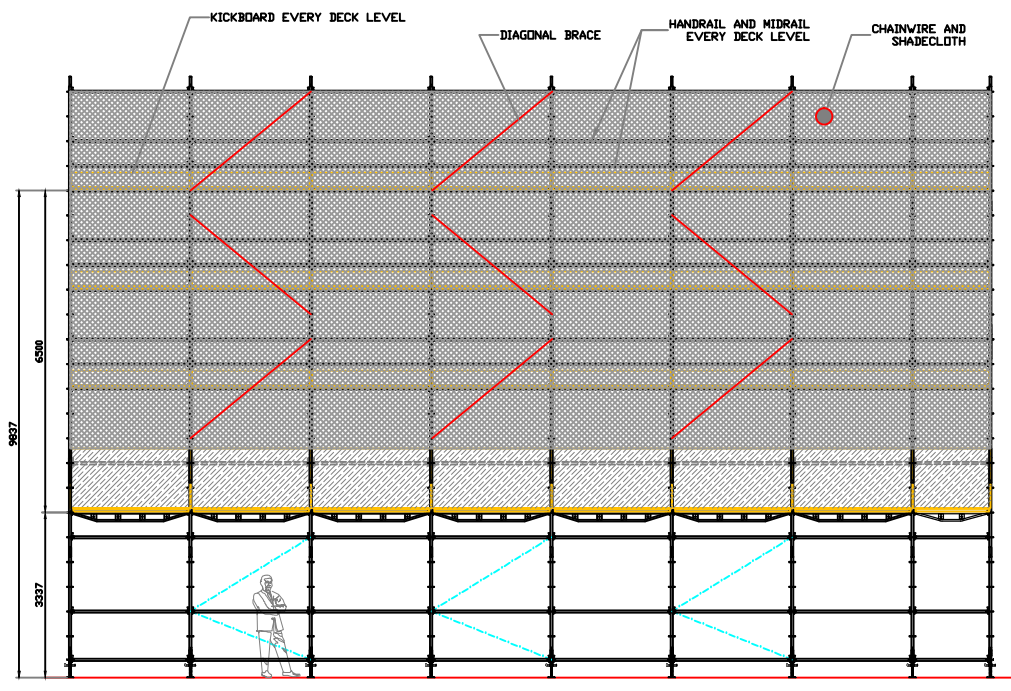
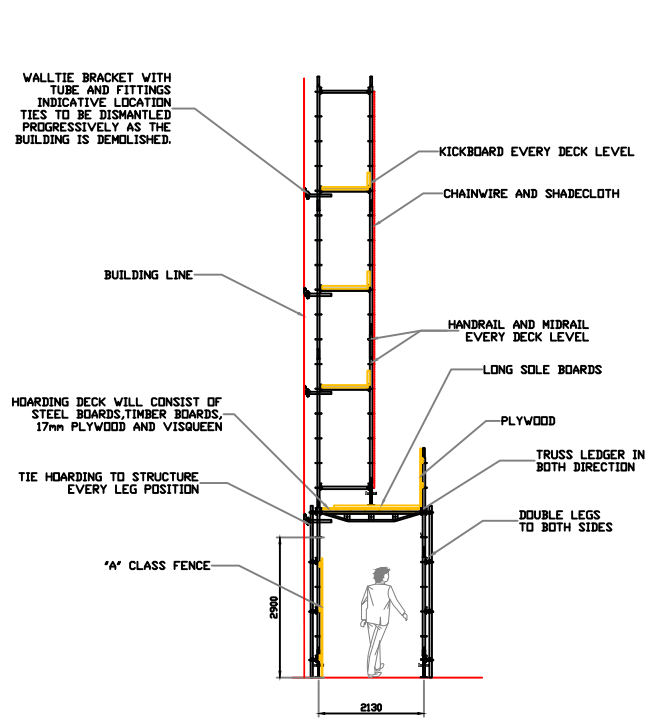
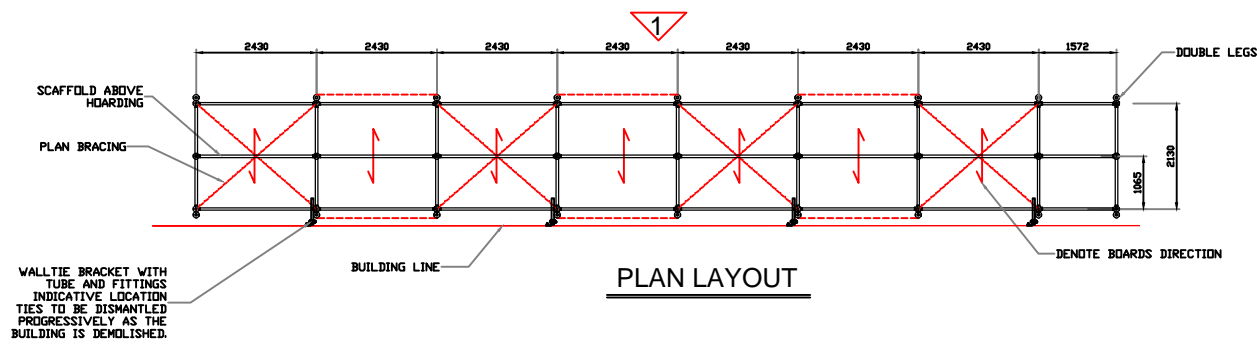
Establish B-Class hoarding as per diagram after the removal of awning. (3 weeks to demolish shops then install fence).

Setup silt fences/sand bags/geofab around site perimeter as required.





DD-SYDM-DWG-0001-04



GENERAL NOTES

THIS SCAFFOLD DESIGN RELATES SPECIFICALLY TO AT-RIG RING LOCK SYSTEM COMPONENTRY SUPPLIED BY DB GLOBAL GROUP.

SCAFFOLD BOARD LOCATIONS ARE SHOWN FOR ILLUSTRATION PURPOSES ONLY

ALL DIMENSIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO ACTUAL SITE CONDITIONS DO NOT OBTAIN DIMENSIONS BY SCALING FROM THE DRAWINGS

IT IS THE HIRER'S RESPONSIBILITY TO ENSURE THAT THE SCAFFOLD IS NOT ALTERED (INCLUDING THE REMOVAL OF TIES) WITHOUT PERMISSION FROM DB GLOBAL GROUP.

A) SCAFFOLD TIES
ALL TIES ON THE SCAFFOLDING TO SATISFY THE REQUIREMENTS OF BS11874 (ALL PARTS). ALL BRICKWORK, MASONRY, CONCRETE SLAB EDGES OR ANY OTHER SURFACE / STRUCTURE USED AS SUPPORT FOR TIES, MUST SUSTAIN A MINIMUM OF 6.0kn PULL OUT OR PUSH IN LOAD.
ALL BRICKWORK, MASONRY, CONCRETE SLAB EDGES OR ANY OTHER SURFACE / STRUCTURE USED AS SUPPORT FOR TIES, MUST SUSTAIN A MINIMUM OF 6.0kn PULL OUT OR PUSH IN LOAD.

MAXIMUM VERTICAL SPACING OF TIES TO BE 4.0M.
MAXIMUM OF 2.0M OF CHAMFER & SHADECLOTH IS ALLOWED ABOVE THE LAST ROW OF TIES.

WHERE CHEMICAL OR MECHANICAL MASONRY ANCHORS ARE USED IT IS RECOMMENDED

B) ALL ANCHORS USED TO SECURE CHAINWIRE BRACE TO BE PROOF LOADED IN TENSION TO THE MANUFACTURERS RECOMMENDED WORKING LOAD.

C) 10% OF ALL ANCHORS (20 RANDOM) USED TO SECURE TIES AND ACTING IN TENSION ONLY TO BE PROOF LOADED AS IN PARAGRAPH (C) ABOVE.

D) SCAFFOLD BRACING
D) BRACING TO THE OUTSIDE FACE OF THE SCAFFOLD SHOULD BE INSTALLED TO AT LEAST ONE BAY OF EVERY THREE.

E) BRACING SHOULD BE INSTALLED TO ALL END BAYS.

F) SCAFFOLD LOADING
F) THE SCAFFOLDING IS RATED FOR A HEAVY DUTY LOAD OF 675 KG PER BAY WITH A MAXIMUM BAY LENGTH OF 3.00M. A MAXIMUM OF 2 WORKING DECKS (DATED HEAVY DUTY) AT ANY ONE TIME IS ALLOWED UNLESS SPECIFIC DESIGN IS PROVIDED AND STATED ON THE DRAWING. REFER TO BS11874.1 FOR DUTY LOADINGS

G) MAXIMUM VERTICAL LIFT HEIGHT OF 2.0M

H) MAXIMUM NUMBER OF DECKS AND SCAFFOLD HEIGHT IS SPECIFIED ON THE DRAWING.

I) SOLEBOARD / FOUNDATION
I) SOLEBOARDS TO BE A MINIMUM OF 225mm x 300mm x 30mm ASSUMING SOIL BEARING CAPACITY OF 100kPa. WHERE PRACTICAL, THE USE OF CONTINUOUS 225mm x 180mm x 30mm SOLEBOARDS TO BE USED.

J) THE HIRER SHALL ENSURE THAT ALL FOUNDATIONS FOR THE SCAFFOLD OR ITS RELEVANT SUPPORT SYSTEM ARE ADEQUATE TO SUPPORT THE IMPOSED LOADS DUE TO THE SCAFFOLD'S DEAD LOAD AS ERECTED AND THE ASSOCIATED LIVE LOADS.

FOR CONSTRUCTION

REV No.	REVISION NOTES	SIGN	DATE
A	C/W DELETED, REDUCE THE OF HOARDING	DJ	14/07/2016



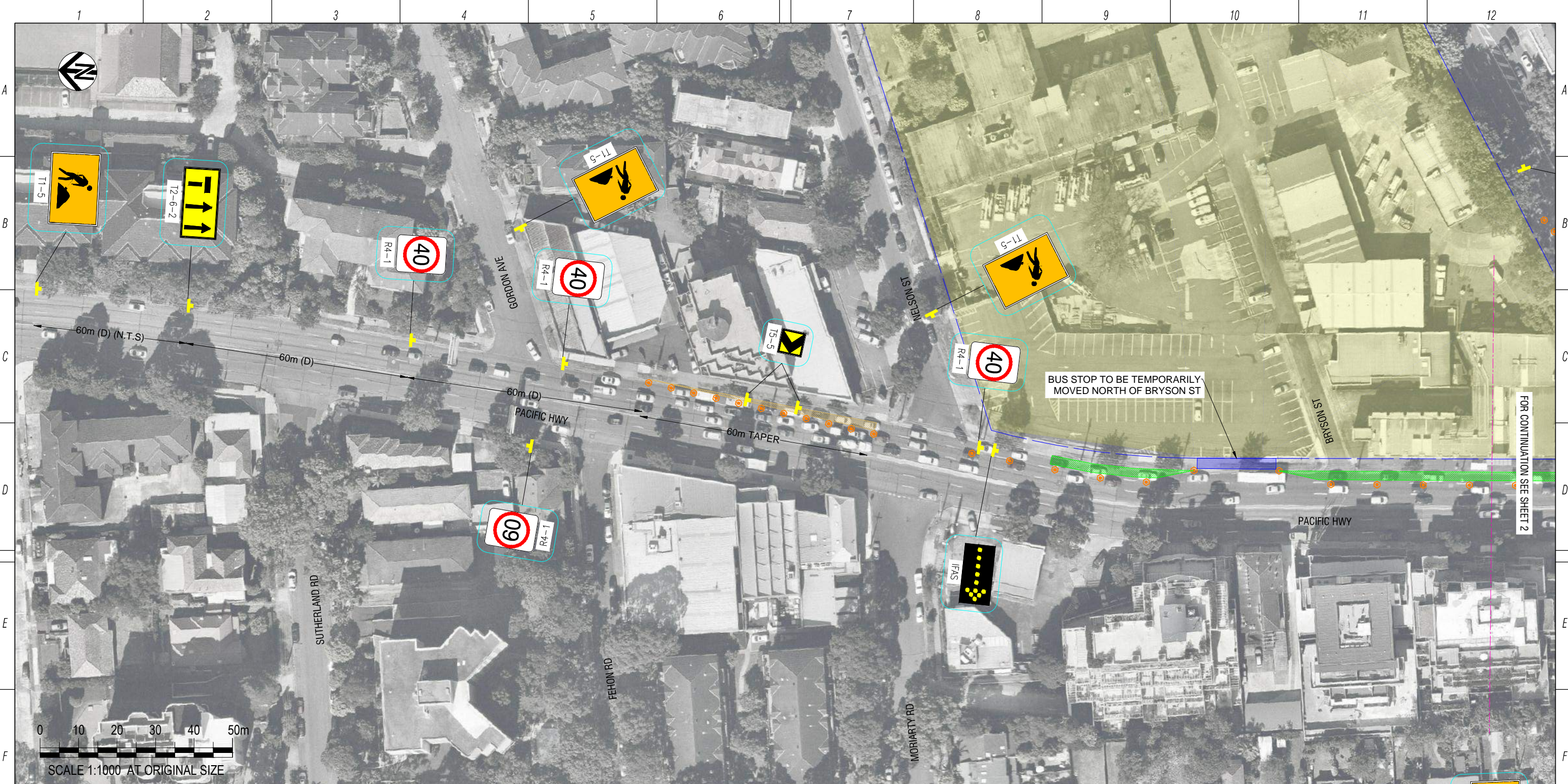
Customer: DELTA DEMOLITION GROUP

Project: TYPICAL HOARDING DETAIL

Paper Size: A1	Scale: 1:50	Drawn by: DJ-C
Date: 17/05/2016	Checked by:	
DD-SYDM-TYP-0000-01	Rev No. A	Sheet no. 1 of 1

Appendix 3 Traffic control plans and implementation

TCP #	LOCATION	FROM	TO	TIMING	TRAFFIC CONTROL	WORKS	IMPACTS
0001-06	Pacific Highway	North of Nelson Street	Mowbray Road	Nights	Kerbside lane closure	<ul style="list-style-type: none"> Awning removal Scaffold installation Plant delivery/ removal 	<p>Minimal impacts to traffic as taken outside of peak periods</p> <p>No impact to parking as this area is No Parking/ No Stopping</p> <p>Bus stop to be relocated during implementation</p>
0001-08	Pacific Highway & Mowbray Road	Length of hoarding installation	Length of hoarding installation	Night	Pedestrian management	<ul style="list-style-type: none"> Hoarding/ Scaffold installation 	<p>Minimal impacts to traffic as taken outside of peak periods</p> <p>No impact to parking as this area is No Parking/ No Stopping</p> <p>Bus stop to be relocated during implementation</p>
0001-11	Mowbray Road	West of Pacific Highway	Railway bridge	Night	Slow lane closure	<ul style="list-style-type: none"> Driveway modifications Signage installation Hoarding/ Scaffolding installation 	<p>Minimal impacts to traffic as taken outside of peak periods</p> <p>No impact to parking as this area is No Parking/ No Stopping</p>
0000-02	Site access/ egress			All times	Pedestrian management	<ul style="list-style-type: none"> Site access/ egress 	Minimal impact to traffic and pedestrian movements



FOR CONTINUATION SEE SHEET 2

- GENERAL NOTES**
- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TCP NO.s 57 & 100.
 - ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUCPLICATED IN ACCORDANCE WITH TCAWS V4 2010, SECTION 3.2.4
 - THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TCP.
 - ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TCP MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
 - THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V4 2010 EDITION AN A.S. 1742.3
 - SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, SECTION 3.2.8
 - BOLLARDS OR TRAFFIC CONES ARE TO BE INSTALLED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, TABLE 5.1
 - TAPER LENGTHS ARE TO BE IN ACCORDANCE WITH THE TCAWS MANUAL 2010, TABLE 5.2
 - DIMENSION 'D' IS DETERMINED IN ACCORDANCE WITH THE TCAWS MANUAL 2010, SECTION 2.11
 - REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

REVIEWED BY
NAME: S. SCHARF
ORANGE CARD: 0021232283

APPROVED BY
NAME: S. LEWIS
ORANGE CARD: 0022818927

REV	BY	DATE	DESCRIPTION	APPD.
C-01	LS	08/05/2017	INCLUDE RMS TICKET NUMBERS	SL
C-00	LS	08/05/2017	INCLUDE RMS TICKET NUMBERS	SL
B-01	LS	16/03/2017	INCLUDE RMS TICKET NUMBERS	SL
B-00	LS	04/03/2017	TINSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL
A-00	SS	11/02/2017	INITIAL DRAFT	SL

DRAWN BY: LS

DRW CHECK: SS

APPROVED: SL

IND REVIEW: N/A

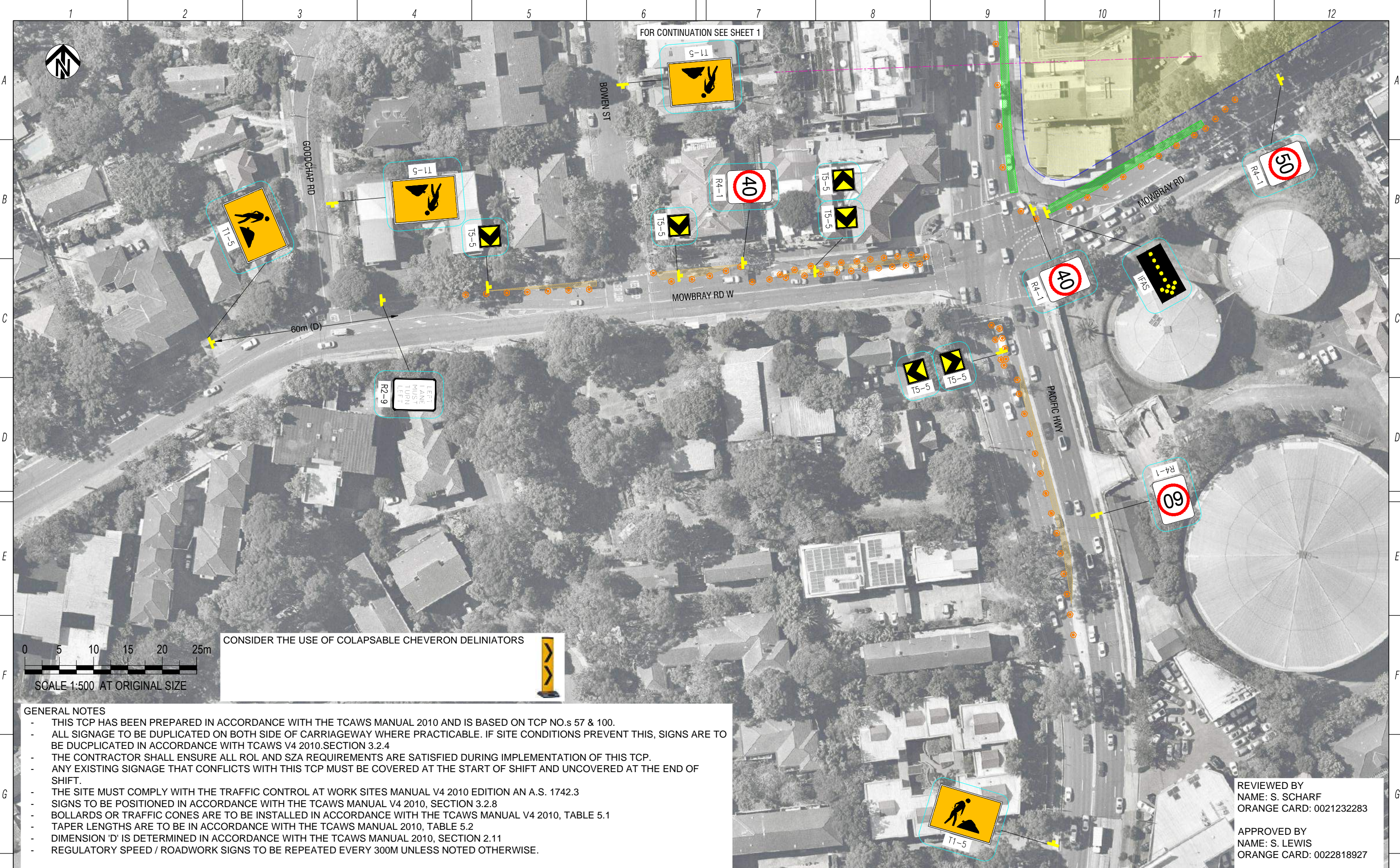
PROJECT

CLIENT

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
SLOW LANE CLOSURE
PACIFIC HIGHWAY - CHATSWOOD

DRAWING No:	DD-SYDM-DWG-0001-06	
SHEET	1	OF 2
REVISION	C-01	



CONSIDER THE USE OF COLAPSABLE CHEVRON DELINIATORS

- GENERAL NOTES**
- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TCP NO.s 57 & 100.
 - ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUCPLICATED IN ACCORDANCE WITH TCAWS V4 2010,SECTION 3.2.4
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 - REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

REVIEWED BY
NAME: S. SCHARF
ORANGE CARD: 0021232283

APPROVED BY
NAME: S. LEWIS
ORANGE CARD: 0022818927

REV	BY	DATE	DESCRIPTION	APPD.
C-01	LS	08/05/2017	INCLUDE RMS TICKET NUMBERS	SL
C-00	LS	08/05/2017	INCLUDE RMS TICKET NUMBERS	SL
B-01	LS	16/03/2017	INCLUDE RMS TICKET NUMBERS	SL
B-00	LS	04/03/2017	TNSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL
A-00	SS	11/02/2017	INITIAL DRAFT	SL

DRAWN BY: LS
DRW CHECK: SS
APPROVED: SL
IND REVIEW: N/A

PROJECT: **DELTA GROUP AUSTRALIA WIDE**

CLIENT: **NSW GOVERNMENT Transport for NSW**

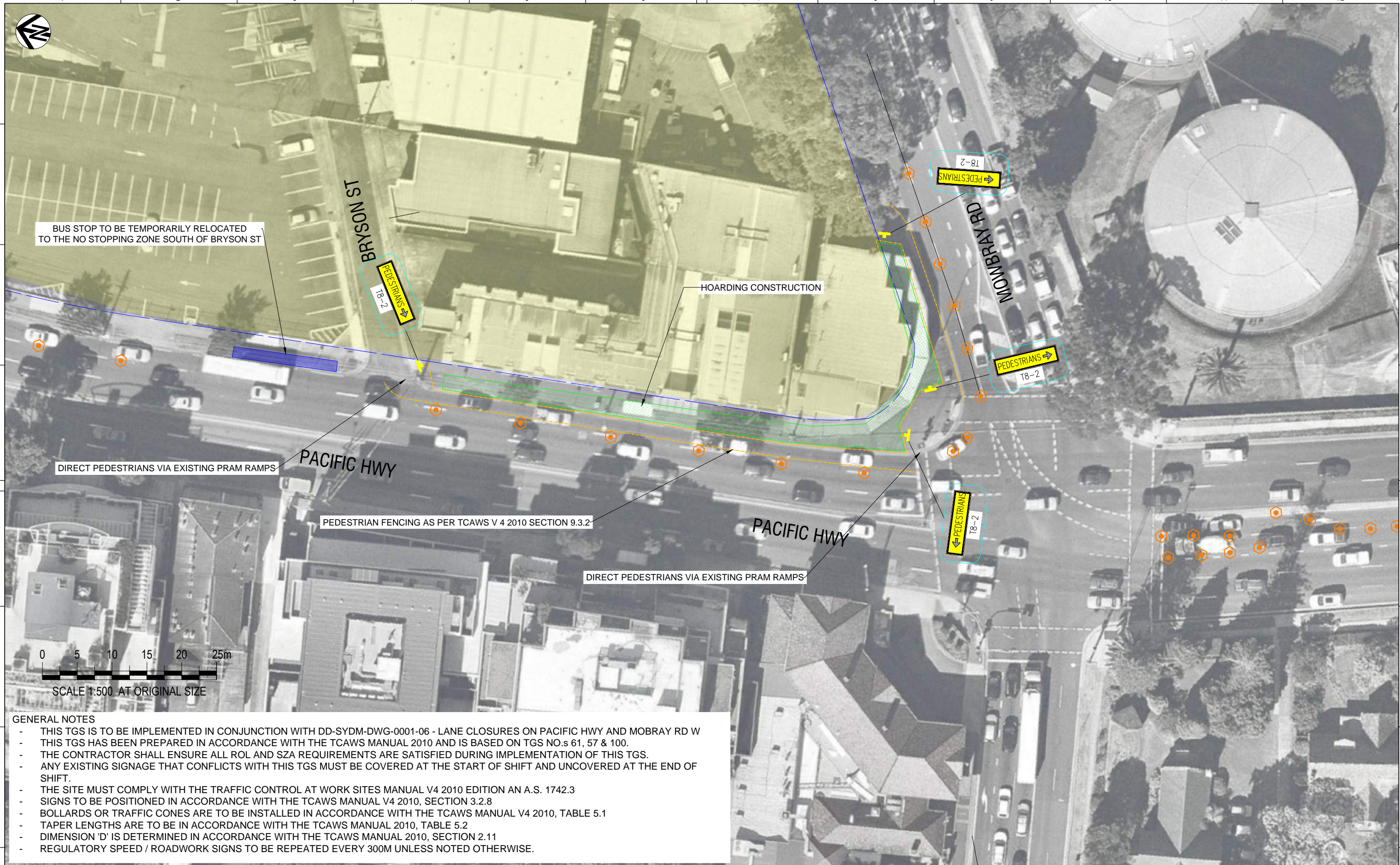
SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
SLOW LANE CLOSURE
PACIFIC HIGHWAY - CHATSWOOD

DRAWING No: DD-SYDM-DWG-0001-06

SHEET 2 OF 2

REVISION C-01



GENERAL NOTES

- THIS TGS IS TO BE IMPLEMENTED IN CONJUNCTION WITH DD-SYDM-DWG-0001-06 - LANE CLOSURES ON PACIFIC HWY AND MOBRAY RD W
- THIS TGS HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TGS NO.s 61, 57 & 100.
- THE CONTRACTOR SHALL ENSURE ALL ROL AND SZA REQUIREMENTS ARE SATISFIED DURING IMPLEMENTATION OF THIS TGS.
- ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TGS MUST BE COVERED AT THE START OF SHIFT AND UNCOVERED AT THE END OF SHIFT.
- THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V4 2010 EDITION AN A.S. 1742.3
- SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V4 2010, SECTION 3.2.8
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- REGULATORY SPEED / ROADWORK SIGNS TO BE REPEATED EVERY 300M UNLESS NOTED OTHERWISE.

REV	BY	DATE	DESCRIPTION	APPD.
B-00	LS	04/03/2017	TINSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	UPDATE TRAFFIC CONTROLLER LOCATION	SL
A-00	SS	11/02/2017	INITIAL DRAFT	SL

DRAWN BY: LS
 DRW CHECK: SL
 APPROVED: SL
 IND REVIEW: N/A

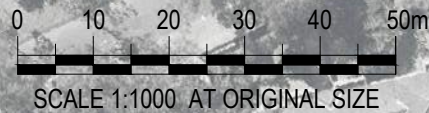
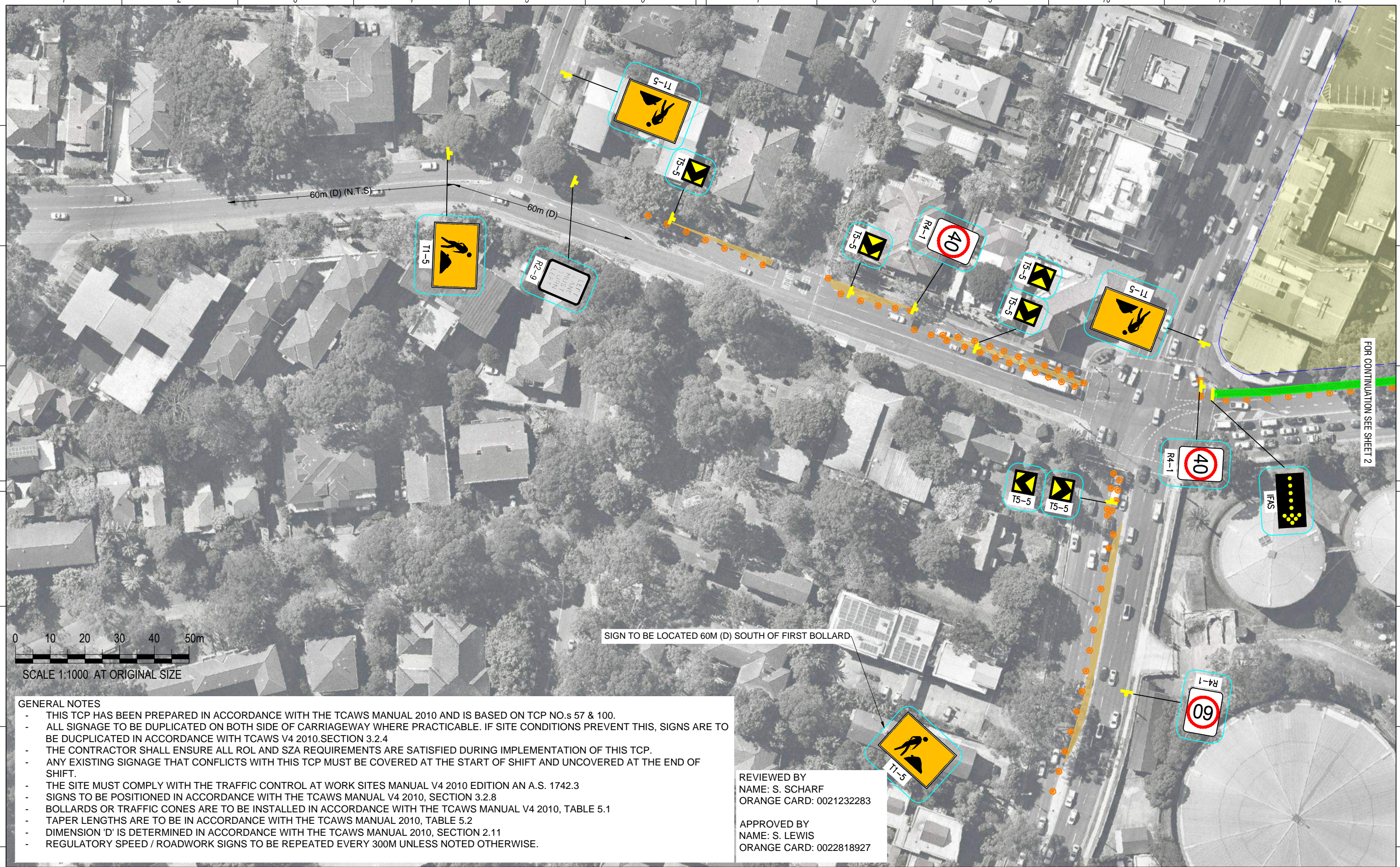
PROJECT: **DELTA GROUP AUSTRALIA WIDE**

CLIENT: **NSW GOVERNMENT Transport for NSW**

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
HOARDING CONSTRUCTION
CHATSWOOD

DRAWING No:	DD-SYDM-DWG-0001-08	
SHEET	1	OF 1
REVISION	B-00	



GENERAL NOTES

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- ALL SIGNAGE TO BE DUPLICATED ON BOTH SIDE OF CARRIAGEWAY WHERE PRACTICABLE. IF SITE CONDITIONS PREVENT THIS, SIGNS ARE TO BE DUPLICATED IN ACCORDANCE WITH TCAWS V4 2010, SECTION 3.2.4
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REVIEWED BY
NAME: S. SCHARF
ORANGE CARD: 0021232283

APPROVED BY
NAME: S. LEWIS
ORANGE CARD: 0022818927

REV	BY	DATE	DESCRIPTION	APPD.
C-01	LS	16/05/2017	AMENDED LANE CLOSURE WESTBOUND	SL
C-00	LS	08/05/2017	RSA COMMENTS	SL
B-01	LS	16/03/2017	INCLUDE RMS TICKET NUMBERS	SL
B-00	LS	04/02/2017	TINSW COMMENTS (27 FEB) ADDRESSED	SL
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL
A-00	SS	11/02/2017	INITIAL DRAFT	SL

DRAWN BY: LS

DRW CHECK: SL

APPROVED: SL

IND REVIEW: N/A

PROJECT

CLIENT

SYDNEY METRO

DELTA GROUP AUSTRALIA WIDE
SLOW LANE CLOSURE
MOWBRAY RD - CHATSWOOD

DRAWING No: DD-SYDM-DWG-0001-11

SHEET 1 OF 2

REVISION C-01



GENERAL NOTES

- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL 2010 AND IS BASED ON TCP NO.s 57 & 100.
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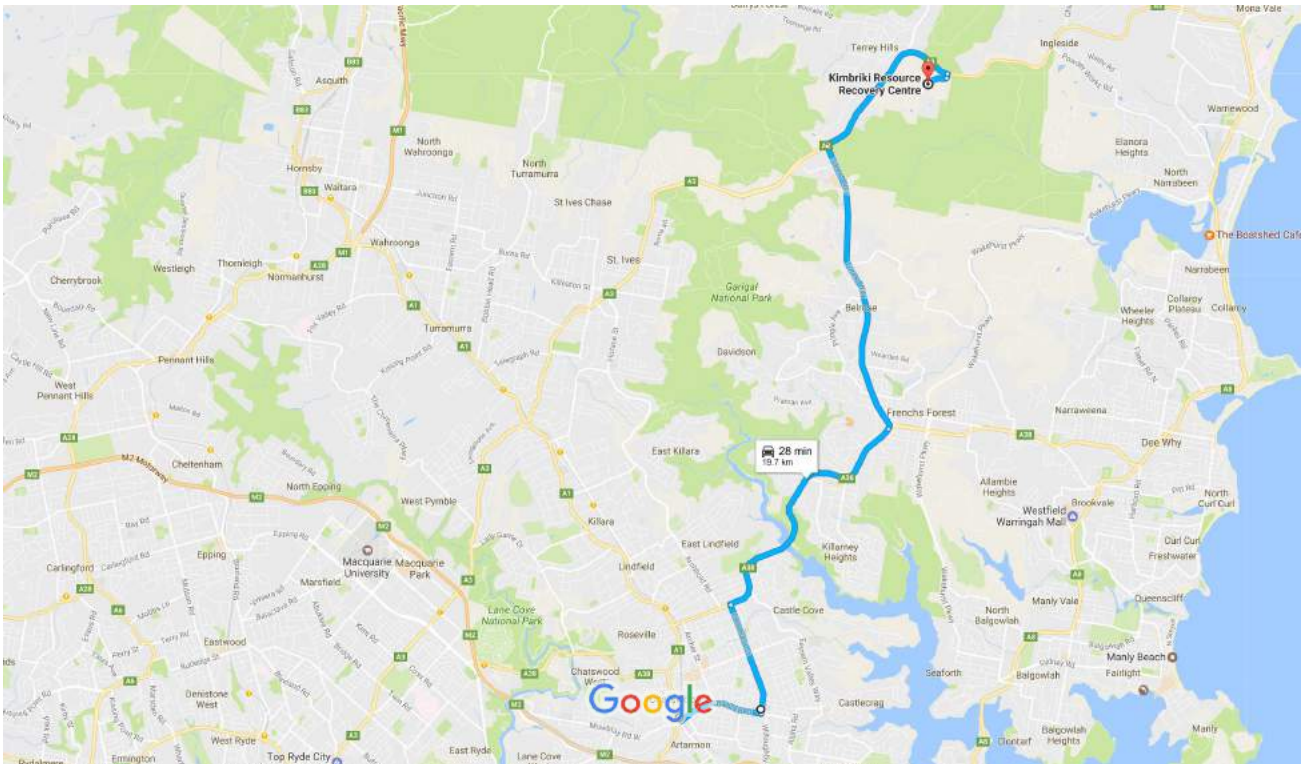
REVIEWED BY
NAME: S. SCHARF
ORANGE CARD: 0021232283

APPROVED BY
NAME: S. LEWIS
ORANGE CARD: 0022818927

DRAWN BY: LS			SYDNEY METRO DELTA GROUP AUSTRALIA WIDE SLOW LANE CLOSURE MOWBRAY RD - CHATSWOOD	DRAWING No: DD-SYDM-DWG-0001-11			
DRW CHECK: SL				SHEET	2	OF	2
APPROVED: SL				REVISION			
IND REVIEW: N/A				C-01			
REV	BY	DATE	DESCRIPTION	APPD.			
B-01	LS	16/03/2017	INCLUDE RMS TICKET NUMBERS	SL			
B-00	LS	04/02/2017	TINSW COMMENTS (27 FEB) ADDRESSED	SL			
A-01	LS	17/02/2017	CORRECT MINOR DRAFTING ERRORS	SL			
A-00	SS	11/02/2017	INITIAL DRAFT	SL			
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:				

Appendix 4 Haulage routes

DRAWING NUMBER	DESCRIPTION
DD-SYDM-DWG-0001-12	Logistics route – Chatswood to Terrey Hills
DD-SYDM-DWG-0001-13	Logistics route – Terrey Hills to Chatswood
DD-SYDM-DWG-0001-14	Logistics route – Eastern Creek to Chatswood
DD-SYDM-DWG-0001-15	Logistics route – Chatswood to Eastern Creek



Map data ©2017 Google 1 km

337 Mowbray Rd

Chatswood NSW 2067

- ↑ 1. Head north-east on Mowbray Rd 1.7 km
- ↶ 2. Turn left onto Penshurst St 83 m

4 min (1.8 km)

66-72 Penshurst St

Willoughby NSW 2068

- ↑ 3. Head north on Penshurst St towards Oakville Rd 2.2 km
- ↷ 4. Turn right onto Boundary St/A38 5.6 km
[Continue to follow A38](#)
- ↶ 5. Use the left 2 lanes to turn left onto Forest Way 6.0 km

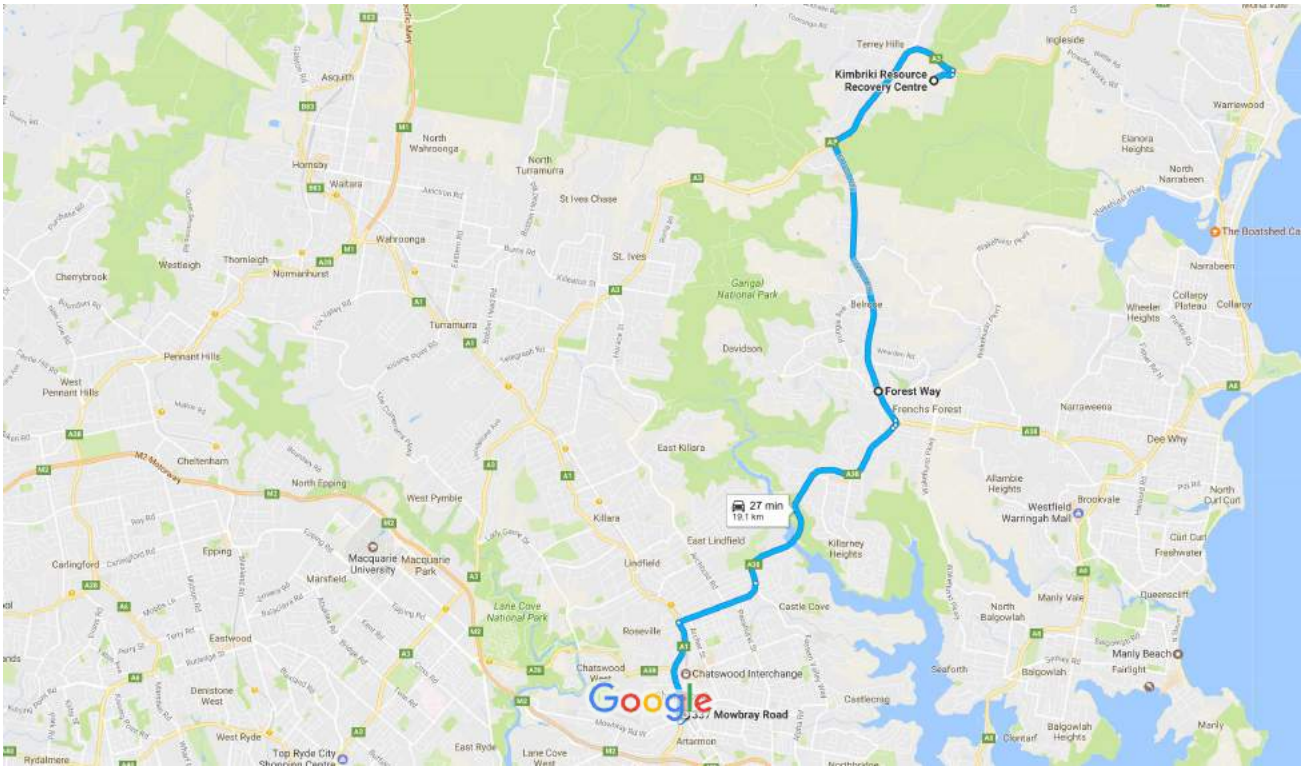
- 6. Use any lane to turn right onto Mona Vale Rd/A3
3.5 km
- 7. Turn right onto Kimbriki Rd
82 m
- ↗ 8. Slight right
400 m
- ↶ 9. Turn left
ⓘ Destination will be on the left
30 m

24 min (17.9 km)

Kimbriki Resource Recovery Centre

Kimbriki Road, Off Mona Vale Road, Terrey Hills NSW 2084

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 1 km

Kimbriki Resource Recovery Centre

Kimbriki Road, Off Mona Vale Road, Terrey Hills NSW 2084


- ↑ 1. Head north
_____ 30 m
- 2. Turn right towards Kimbriki Rd
_____ 400 m
- ↙ 3. Slight left onto Kimbriki Rd
_____ 82 m
- ↶ 4. Turn left onto Mona Vale Rd/A3
_____ 3.4 km
- ↶ 5. Turn left onto Forest Way
_____ 5.3 km


10 min (9.2 km)


Forest Way


Frenchs Forest NSW 2086


- ↑ 6. Head south-east on Forest Way towards Naree Rd
_____ 700 m


-  7. Continue straight to stay on Forest Way


110 m
-  8. Keep right to stay on Forest Way


80 m
-  9. Merge onto Warringah Rd/A38


5.0 km
-  10. Use the right 3 lanes to turn slightly right to stay on Warringah Rd/A38

 Continue to follow A38

1.8 km
-  11. Use the left 2 lanes to turn left onto Pacific Hwy/A1/A38

 Continue to follow Pacific Hwy/A1

2.1 km
-  12. Turn left onto Mowbray Rd

 Destination will be on the left

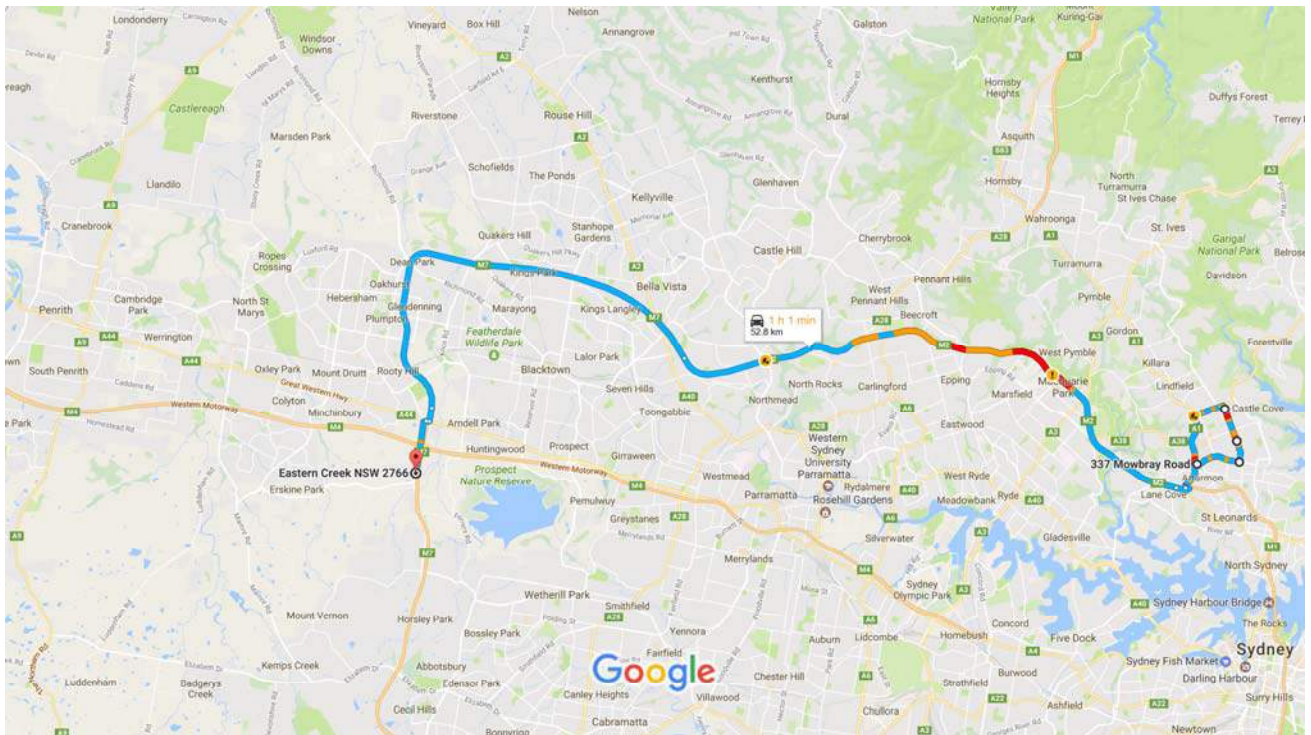
160 m

17 min (9.9 km)

337 Mowbray Rd

Chatswood NSW 2067

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 2 km

337 Mowbray Rd

Chatswood NSW 2067

Get on M2 from Penshurst St and Pacific Hwy

- ↑** 1. Head north-east on Mowbray Rd

18 min (9.0 km)
- ↶** 2. Turn left onto Penshurst St

1.7 km
- ↶** 3. Turn left onto Boundary St/A38

2.3 km
- ↶** 4. Use the left 2 lanes to turn left onto Pacific Hwy/A1/A38
i Continue to follow Pacific Hwy

1.2 km
- ↶** 5. Turn left onto the Ln Cove Tunnel/M2 ramp to Epping/Windsor
▲ Toll road

3.0 km
- 750 m

Follow M2 and M7 to Great Western Hwy/A44 in Bungaribee. Take the Gt Western Hwy/A44 exit from M7

29 min (41.5 km)

- 6. Merge onto M2
⚠ Toll road
22.6 km
- 7. Continue onto M7
⚠ Toll road
18.3 km
- 8. Take the Gt Western Hwy/A44 exit towards Eastern Creek/St Marys
⚠ Toll road
550 m

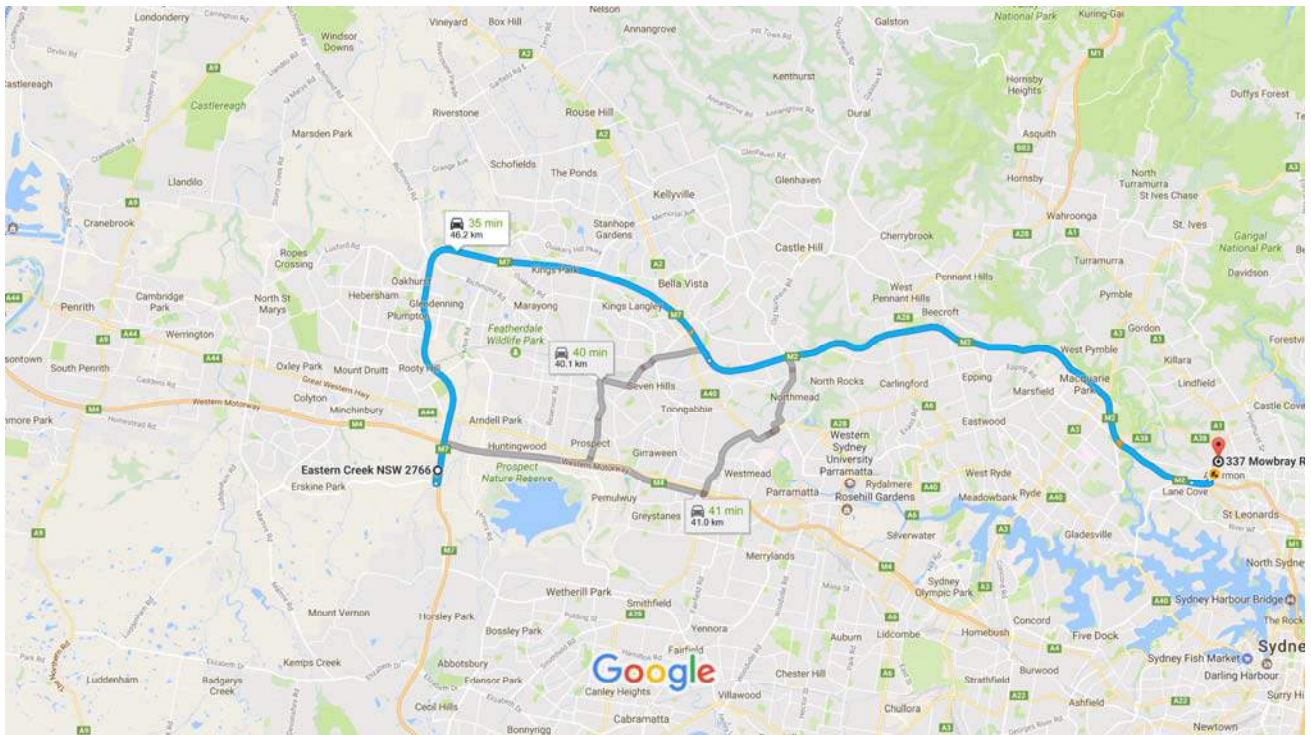
Drive to Wallgrove Rd in Eastern Creek

- 9. Use the 2nd from the left lane to turn right onto Great Western Hwy/A44
4 min (2.2 km)
150 m
- 10. Turn left onto Wallgrove Rd
2.1 km

Eastern Creek

New South Wales 2766

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.



Map data ©2017 Google 2 km

Eastern Creek

New South Wales 2766

- ↑ 1. Head south on Wallgrove Rd

550 m
- ↗ 2. Turn left to merge onto M7 towards Blacktown/NewCastle

⚠ Toll road

21.8 km
- ↑ 3. Continue onto M2

⚠ Toll road

22.1 km
- ↙ 4. Take the A1/Pacific Hwy exit

⚠ Toll road

900 m
- ↘ 5. Turn right onto Pacific Hwy/A1

700 m
- ↘ 6. Use the right lane to turn right onto Mowbray Rd

i Destination will be on the left

180 m

337 Mowbray Rd

Chatswood NSW 2067

Appendix 5 Road safety audit

ROAD SAFETY AUDIT FINDINGS AND RECOMMENDATIONS

Audit Stage: Desktop Traffic Management (CTMP)

Title of package: Chatswood Dive Site

Description of works: Building Construction

Plans / documents: CTCP 0001 Rev C00 May 2017

Client: Delta Group (Sue Lewis Consulting)

Audit company: Road Safety Audits P/L

Senior auditor/s: Peter Harris / Raj Muthusamy (Level 3 SRSA NSW)

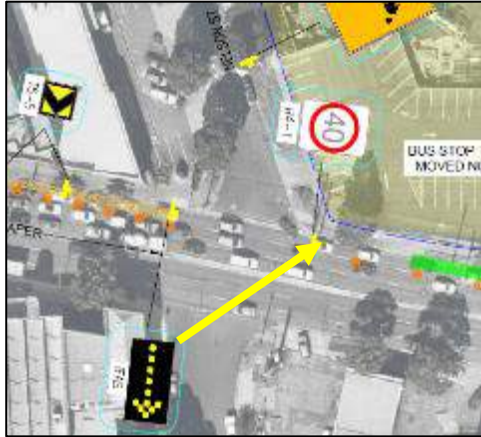
Report date: 02/05/07



RSA P/L reference: RSA-05625

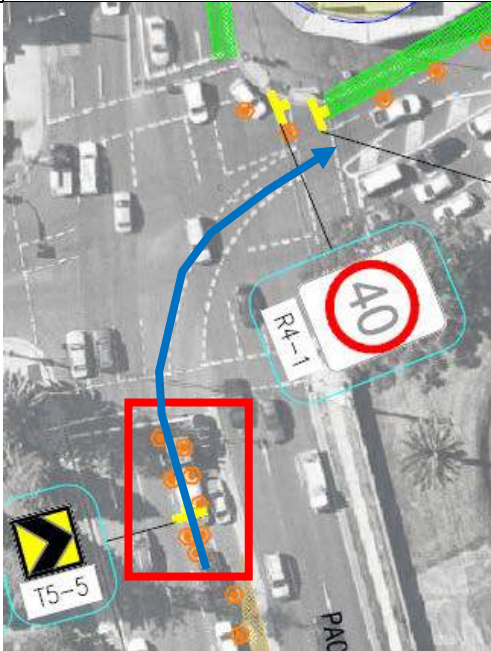
Notes: - Nil.


Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
General: It's considered that the key <i>road safety</i> issue with this CTCP Site Establishment Plan (CTCP sheet 19) is the management of the footpaths in the long-term with regard to visibility, light, sight lines, trip hazards and so on.				
1.	It's unclear whether 'chainwire fencing' on the plan refers to standard temporary fencing. If it does, temporary fence bases often can be trip hazards within pedestrian paths.	Consider aligning bases with fence and providing other means of lateral fence support. Alternatively, bump rails can steer pedestrians away from the supports. Examples below.	Reject	The temporary fencing will be installed into the ground and there is no need for temporary bases
				

Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
2.	Similar to point 1, the specific details are unclear for the support feet of the hoarding (as it connects to the ground).	Ensure they are not trip hazards (e.g. white / left) or that trip hazards are 'buffered' (e.g. yellow / right). Examples below.	Accept	Hoarding footings will be placed to not impact pedestrian movements
 <p data-bbox="248 794 533 820"><i>Examples of trip hazards</i></p> <p data-bbox="1406 794 1664 820"><i>Trip protection device</i></p>				
3.	The 2.13m wide pathway under the hoarding narrows down the existing footpath. There may be road furniture that needs to be removed as part of the CTCP such as the metal hoop shown below.	Review road furniture and adjust accordingly.	Accept	Prior to installation of hoarding DELTA will confirm the metal hoop and its possible removal
				

Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
4.	Sheet 0001-06 shows a two-lane to one lane drop even though three (3) lanes are present. It's assumed that this plan only shows two active lanes because of off-peak parking that's to remain during the lane closure time.	Confirm.	Accept	Drawing amended
5.	Sheet 0001-06: Flashing arrow boards mounted on trailers or vehicles can interfere with sight distance from side roads.	Consider relocating the flashing arrow board to the other side of Nelson Street to design-out this issue. It should offer similar/same visual benefit slightly further back.	Accepted	Drawing amended
				

Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
6.	<p>Plan 0001-06: T5-5 signs mounted in frames can be awkward in these small 'wedge' areas.</p> 	<p>Consider using collapsible chevron delineators for this purpose.</p> 	Accept	Note added to drawing

Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
7.	<p>Plan 0001-06: Further to the above point, in the experience of RSA P/L, sometimes these small lateral shifts into the 'correct' turn lane are more trouble than they are worth unless they are exceptionally clear, well set up with good approach site distance and maintenance of knocked over devices.</p>	<p>An alternative worth considering is closing the right turn lane only, resulting in no driver decision, or lateral shifts, or knocked over traffic devices into active lanes.</p> <p>Although drivers are in the 'incorrect' turn lane, it's very easy to see the closed Mowbray Rd lane due to the flashing arrow / other devices, and drivers tend to gently shift over without difficulty.</p> <p>Consider this option.</p>	Reject	This set up is done to limit worker exposure to traffic and for maintenance of the bollards
				

Point No.	Issue	Suggestion	Delta Group (Sue Lewis Consulting) Response	
			Accept / Reject	Comment / Status
8.	Various: Quite often, the end point of lane closures are not obvious to drivers, sometimes due to scattered work activity and sometimes due to long spacing between bollards. This could result in an impatient driver inadvertently cutting into the work zone.	It is suggested that the bollard spacing be suitable and that 'taper-backs' be used to indicate the end of work zone. Example in photo below.	Accept	Drawing amended
				

Notes: -

Appendix 6 Review comments

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
----------------------	---

CTMP Title:

Document Number:

DD SYDM CTMP 0001 Chatswood A-03

Reviewer: Alan McNamara TfNSW

Responder: Sue Lewis

Date: 27 February 2017

Date: 4 March 2017

Item	Section/ Page	Comments	Response
1	P6 1 st Paragraph	The correct document name is Construction Traffic Management Framework (CTMF).	Document amended
2	S2.1.2	Mowbray Road – while it is a Regional Road, they generally fall under Councils care and control	After our discussions with North Sydney Council on regional roads it was noted that although the road comes under the care of council, the control is exercised only with RMS consent to proposals – document amended
3	S2.1.4	Bryson Street – for consistency, even though no speed limit is posted the default speed limit would apply	Document amended
4	S2.2	Security requirement for Mowbray House. To be covered by the Security Management Plan? Reference should be made here	Document amended
5	S2.2.1	Works Required and Appendix 3, TCP's – Does not detail or indicate management of the bus stop on the Pacific Highway during the demolition of the awnings	Document amended
6	S2.2.2.1	Scaffolding and Hoarding branding should be covered to be in line with Sydney Metro requirements.	Scaffolding and hoarding layout shown to ensure that appropriate path widths and sight lines are

Item	Section/ Page	Comments	Response
			maintained. The branding is a discussion between the relevant Council and TfNSW – no change to document
7	S3.1	‘Where possible’ is fairly ambiguous. Any departure from the EIS should be clear.	Document amended
8	S3.4	Queuing of trucks will be avoided how? Is there a staging area set aside prior to 7am? If this can be detailed or shown on plan	No staging area is provided as there is only 1 truck arriving every couple of hours which will arrive post opening of the site
9	General	A section on traffic controllers, when/how/what they will be implemented for and what standards/procedures will they follow.	This should be covered under the Construction Traffic Management Framework as it is relevant to all sites. No change to document
10	General	Section on Approvals and how they will be achieved also how changes will be managed. Coordination with stakeholder etc (TCG, TTLG etc)	This is covered under the Construction Traffic Management Framework and is not repeated here as this CTMP is in accordance with the CTMF
11	Appendix 1	Mowbray’ is misspelt, Hampden Road is labelled as Elizabeth St	Document amended
12	Appendix 2	Proposed Access/Egress – ‘Mowbray’ misspelt in Frame titles	Document amended
13	Appendix 3	TCP Sheet 2 – Departing speed limit on Mowbray Road East should be ‘50km/h’, not 60 as shown	Document amended
14	Appendix 3	TCP Sheet 2 – The northbound approach and management of the dual lane right turn bay on Pacific Highway appears confusing. The right lane is closed off on the approach but then vehicles are directed into the right lane at the stop line. Is that correct?	This is typically how these are closed as it provides greater safety for the traffic controller during set up/ demobilising and for maintenance during the lane closure

Item	Section/ Page	Comments	Response
15	Appendix 4	Logistics routes, Kimbriki to Mowbray Road – Should either be along Mona Vale Rd, Forest Way, Warringah Road, Boundary Road to the Pacific Highway at Chatswood or Mona Vale Road to the Pacific Highway at Gordon.	Drawing amended as per direction

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
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CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 B 00

Reviewer: Peta Smith

Responder: Sue Lewis

Date: 15 March 2017

Date: 15 March 2017

Item	Section/ Page	Comments	Response
1		The vehicle swept path for vehicles exiting the site on the Mowbray Road W Option 2 shows vehicles entering the right turn bay to Hampden Road	Correct – this is for a semi-trailer movement. The use of semi-trailers will be limited to outside of peak hours – document amended
2		Do the turning paths fit within the existing site gates? Is this what DD-SYD-DWG-0001-04 demonstrates	Yes
3		The TMP doesn't mention when the TCP are likely to be implemented and duration	Document amended
4		If the TCPs are utilised during oversize/ over mass delivery to the site, is there a plan for the vehicle to enter the closed section lane 1 on Mowbray Road prior to site entry? The turning path shows entry from lane 1	Typically the oversize/ over mass vehicles are under pilot – document amended

CTMP Review Comments Sheet

Project Title
Sydney Metro City & south west – Demolition Contract

CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 Chatswood B-00

Reviewer: Neil Strickland

Responder: Sue Lewis

Date: 23/ 24 March 2017

Date: 23 March 2017

Item	Section/ Page	Comments	Response
1		RMS has advised that this does not require approval from the traffic committee. However, has Delta submitted it to the Transport Management Centre and RMS separately	The CTMP has gone through to RMS and Sydney Coordination Office as I have been instructed to do. RMS have provided comments on this CTMP already
2		Thanks. I don't know about you, but I'm confused as to why separate approvals are needed from TMC and RMS for some matters. An integrated approval process for TfNSW agencies should be possible	NA

CTMP Review Comments Sheet

Project Title
Sydney Metro City & south west – Demolition Contract

CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 Chatswood B-00

Reviewer: Neil Strickland

Responder: Sue Lewis

Date: 4 April 2017

Date: 1 May 2017

Item	Section/ Page	Comments	Response
1		Council's Assets Engineer has advised that Council should hold a bond (bank guarantee with no expiry date) during the works. The bond they would use is pretty much half the cost of the replacement of the infrastructure around the site using Council's restoration rates. He will assist the calculation of this bond today. There is a bus shelter on the highway, I assume bond will need to cover this also. It appears to be a JCDecaux shelter. Other fees will also apply eg hoarding fees, various permits, work zones etc	Sydney Metro will be discussing this matter with Willoughby Council
		You have said that the bus stop will be relocated north of the highway temporarily to allow the demolition of the awning on the highway. I assume the shelter is remaining? It is assumed DELTA will protect the shelter during the works	The shelter will remain as the relocation is only for a couple of nights during the awning works. There is a small section of awning immediately to the south of the site. The main awning works are at the intersection of Mowbray Road and Pacific Highway. The bus stop is to be relocated mainly due to the lane closure rather than the awning works. Document amended

Item	Section/ Page	Comments	Response
		Council is responsible for the maintenance of the footpath on the Pacific Highway. pram ramps at traffic signals and K&G (on the Pacific Highway) are RMS responsibility	Noted
		Please ensure that hoarding is not installed on drainage infrastructure in the footpath	Noted and confirmed
		You propose a structural engineer to carry out the condition inspections, however it should be a suitably qualified civil engineer that has experience with condition audits of road pavement, footpath, k&g, storm water infrastructure etc. a structural engineer is not likely to have this experience.	The survey will be undertaken by ARRB or similar. It is noted that the dilapidation report does not form part of the CTMP
		What form will the dilapidation report take? It is assumed it will be a detailed photographic survey with commentary on any pre-existing damage with approx.. dimensions and positions marked on a map	The report will be similar to reports provided by ARRB. I note that damage details will not likely include dimensions but will have photos of the damaged infrastructure. It is noted that the dilapidation report does not form part of the CTMP
		<p>The detail below does not include but should include:</p> <ul style="list-style-type: none"> • Street trees • Street furniture including bins, signs and bus shelter on Pacific Highway • No mention of Bryson Street in the dilapidation survey. This should be done also including pavement, K&G, footpath and signage • Drainage infrastructure (pits and grated drains) in the footpath at the corner of Pacific Highway and Mowbray Road and adjacent 	<p>These details will be provided by Hawkeye or similar. It is noted that the dilapidation report does not form part of the CTMP</p> <p>Note that Bryson Street was gazetted by TfNSW in February 2017</p>

Item	Section/ Page	Comments	Response
		<p>to Nick Scali. Grated drains in the footpath area adjacent to Mowbray Road. And stormwater pits generally on the kerb</p> <ul style="list-style-type: none"> • Bus shelter on Pacific Highway near Nick Scali • On Mowbray Road, pavement survey shall include 2 lanes eastbound between Pacific Highway and Railway line due to wide turning vehicles • On Nelson Street road pavement survey shall include both lanes, east and westbound and parking lane adjacent to site, including dish drain in pavement due to wide turning vehicles 	
		<p>Delta states that the main access/ egress is proposed from Mowbray Road at the existing driveway into Ausgrid. Semi trailer access will be from the driveway immediately east of the Pacific Highway on Mowbray Road. Note that this driveway is not used very often I believe. With other existing driveways on Mowbray Road used for light vehicle access</p>	<p>It is noted that Council believe that the driveways were used for light vehicle access. However, in discussions with Ausgrid staff this was not the case of the term of this depot's life.</p>
		<p>Appendix 2 Frame 1 – egress on Nelson Street shows removal of parking spaces due to swept path of vehicles including travel over K&G and footpath and through a street tree. Has this been approved?</p>	<p>As Council would be aware, this CTMP has not been approved, hence neither has the use of this driveway. We have now reviewed the access and will limit access to Mowbray Road. Drawing amended</p>
		<p>Appendix 2 Frame 2 shows that large vehicle cannot turn into driveway without entering second lane. Picture shows vehicle mounting K&G and footway</p>	<p>Drawings have been amended to show the swept paths and the amount of works to modify the current vehicle crossings</p>
		<p>Appendix 2 Frame 3 trailers will not be able to access site without access to second lane without mounting K&G and footway</p>	<p>Driveway will be altered to suit turning movements. Drawing amended</p>

Item	Section/ Page	Comments	Response
		<p>The Assets Engineer noticed a stormwater system at the end of Bryson Street has stormwater been considered for Bryson Street? and will stormwater infrastructure remain? There appears to be a easement through the site we would normally ask for a pre-CCTV and post CCTV of the stormwater systems through the site if Council infrastructure. I need to look at the dealing for the easement to determine if ours</p>	<p>The works at this site only include demolition to ground slab – no excavation works will take place, hence there will be no impact on the existing stormwater infrastructure.</p> <p>Note that Bryson Street gazettal occurred on 10th February 2017 and that notice is included following the review comments in Appendix 7</p>

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
----------------------	---

CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 Chatswood B-00

Reviewer: Neil Strickland

Responder: Sue Lewis

Date: 5 April 2017

Date: 6 April 2017

Item	Section/ Page	Comments	Response
1	Pages 22 and 26	What is the significance of the areas shown in bright green on page 22 and 26? What are the orange areas? What are the orange dots?	the bright green areas are: DWG-0001-6 Sheets 1 and 2- lane closure DWG-0001-07 - swept path orange dots are bollards
2	Page 24	On page 24 it is indicated that pedestrians must move to the right. Does that mean pedestrians should use the footpath on the other side of Pacific Highway? If so how will pedestrians be protected from traffic?	the pedestrians would be within the closed lane - if you would like the pedestrians detoured then please confirm
3	Appendix 5	Do you have the Road Safety Audit referred to in Appendix 5 (blank)	Included in current document

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
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CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 Chatswood B-00

Reviewer: Neil Strickland

Responder: Sue Lewis

Date: 10 April 2017

Date: 16 April 2017

Item	Section/ Page	Comments	Response
1		I refer to the Construction Traffic Management Plan and traffic control plans (in respect of the subject development)	
		Council gives approval to these plans subject to:	
		1. Compliance with the terms and conditions of any Road Occupancy License(s) issued by NSW Roads and Maritime Services	Noted
		2. Amendment to the Traffic Control plans to include placement of suitable water filled or concrete barriers for the protection of pedestrians where they are being diverted to the carriageway along Pacific Highway and Mowbray Road	Water filled barriers are not approved for use by RMS. The installation of concrete barriers is not possible due to time constraints. Traffic controllers will be used to guide pedestrians through the lane closures
		3. Implementation of the Traffic Control Plan(s)	Noted
		4. A separate hoarding permit application being submitted to Willoughby City Council. In this regard, pavement of hoarding fees in accordance with Council's published Fees and Charges 2016/ 17 is required as follows	Noted

Item	Section/ Page	Comments	Response
		Administration Fee – Class A or B \$980 Plus \$ per square metre per week-Class A or B \$15 Plus if airspace utilised-per square meter per week Class B \$15 Builders Fence on Council’s Property (max 600mm off the property boundary-per meter per week in residential areas only \$15	
		5. A separate road occupation application between submitted to Willoughby City Council. In this regards, pavement of fee for occupation of part road in Nelson Street in accordance with Council’s published fees and charges schedule (Crane/ Heavy Permit-Occupation of Pubic Road for Crane/ Concrete Pump/ Heavy Plant is required as follows: One road lane or Work zone per day \$250 Plus each additional lane per plant item per day \$350 Half road/ lane closure if permitted per day (No Plant) \$775 Full road/ lane closure if permitted per day (No Plant) \$1400 Plus Damage Deposit (Non DA related application) \$4000	Noted
		6. Payment of all other fees and charges in accordance with Council’s published fees and charges schedule. Total charges will be the subject of separate advice from Council	Assume that this was the email sent Wednesday 5 th April by Paul Gomez (see below). Note that this is subject to discussions with TfNSW
		7. Notification of all neighbouring businesses and properties prior to commencement	TfNSW is responsible for all notifications
		8. Construction of widened vehicular crossings in Nelson Street and Mowbray Road to encompass the swept path of heavy vehicles as indicated by the proposed site access diagrams and other images included in the CTMP	Noted. Areas of modifications now noted on drawing. Note that Nelson St is no longer proposed for use

Item	Section/ Page	Comments	Response
		<p>9. Payment of associated Footpath Crossing Permit fees in accordance with Council's published fees and charges schedule:</p> <p>Inspection fee per crossing (includes initial, formwork and final inspection) \$260</p> <p>Each additional inspection visit \$110</p> <p>Damage deposit (complying Development certificate CDC application) \$4000</p> <p>Damage deposit (replace existing crossing only).....\$1200</p> <p>Inspection-release of Damage Deposit \$110</p> <p>Permit Assessment Free for Non DA application (includes one Permit Cancellation Fee (50% of the permit fees) inspection \$260</p>	Noted
		<p>10. Payment of a damage deposit or submission of an unconditional bank guarantee in respect of Council's civil infrastructure. This deposit will be held by Council to be called up, if necessary, during the works where damage caused to Council's infrastructure by Delta Group creates a public risk. The deposit will be considered to take place of any other deposits referred to above</p>	Sydney Metro will be discussing this matter with Willoughby Council
		<p>11. Suitable arrangements, to the satisfaction of STA, being made for the temporary relocation of the bus stop in Pacific Highway</p>	Discussed and agreed with Sydney Buses
		<p>12. Delta making suitable arrangements for the protection of the JC Decaux bus shelter on Pacific Highway and being responsible for the cost of repairs for any damage caused to this shelter as a result of the subject work.</p>	TfNSW to discuss. Note that in the final arrangement, it is understood that this bus stop will no longer exist

Item	Section/ Page	Comments	Response
		13. Suitable devices being placed around street tress to ensure they are not damaged as a result of the proposed work	Confirmed
		14. Compliance with the requirements of NSW Roads and Maritimes Services	Confirmed
		It is noted that the value of the cash bond/ bank guarantee and the hoarding fees have been sent to Delta Group under separate cover	Except below. Note that TfNSW will discuss with Council

CTMP Review Comments Sheet

Project Title	Sydney Metro City & south west – Demolition Contract
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CTMP Title: Chatswood Dive Site

Document Number:

DD SYDM CTMP 0001 C 00

Reviewer: Gordon Farrelly

Responder: Sue Lewis

Date: 15 May 2017

Date: 15 May 2017

Item	Section/ Page	Comments	Response
1	General	Willoughby City Council has no objections to on the latest version of the Construction Traffic Management Plan (CTMP), copy attached. Notwithstanding Council provides the following comments:	Noted
2	2.3.1	Council's policy for normal construction hours that are applied to land use development conditions of consent are Monday to Friday 7.00 am – 5.00 pm, Saturday 7.00 am – 12 Midday, no work permitted Sunday and public holidays. The adoption of these construction times is requested.	Noted, however, Minister's Conditions of Approval are listed in the CTMP and will be retained
3	2.3.2	Council would like it confirmed through turning path analysis that all vehicles egressing the site will do so in a safe manner when turning left into Mowbray Road.	Previously included on drawing 0001-02
4	2.3.11	Council's requirement for permits and approvals for work related to the site and on Mowbray Road, Pacific Highway and Nelson Street (road and footpath) that bound the site to be accepted. Council has supplied a detailed outline of all	Councils requirements for permits and approvals is noted within the CTMP. The requirement for bonds is a discussion between TfNSW and Council

Item	Section/ Page	Comments	Response
		<p>relevant permits and approvals information that advises of the need for Council permits and approvals including potential conditions and fees. Please refer to Appendix 6 CTMP Review Comments Sheet Neil Strickland's advice dated 4 and 10 April 2017. Council is keen to work with TfNSW to understand the scope of work/ permits and other Council approvals necessary to ensure efficient and timely delivery of permits / approvals.</p>	
5	3.2	<p>Willoughby Council is keen to ensure that the use of Mowbray Road by SPV and OSOM vehicles is undertaken with optimal road safety, efficiency and asset protection and is willing to work with RMS to maximise road safety, traffic management and asset condition. Willoughby Council is unaware whether Mowbray Road is classified by NHVR for use by Special Purpose Vehicles (SPV) and Oversize/ Overmass vehicles (OSOM) between Pacific Highway and Penshurst Street that will be used for site demolition purposes. If Mowbray Road is not classified for SPV and OSOM vehicles that will be used for site demolition purposes and needs to be classified to permit operation of these vehicles then TfNSW are to obtain approval for Mowbray Road to be used by SPV and OSOM from NHVR prior to the start of work. TfNSW should also fund all activities (including design, environmental assessment and community / stakeholder engagement) and works (including property acquisition and infrastructure changes)</p>	<p>RMS OSOM (Oversize and Over Mass) Permit section have advised that the only requirement to allow SPV vehicles is Council approval. Considering it is unlikely that this approval will be forthcoming based on the current comments, Delta will use the Pacific Highway for these deliveries. Document amended</p>

Item	Section/ Page	Comments	Response
		necessary to implement any changes to Mowbray Road that may be necessary to support the use and/ or classification of Mowbray Road for use by SPV and OSOM	
6	TCP 11	Appendix 3 TCP 0001-11 – add hoarding installation	Noted and document amended
7	TCP	Appendix 3 TCP Mowbray Road B-00 & C-00 Sheet 2 of 2 – Council would prefer the left turn only lane in Mowbray Road be available for traffic flow to reduce delays for eastbound traffic flow.	Noted and drawing amended

Willoughby Council Comments 16 May 2017:

Drawing 0001-02 text is Nelson Street but is actually Mowbray Road - Drawing amended

Gomez, Paul <Paul.Gomez@willoughby.nsw.gov.au>
 To: "siouxzie.lewis@gmail.com" <siouxzie.lewis@gmail.com>
 Cc: "Strickland, Neil" <Neil.Strickland@willoughby.nsw.gov.au>, "Bazergy, Joseph" <Joseph.Bazergy@willoughby.nsw.gov.au>, "Sung, David" <David.Sung@willoughby.nsw.gov.au>

Wed, Apr 5, 2017 at 1:45 PM

Hi Sue,

Further to the email from Neil below, I have spoken to the Development engineers. Council is seeking requesting a bond against potential damage of Councils infrastructure. This has been calculated as \$285,000. As per Councils Fees and charges for "Damage Deposit for other developments to be assessed on a case by case basis.". We have calculated the bond by the method below using Councils restoration rates for where applicable. Consideration is given that we do not believe it is likely that damage occurs at all locations so the calculation is overall halved.

	Quantity	Unit	Rate (inc Weeks	Amount (incGST)
K&G (Nelson St)	20 m		\$ 305	\$ 6,100
Driveway (Nelson St)	30 sq.m		\$ 530	\$ 15,900
Road Pavement (Nelson St)	180 sq.m		\$ 530	\$ 95,400
Driveway 1 Mowbray Road (west)	24 sq.m		\$ 530	\$ 12,720
K&G Mowbray Road (west)	20 m		\$ 305	\$ 6,100
Road Pavement adjacent to crossing Mowbray Road (west)	90 sq.m		\$ 530	\$ 47,700
Driveway 2 Mowbray Road (East)	21.6 sq.m		\$ 530	\$ 11,448
K&G Mowbray Road (East)	20 m		\$ 305	\$ 6,100
Road Pavement adjacent to crossing Mowbray Road (East)	90 sq.m		\$ 530	\$ 47,700
Damage to footpath adjacent to boundary all around site 1.5m wide.	756 sq.m		\$ 305	\$ 230,580
Bus Shelter Pacific Highway	1 each		\$ 20,000	\$ 20,000
Stormwater Infrastructure in footpath				
Pits	6 each		\$ 5,000	\$ 30,000
Grate Drains/pipes	80 m		\$ 500	\$ 40,000
Sub Total				\$ 569,748
Sub Total /2				\$ 284,874

Under Councils Fees and charges hoarding fee of \$139,643 has been calculated as below.

Hoarding Fee Class B				
Admin Fee	1 each		\$ 980	\$ 980
Rate per sqm/week (100x2.13m) (152 days/5 months)=21.7 weeks	213 sq.m		\$ 15	21.7 \$ 69,331.50
If air space is utilised then additional \$15/sq.m/week	213 sq.m		\$ 15	21.7 \$ 69,331.50
Sub Total				\$ 139,643

I notice that your turning path at most of the driveways as visible from the drawings provided, shows the vehicles mounting the kerb and footpath area, which is unacceptable.

It may be that the driveways need to be widened to accommodate the turning paths or alternative movements proposed. Fees for vehicular crossing approvals are also in Councils fees and Charges. Nelson street is of particular concern as the vehicle mounts footpath, kerb and is shown to be going through a street tree and though two parking spaces.

This tree has not been approved to be removed as far as I am aware.

If these parking spaces are to be removed then a fee of \$250 per day for one road lane or work zone per day would apply.

Please do not hesitate to contact us if you have any questions.

Regards

COMMENTS REGISTER

Report Name:	CTMP Chatswood Dive Site		
Author:	Sydney Metro Chatswood to Sydenham		
Version:	Revision B-00		
Date:	24/03/17		
Section	Issue	Stakeholder Comment	Delta Response
2.2.1	Bus Stop	<p>Delta is to approach the Bus Operator and discuss the issue. It may be that the better solution is to close the bus stop for the two nights you will be undertaking the works</p> <p>It is preferable that all vehicles use the easternmost driveway. Semitrailers using the driveway closest to the Pacific Highway may impact on the operation of the intersection, even more when they are turning into the driveway from lane 2</p>	<p>Document amended post discussion with Sydney buses.</p> <p>Noted that semi-trailer movements will be limited and undertaken outside of peak periods. The existing driveway and gate configuration at the eastern end is not suitable for semi-trailer movements.</p>
2.2.2.1	Hoarding Placement	<ul style="list-style-type: none"> • Has the hoarding design been undertaken such that sight distance to all signal lanterns is not obstructed? • And the bus stops are not affected? • Is the residual footpath width around the bus shelter sufficient for disabled people using wheelchairs? • Is the red light camera at the Pacific Highway and Mowbray Road being affected by the hoarding? If it is then there is need to approach the RMS to advise of the impacts. 	<p>Yes</p> <p>No</p> <p>Yes</p> <p>No</p>
3.2	Permits Over Dimensional vehicles	Please advise what over dimensional vehicles are intended for the site.	Document amended
4.1	Road safety audits	There is need to include a road safety audits in the draft TMPs, please include. This may result in further comments being submitted.	Noted and will be submitted as part of initial submission for the remaining sites

4.2	Police & Emergency Services	Please include in this section how the company intends to advise the emergency services of their activities.	Document amended
4.7	On Site contacts	This section is currently blank. There is need to have 24 hour contacts should an emergency arise.	Document amended
Appendix 3	DD-SYDM-DWG-0001-02	Frame 3 shows vehicles exiting the project site into lane 3, the right turn lane. This movement would obstruct all eastbound traffic on Mowbray Rd.	This manoeuvre would occur between the hours of 2000-0500 – document amended
Appendix 3	TCP's	Excluding hoarding installation, no information is supplied regarding when each TCP will be used.	Document amended
Appendix 5	Road Safety Audit	Please provide road safety audits in draft TMP submissions. Once provided this may generate further comments.	Refer to item 4.1 response

Appendix 7 Bryson Street gazettal notice

The position and extent for this feature is recorded and shown within the Geographical Names Register of New South Wales. The proposal can also be viewed and submissions lodged on the Geographical Names Board website at www.gnb.nsw.gov.au from 8 February to 8 March 2017. Alternatively, written submissions may be lodged with the Secretary, Geographical Names Board, 346 Panorama Ave, Bathurst, NSW 2795.

In accordance with Section 9 of the *Geographical Names Act 1966*, all submissions lodged may be subject to a freedom of information application and may be viewed by a third party to assist the Board in considering this proposal.

NARELLE UNDERWOOD
Chair
Geographical Names Board
PO Box 143
BATHURST NSW 2795

POISONS AND THERAPEUTIC GOODS REGULATION 2008

ORDER

Withdrawal of Drug Authority

In accordance with the provisions of clause 175(1) of the *Poisons and Therapeutic Goods Regulation 2008* an Order has been made on **Dr Bevan George WALKER (MED0001141053)**, of Dubbo NSW 2830, prohibiting him until further notice, as a medical practitioner, from supplying or having possession of drugs of addiction as authorised by clause 101 of the Regulation and issuing a prescription for a drug of addiction as authorised by clause 77 of the Regulation.

This Order is to take effect on and from 6 February 2017.

Dated at Sydney, 3 February 2017.

ELIZABETH KOFF
Secretary
NSW Health

TRANSPORT ADMINISTRATION ACT 1988

LAND ACQUISITION (JUST TERMS COMPENSATION) ACT 1991

Notice of Compulsory Acquisition of Land in the Local Government Area of Willoughby

Transport for NSW by its delegate declares, with the approval of His Excellency the Governor, that the land described in the Schedule 1 below is acquired by compulsory process under the provisions of the *Land Acquisition (Just Terms Compensation) Act 1991* as authorised by clause 11 of Schedule 1 of the *Transport Administration Act 1988* for the purposes of the *Transport Administration Act 1988*.

TOM GELLIBRAND
Acting Program Director
Sydney Metro
Transport for NSW

SCHEDULE 1

All that piece of land situated in the Local Government Area of Willoughby, Parish of Willoughby and County of Cumberland comprising:

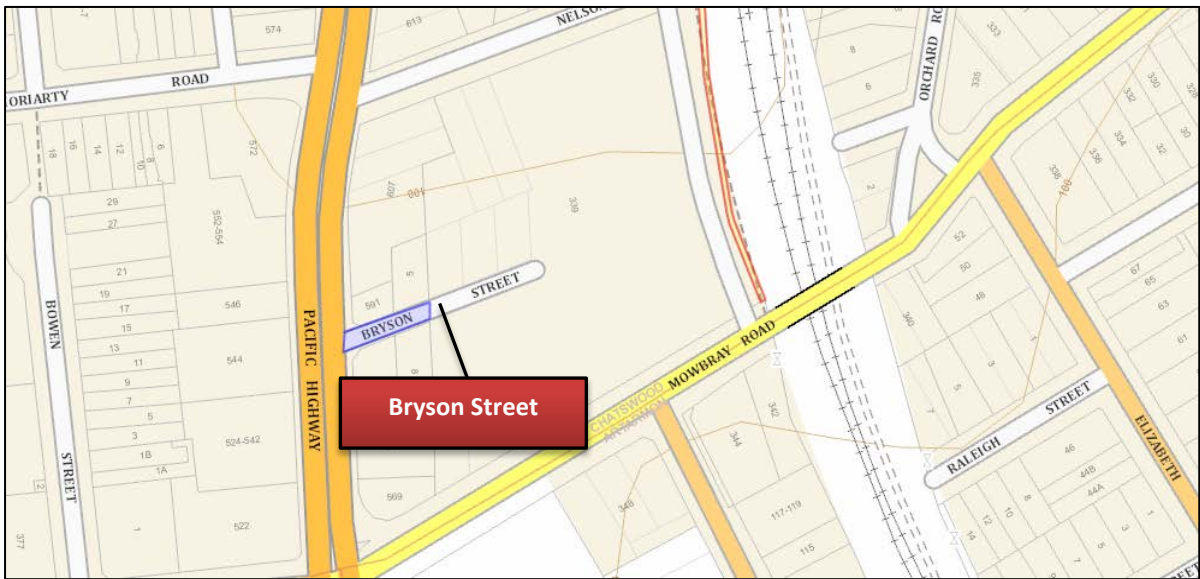
- (a) Lot 1 in DP221896, being the whole of land in Certificate of Title 1/DP221896, excepting the interest in land described in Schedule 2 below; and
- (b) Lot 2 in DP1223080 identified in the plan of acquisition registered at Land and Property Information NSW on 17 August 2016,

both said to be in the possession of the Council of the Municipality of Willoughby.

SCHEDULE 2

N370192 Easement for pipeline affecting the whole of the land described in DP221896
(Transport for NSW Document Number: *SM16/09508 A5653172*)

Attachment B – Willoughby Council Road – Chatswood



From: [Alan McNamara](#)
To: [McNamara, Alan](#)
Subject: FW: CTMP for Chatswood Dive Site
Date: Friday, 9 June 2017 2:34:00 PM

From: RAMIREZ Giovanni [<mailto:Giovanny.RAMIREZ@tmc.transport.nsw.gov.au>]
Sent: Friday, 9 June 2017 10:10 AM
To: Farrelly, Gordon
Cc: GALE Aaron J; Fiegel, Naomi; McNamara, Alan; Hind, Kenneth; Brogan, Philip
Subject: FW: CTMP for Chatswood Dive Site

Gordon

Please see below email.

Regards / Gio

From: RAMIREZ Giovanni
Sent: Friday, 9 June 2017 10:07 AM
To: 'Ben Shum'; 'Sue'; O'Leary, Peter
Cc: ISSA Steven C; SMITH Peta A
Subject: CTMP for Chatswood Dive Site

Ben / Sue / Peter

In accordance with clause 2.3 (c) of the General Conditions, Schedule E3-Project Planning Approval and Conditions & the SMR E – Sydney Metro City and Southwest Demolition Contract the Roads and Maritime Service of NSW and the Sydney Coordination Office approve the Sydney Metro City & South West Construction Traffic Management Plan Chatswood Dive Site May 2017 Rev C-01 for demolition and related removal works for the Chatswood Site subject to the following requirements:

- Shall apply and obtain approval from the Transport Management Centre for Road Occupancy Licences (ROLs) for any required lane closures and/or speed limit reductions;
 - Undertake lane closures and reduce speed limits only as per approved ROLs/SZAs issued by TMC;
 - Addressing any safety issues identified within the Road safety Audit review of the Construction Traffic Management Plan;
 - Addressing any issues raised by Council, residents/business and/or Emergency Services as required;
 - The Construction Traffic Management Plan being updated as required prior to construction commencing to ensure consistency with the final Construction Traffic Management Framework when it is approved
-
- Promptly addressing any CBD Taskforce and/or TMC and/or RMS issue that eventuates during the works

Regards

Giovanny Ramirez
Principal Manager Taskforce Operations
CBD Transport Taskforce | Sydney Coordination Office
Transport for NSW
T 02 8396 1460 M 0403 098 060
www.transport.nsw.gov.au

Transport for NSW

CBD Transport Taskforce

25 Garden St | Bio Medical Bldg | Eveleigh 2015

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Mr Stuart Hodgson
Principal Manager,
Program Sustainability Environment & Planning
Sydney Metro
Transport for NSW
PO Box 588
NORTH RYDE BC NSW 1670

29 June 2017

Ref: CTMP-Chatswood

Dear Stuart

RE: Endorsement of Construction Traffic Management Plan – Chatswood Dive Site, Sydney Metro City & Southwest

Thank you for providing the following documents for Environmental Representative (ER) review and endorsement as required by the Condition of Approval E82 of the Sydney Metro City & Southwest project (SSI – 15_7400 January 9 2017).

- Construction Traffic Management Plan – Chatswood Dive Site, (Revision C-01 dated 18 May 2017).
- Email dated 9 June 2017 stating *“In accordance with clause 2.3 (c) of the General Conditions, Schedule E3-Project Planning Approval and Conditions & the SMR E – Sydney Metro City and Southwest Demolition Contract the Roads and Maritime Service of NSW and the Sydney Coordination Office approve the Sydney Metro City & South West Construction Traffic Management Plan Crows Nest Sites April 2017 Rev C-01 for demolition and related removal works for the Crows Nest Sites”*

It is noted that the Roads and Maritime Service of NSW and the Sydney Coordination Office approval above is conditional on the following:

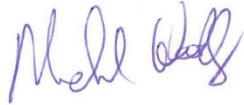
- (the project) Shall apply and obtain approval from the Transport Management Centre for Road Occupancy Licences (ROLs) for any required lane closures and/or speed limit reductions;
- Undertake lane closures and reduce speed limits only as per approved ROLs/SZAs issued by TMC;
- Addressing any safety issues identified within the Road safety Audit review of the Construction Traffic Management Plan;
- Addressing any issues raised by Council, residents/business and/or Emergency Services as required;
- The Construction Traffic Management Plan being updated as required prior to construction commencing to ensure consistency with the final Construction Traffic Management Framework when it is approved
- Promptly addressing any CBD Taskforce and/or TMC and/or RMS issue that eventuates during the works

As an approved ER for the Sydney Metro City & Southwest project, I have reviewed the above documents. The review did not comprise a technical review, as the ERs are not traffic experts. It is noted the document has had consultation as described in Table 3 and includes review comments and responses.

It is considered that the plan is generally consistent with Condition E82 and the Sydney Metro Construction Traffic Management Framework (CTMF) as referenced in Condition E81 of the Project Approval (subject to the above conditions being complied with).

It is noted that the CTMP defines truck egress to the east along Mowbray Road. This is different to the EIS which indicates a turn right signal. Sydney Metro have assessed this change from the EIS for demolition in an approved Consistency Assessment which has also been provided.

Yours sincerely



Michael Woolley
Environmental Representative – Sydney Metro – City and South West