



Integrated
Management
System

Martin Place, Pitt St and Bligh Street utility augmentation

Planning Approval Consistency Assessment Form

SM ES-FT-414

Sydney Metro Integrated Management System (IMS)

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Prepared for:	TSE contract
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The Planning Approval Consistency Assessment Form should be completed in accordance with the Sydney Metro Planning Approval Consistency Assessment Procedure (SM ES-PW-314) and Sydney Metro Environmental Planning and Approval Manual (SM ES-ST-216)

1.0 Existing Approved Project
<p>Planning approval reference details (Application/Document No. (including modifications)): Sydney Metro Chatswood to Sydenham SSI-7400 as modified 18 October 2017</p>
<p>Date of determination: EIS Approval date 09/01/17 Modification 1 Approval date 18 October 2017</p>
<p>Type of planning approval: Critical State Significant Infrastructure</p>
<p>Description of existing approved project you are assessing for consistency: The Chatswood to Sydenham component of Sydney Metro City & Southwest comprises a new metro rail line, approximately 16 kilometres long, between Chatswood and Sydenham. New metro stations would be provided at Crows Nest, Victoria Cross, Barangaroo, Martin Place, Pitt Street and Waterloo, as well as new underground metro platforms provided at Central Station. Utilities such as water, sewer and telecommunications need to be supplied to each of the major construction sites. Generally, these utilities are located close to the sites (such as the adjacent footpath) and the supply is considered ‘business as usual’ for supply companies. Utilities need to be adjusted, relocated and / or protected where there is a possibility they would otherwise be impacted by construction. The location of utilities has been determined from Dial Before You Dig plans, utility data, and local authority and council records. Further investigation and consultation with service asset owners would be carried out as the design develops to confirm exact locations, heights and depths of the utilities.</p> <p>Where an existing utility conflicts with the proposed design, it may be necessary to:</p> <ul style="list-style-type: none"> • Provide physical protection for the utility where the utility is not directly affected but may be indirectly affected by vibration or accidental impact. Protection could include constructing a piled wall between the excavation and the utility, plating over the utility to minimise the impact of construction traffic, or marking out or fencing off the location of a utility to avoid it being accidentally damaged • Modify construction methods to avoid impacting a nearby utility. For example, this could involve using only hand excavation and compaction tools such as hand digging tools, a vibration plate or pedestrian rollers where compacting within a specified distance of utilities

- Wrap and support the utility service to provide mechanical protection
- Divert the utility around the construction site.

Utility works assessed under the EIS for the Martin Place and Pitt St worksite are limited to

- Low voltage (LV) and High voltage (HV) power

The PIR assessed and approved an additional underground pedestrian link which brought in 33 Bligh Street site. Whilst not shown on a layout utilities are required to be provided for the excavation of the shaft and underground pedestrian link.

(See maps in Appendix A for the original routes assessed).

Relevant background information (including EA, REF, Submissions Report, Director General’s Report, MCoA):

- Chatswood to Sydenham Environmental Impact Statement and accompanying technical papers (May, 2016)
- Chatswood to Sydenham Submissions and Preferred Infrastructure Report (October, 2016)
- Conditions of Approval (dated 9 January 2017).
- Modified Conditions of Approval (dated 18 October 2017)

All proposed works identified in this assessment would be undertaken in accordance with the mitigation measures identified in the EIS and PIR and the conditions of approval.

2.0 Description of proposed development/activity/works

Describe ancillary activities, duration of work, working hours, machinery, staffing levels, impacts on utilities/authorities, wastes generated or hazardous substances/dangerous goods used.

The Sydney Metro Chatswood to Sydenham Environmental Impact Statement (EIS) detailed the utility supply and relocation routes as they were known at the time. Detailed design carried out since determination of the EIS has resulted in changes to some of the utility works scope, including altered alignments and requirements for additional services relocation. Works are required for the following utilities:

- Low voltage (LV) and High voltage (HV) power
- Communication services

The following details the proposed site alignments for the above listed utilities, illustrated in Appendix B

Martin Place (including Bligh Street)

The concept design provided in the EIS for Martin Place HV supply has been further developed and rationalised, allowing the supply to be sourced from a local feeder and significantly reducing the trenching required. A revised construction staging has also required the change in point of supply which will now be constructed on the site at 33 Bligh Street. This supply would be provided from an existing HV feeder on Hunter Street and would require approximately 60m of trenching from Hunter Street and along Bligh Street footway.

Further HV and LV reticulation is required for the Martin Place site to enable the decommissioning of two existing substations, sub 395 located beneath Martin Place shopping circle and sub 2853 located in the basement of 39 Martin Place. Reticulation includes trench conduit and cable installation along Elizabeth Street east and west footpaths and in the roadway between Hunter Street and 80m south of Martin Place.

An existing bank of 20 Ausgrid conduits requires relocation from within the excavation footprint. The proposed relocation route extends from a pit within the roadway of Castlereagh Street near Martin Place and would require trenching, conduit and cable installation across the road, along the north Martin Place and into a pit on Elizabeth Street.

In addition to the Ausgrid services, there are various communications services that require relocation; these would be relocated in a similar alignment to the 20 Ausgrid conduits noted above on Castlereagh Street and Elizabeth Street.

Overall the amount of required trenching works are reduced along Hunter Street from what was assessed in the EIS. New impacts associated with the power relocation work are predominantly localised around Elizabeth street and Martin Place.

Pitt Street

The concept design for Pitt Street HV supply detailed in the EIS has been further developed and rationalised allowing supply to be drawn from feeders in the intersection of Pitt & Park St. This new design has substantially reduced the overall trenching required to achieve the supply to approx. 30m.

3.0 Timeframe

When would the proposed change take place? For how long?

There would be no change to the project program as a result of this assessment. Works are expected to occur between December 2017 and June 2018.

Approved, standard working hours for the Project are as follows:

- 07:00 – 18:00 Monday to Friday
- 08:00 – 13:00 Saturdays
- No works Sundays or Public holidays

Out of hours works are required due to impacts on the road network and would be undertaken in accordance with the Project Planning Approval Conditions and Environmental Protection Licence No. 20971.

Note works are due to commence providing relevant approvals are obtained 27th December 2017 – 31st December 2017 at 5am and will recommence on 2nd January 2018 at 5am , there will be no direct impact associated with the works in relation to festive activities.

4.0 Site description

Provide a description of the site on which the proposed works are to be carried out, including, Lot and Deposited Plan details, where available. Map to be included here or as an appendix. Detail of land owner.

Utility relocation works are proposed within 5 new locations between Martin Place, Pitt Street and Bligh street sites

Martin Place

Martin Place is a major urban public open space within the heart of the Sydney Central Business District, and it provides an important pedestrian connection between George Street and Macquarie Street.

The Martin Place Utility works are located within three trenches along Martin Place, Elizabeth Street and Hunter Street. Works would be within the paved area of Martin Place and the road and road reserve areas of Elizabeth Street and Hunter Street. See Appendix B.

Bligh St

Bligh Street works are located along Bligh Street in the road and road reserve and Hunter Street in the road and road reserve. See Appendix B.

Pitt St

Pitt Street works are located within the midtown precinct of the Sydney Central Business District, with a strong retail focus, as well as a mix of commercial, residential and civic buildings. Utility works are planned along Pitt Street in the road at the point of intersect with Park adjacent to the Pitt Street North Construction site. These works are consistent with the Utilities identified and assessed under the EIS. See Appendix B.

5.0 Site Environmental Characteristics

Describe the environment (i.e., vegetation, nearby waterways, land use, surrounding land use), identify likely presence of protected flora/fauna and sensitive area.

The Utility work sites are located around the Martin Place, Pitt Street station and Bligh Street. These utility augmentations works are located around the assessed and approved Martin Place and Bligh Street and Pitt Street station works and currently comprises construction sites.

Martin Place

Martin Place is a major urban public open space within the heart of the Sydney Central Business District, and it provides an important pedestrian connection between George Street and Macquarie Street. Two areas form the proposed Martin Place Station construction sites. The northern site comprises land bounded by Castlereagh, Hunter and Elizabeth Streets. The southern site is located on the southern side of Martin Place in land between Castlereagh and Elizabeth Streets.

The Martin Place Utilities are located around Elizabeth Street, Hunter Street and Martin place within the CBD district and Martin Place precinct.

Part of the works are adjacent to State and Local Heritage building Commonwealth Bank of Australia including interior *(L1895 and D1427) and Local Heritage area areas Former “MLC” building including interior.*(L1894). Works would be within locally listed heritage area of Martin Place *(L1889).

There are no protected flora and/or fauna identified in the area of the proposed Utility locations.

Register of Heritage areas in proximity to the proposed utility site alignments, illustrated in Appendix C

The approved EIS in table 14-14 assessed that there would be minor Demolition impacts and moderate view and vistas impacts on heritage item L1895, this is consistent as utility works will not have a direct impact on this item, Other local heritage areas L1894 and L18889 have been assessed within the EIS with a minor views and vistas impacts, and a moderate direct physical impact on Martin place L18889 from cut and cover excavation within the station concourse, hence there is no change to the impacts stated as a result of the temporary utility works in these areas. All reinstatement would be required to meet City of Sydney standards and be returned to their pre-existing condition.

See attached Heritage Memo regarding Bennelong Stormwater Channel No 29. Produced by AMBS (Appendix D).

Pitt Street

Pitt Street station is located within the midtown precinct of the Sydney CBD, with a strong retail focus as well as a mix of commercial, residential and civic buildings. Located between Pitt Street and Castlereagh Street, and near Park Street and Bathurst Street, in the City of Sydney LGA. The station box is predominantly located below the city block bounded by Pitt, Castlereagh, Park and Bathurst Streets. Station access points are to be located on Park and Bathurst Streets. The site location for this analysis is based on the total area encompassed by the construction site. Tunnelling work at depth for the construction of pedestrian connections and adits (which involve deep horizontal excavation that do not impact the ground surface where archaeological deposits would remain), have not been included in this analysis.

There are no protected flora and/or fauna identified in the area of the proposed Utility locations.

Bligh Street

Bligh Street site utilities will be situated on Bligh Street and Hunter Street with a reduced scope and disruption on Hunter Street.

The Works are planned to intersect a Local Heritage area Richard Johnson square including monument and plinth (L1673) and adjacent to State Heritage area “MLC” building including interior *(L1675). On Hunter Street the works would utilise existing conduits from the Ausgrid network which currently intersect with Bennelong stormwater channel No.29 on Bligh Street, no excavation (trenching) works will take place across the 3M curtilage.

There are no protected flora and/or fauna identified in the area of the proposed Utility locations.

Register of Heritage areas in proximity to the proposed utility site alignments, illustrated in Appendix C.

The approved PIR assessed that there would be minor view and vistas impacts and direct impacts from neutral (vibration) on Heritage item L1673 during construction and temporary utility works (Table 3-14 in the PIR). Similarly there is no change to the impact on state heritage item L1675 as per the PIR.

The Construction Noise and Vibration Impact Statement (CNVIS), has assessed the vibration risks associated with Bennelong Stormwater Channel No 29 and Richard Johnson Square (L1673), and will manage potential vibration impacts as per the Construction Noise Vibration and Management Plan (CNVMP).

See attached Heritage Memo regarding Bennelong Stormwater Channel No 29. Produced by AMBS (Appendix D) and Bligh Street HV electrical Heritage Memo produced by AMBS illustrated in Appendix E.

Clarification: No trenching works are to be undertaken within 5m of the Bennelong Stormwater drain as existing trenches are utilised in this location.

All utilities would be installed using this methodology:

- Saw cutting of the trench profile
- Excavation to the required depth (excavation may be by excavator, vacuum truck or hand tools depending on the surrounding services)
- Trench protection installed if required, ie benching or box shoring
- Plastic conduits laid and joined

- Material returned back into the trench as per Ausgrid standards
- Reinstatement of the road or paved surface as per Ns130 & local council (i.e. City of Sydney) standards (Appendix F)
- Cables would later be pulled through the conduits and connected into the network
- Cables would then be commissioned

6.0 Justification for the proposed works

Address the need for the proposed works, whether there are alternatives to the proposed works (and why these are not appropriate), and the consequences with not proceeding with the proposed work.

Utilities are required to be adjusted, relocated and / or protected where they would otherwise be impacted by construction for the approved Sydney Metro Chatswood to Sydenham State Significant Infrastructure. The routes for the utility adjustments were not known at the time of writing the EIS and have since been determined through detailed design.

These works have been proposed to be undertaken during Christmas / New Year shut down period to reduce a the significant disruption to existing traffic facilities within these areas, while volumes of traffic and pedestrians are at their lowest.

7.0 Environmental Benefit

Identify whether there are environmental benefits associated with the proposed works. If so, provide details:

The environmental benefit of this utility augmentation includes the provision of essential services to the construction worksite and continuation of existing services through relocation and protection works. More specifically, the connection of electricity services reduces emissions from diesel powered generators, which would otherwise be required for construction.

These works additionally would offer a substantial community benefit by undertaking them during the holiday period we can offer less disruption to local stakeholders, the business district and members of the public, enabling safer access to the sites.

8.0 Control Measures

Will a project and site specific EMP be prepared? Are appropriate control measures already identified in an existing EMP?

A site-specific EMP would not be prepared for this scope, as the proposed works would be managed in accordance with the TSE Works Construction Environmental Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002010) and the Project Planning Approval Conditions.

9.0 Climate Change Impacts

Is the site likely to be adversely affected by the impacts of climate change? If yes, what adaptation/mitigation measures would be incorporated into the design?

This scope would not directly be impacted by climate change.


10.0 Impact Assessment – Construction

Attach supporting evidence in the Appendices if required. Make reference to the relevant Appendix if used.




Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	<p>Martin Place There may be an impact to planted street trees (Elizabeth street), as a result of the works assessed in this consistency assessment, namely works in the root zone or trimming required for machine clearance for trenching works associated with the utility trenches. Trees in close proximity to the trench alignments would be retained and protected where possible.</p> <p>Bligh St No Change to from EIS</p> <p>Pitt St No Change to from EIS</p>	Tree trimming and removal would be undertaken in accordance with the Tree Report to be prepared under Condition E6, and the Construction Flora and Fauna Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002016) would be implemented.	Y	Y	CDF
Water	No change from EIS	As per the Conditions of Approval Implementation of the Construction Soil Water and Groundwater Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002014)	Y	Y	CDF
Air quality	No change from the EIS and Modification Assessment impact	As per the Conditions of Approval Implementation of the Construction Air Quality Management Plan	Y	Y	CDF

(Uncontrolled when printed)

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
		(SMCSWTSE-JCG-TPW-EM-PLN-002018)			
Noise vibration	The proposed work would have a noise and vibration impact that extends beyond that assessed in the EIS. Construction Noise and Vibration Impact Statements have been prepared for these works. Expected noise levels have been predicted and the highest noise impacts are predicted to occur when rockbreakers and road saws are utilised in proximity to sensitive receivers. Noise mitigation and management measures have been prescribed in the CNVISs.	Works would not commence until an endorsed CNVIS is in place. The potential noise and vibration impacts associated with the proposed work would be managed in accordance with existing the Project Planning Approval and conditions of the TSE Works EPL No. 20971. Noise mitigation and management measures identified in the CNVIS would be implemented.	Y	Y	CAF
Aboriginal heritage	<p>Martin Place The EIS Technical Paper 5 Aboriginal Heritage Archaeological Assessment identified one recorded Aboriginal site is located within 100 metres of Martin Place Station. That site consists of AHIMS site 45-6-2581, the site is recorded 75 metres to the north of the station. It is likely that the AHIMS coordinates for the site are incorrect and that the site location is 200 metres west at Angel Place.</p> <p>The recorded Aboriginal site at Angel Place indicates the potential for surviving archaeological deposits in sub-surface contexts within the CBD, specifically at those locations where there are not deep sub-surface basement car parks where there is non-Indigenous archaeological potential.</p> <p>Bligh St No identified Aboriginal sites would be impacted by the proposed works at Pitt Street Station</p>	In regards to Aboriginal archaeology, the Unexpected Finds Protocol would apply.	Y	Y	CAF

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
	<p>Pitt St No identified Aboriginal sites would be impacted by the proposed works at Pitt Street Station</p>				
Historic heritage	<p>The EIS Historical Archaeological Assessment & Research (ARD) Design, Appendix H of the PIR, identified areas of archaeological significance within the project areas.</p> <p>Martin Place: The existing Martin Place station was constructed via an open cut excavation. Based on the size of the Martin Place station concourse, this excavation would have covered most of the pedestrian concourse above it between Castlereagh and Elizabeth Streets to a significant depth. The archaeological potential is described in the ARD as having Nil potential.</p> <p>Bligh Site : An underground pedestrian connection to 33 Bligh Street as approved by EIS and PIR (O'Connell Street entry). This would involve mined construction below Hunter Street and spoil extraction via 12 O'Connell Street which has two basement levels. There is no archaeological potential within the proposed Bligh Street Utilities.</p>	<p>The ARD requires site specific Archaeological Method Statements to be produced for all areas with potential archaeology.</p> <p>An Archaeological Method Statement would be produced for the site and complied with for the duration of these works</p> <p>Works within a known heritage area i.e. Martin Place (as defined by the conditions of approval) would not occur until one of following are obtained.</p> <ul style="list-style-type: none"> • OEH consultation and DP&E agreement that they are low impact works i.e. not 'construction' (see construction definition in the Approval Instrument), or • Construction definition – requiring all requirements of pre-construction to be met in accordance with COA A31. <p>The works would be carried out in accordance with the TSE Works</p>	Y	Y	

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Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
	<p>Pitt Street: Pitt Street Station is located within the midtown precinct of the Sydney Central Business District, with a strong retail focus, as well as a mix of commercial, residential and civic buildings. The local area includes a number of listed heritage items, including Sydney Town Hall, the Queen Victoria Building and St Andrews Cathedral. It is also near open spaces such as Hyde Park and Pitt Street Mall. The archaeological potential for the station which is adjacent to this site is described in the ARD as moderate to high.</p> <p>All utility works would occur within the road and road reserve, which have been previously disturbed. All disturbed areas will be restored to their previous condition following completion of works. As such, the works would not have a more than minor impact on the heritage significance of this precinct.</p> <p>See AMBS Memo provided in Appendix C</p>	<p>Construction Heritage Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002015).</p> <p>In regards to historic archaeology, the Unexpected Finds Protocol would apply.</p> <p>All works are temporary and will be restored as per Appendix F – Road opening permit guide.</p>		Y	
Community and stakeholder	Potential impact on community from traffic changes and noise. Any impact would be minor and short-term.	Affected community and appropriate mitigation measures are identified in the CNVIS.	Y	Y	
Traffic	Localised impacts during utility relocation include possible lane closures. The extent of trenching and therefore impact has been reduced from the EIS indicative layouts.	Works have been scheduled for the Christmas period to avoid peak traffic and pedestrian flow. The proposed works would be managed in accordance with existing	Y	Y	

(Uncontrolled when printed)

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
		the Project Planning Approval conditions. Where permits/licences are required, these would be obtained prior to commencement of works, in accordance with the Construction Traffic Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002013).			
Waste	Excavation for utilities works would result in a minor increase in spoil generated.	All waste generated would be classified and disposed of in accordance with the Construction Waste and Recycling Management Plan (SMCSWTSE-JCG-TPW-EM-PLN-002022).	Y	Y	COT
Social	No change from the EIS and Modification Assessment impact	No change from the EIS and Modification	Y	Y	COT
Economic	No change from the EIS and Modification Assessment impact	No change from the EIS and Modification	Y	Y	COT
Visual	There would be minor visual impacts associated with construction. These would be temporary, and disturbed areas would be restored to pre-existing conditions.	NA	Y	Y	COT
Urban design	No change from the EIS and Modification Assessment impact	NA	Y	Y	COT
Geotechnical	No change from the EIS and Modification Assessment impact As per Project Planning Approval conditions and REMMs	NA	Y	Y	COT
Land use	No change from the as per Project Planning Approval conditions and REMMs	NA	Y	Y	COT

(Uncontrolled when printed)

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Climate Change	No change from the as per Project Planning Approval conditions and REMMs	NA	Y	Y	CAF
Risk	There are no new additional risks associated with these changes.	NA	Y	Y	CAF
Other	NA	NA	Y	Y	CAF
Management and mitigation measures	There are no new mitigation measures required	NA	Y	Y	CAF.

11.0 Impact Assessment – Operation

Attach supporting evidence in the Appendix if required. Make reference to the relevant Appendix if used.

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Water	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Air quality	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Noise vibration	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Aboriginal heritage	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Historic heritage	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Community and stakeholder	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Traffic	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Waste	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Social	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Economic	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Visual	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Urban design	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Geotechnical	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Land use	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Climate Change	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Risk	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Other	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			
Management and mitigation measures	No change to the operational impacts described in the EIS and Modification Assessment impact.	Not applicable			

12.0 Consistency with the Approved Project

Based on a review and understanding of the existing Approved Project and the proposed modifications, is there is a transformation of the Project?	No. The proposed works would not transform the project. The project would continue to provide a new metro rail line between Chatswood and Sydenham.
Is the project as modified consistent with the objectives and functions of the Approved Project as a whole?	Yes. The proposed works would be consistent with the objectives and functions of the approved project.
Is the project as modified consistent with the objectives and functions of elements of the Approved Project?	Yes. The changes identified in this assessment are temporary and are consistent with the objectives and functions of the approved project.
Are there any new environmental impacts as a result of the proposed works/modifications?	No new environmental risks are outstanding. All risks would be adequately addressed through the application of the mitigation measures in the above tables.
Is the project as modified consistent with the conditions of approval?	Yes. The proposed works would be consistent with the conditions of approval.
Are the impacts of the proposed activity/works known and understood?	Yes. The impacts of the proposed works are understood.
Are the impacts of the proposed activity/works able to be managed so as not to have an adverse impact?	Yes. The impacts of the proposed works can be managed so as to avoid an adverse impact.

13.0 Other Environmental Approvals

Identify all other approvals required for the project:	<p>Works would be undertaken in accordance with EPL No. 20971</p> <p>Works within heritage areas require either OEH and DP&E concurrence that they are low impact works or works must occur after construction commencement approval (ie CEMP approval) has been given.</p> <p>Noise would be managed in accordance to the approved CNVISs</p>
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Works would not proceed prior to obtaining a reviewed and approved Tree Report (Condition E6)
Road occupancy licenses have been submitted to TMS where required and works would not commence until approved.



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Author certification

To be completed by person preparing checklist.

I certify that to the best of my knowledge this Consistency Checklist:

- Examines and takes into account the fullest extent possible all matters affecting or likely to affect the environment as a result of activities associated with the Proposed Revision; and
- Examines the consistency of the Proposed Revision with the Approved Project; is accurate in all material respects and does not omit any material information.

Name:	Georgina Luck	Signature:	
Title:	Environmental Coordinator		
Company:	TfNSW	Date:	20/12/2017

Environmental Representative Review

As an approved ER for the Sydney Metro City & Southwest project, I have reviewed the information provided in this assessment. I am satisfied that mitigation measures are adequate to minimise the impact of the proposed work.

Name:	Jo Robertson	Signature:		Please ensure the Heritage Management Plan is updated to detail the change in impacts to Richard Johnson Square (i.e. from indirect to direct) and detail appropriate mitigations to prevent inadvertent damage to this item. Please note with regards to the Bennelong stormwater channel, the CNVIS includes an obligation for consultation with Sydney Water which will need to be met if you are working within the safe working distances identified in the CNVIS.
Title:	Environmental Representative			
		Date:	21/12/17	

This section is for Sydney Metro only.

Application supported and submitted by

Name:	Craig Tucker	Date:	21/12/2017
Title:	TSE IG ENVIRONMENTAL MANAGER	Comments:	
Signature:			

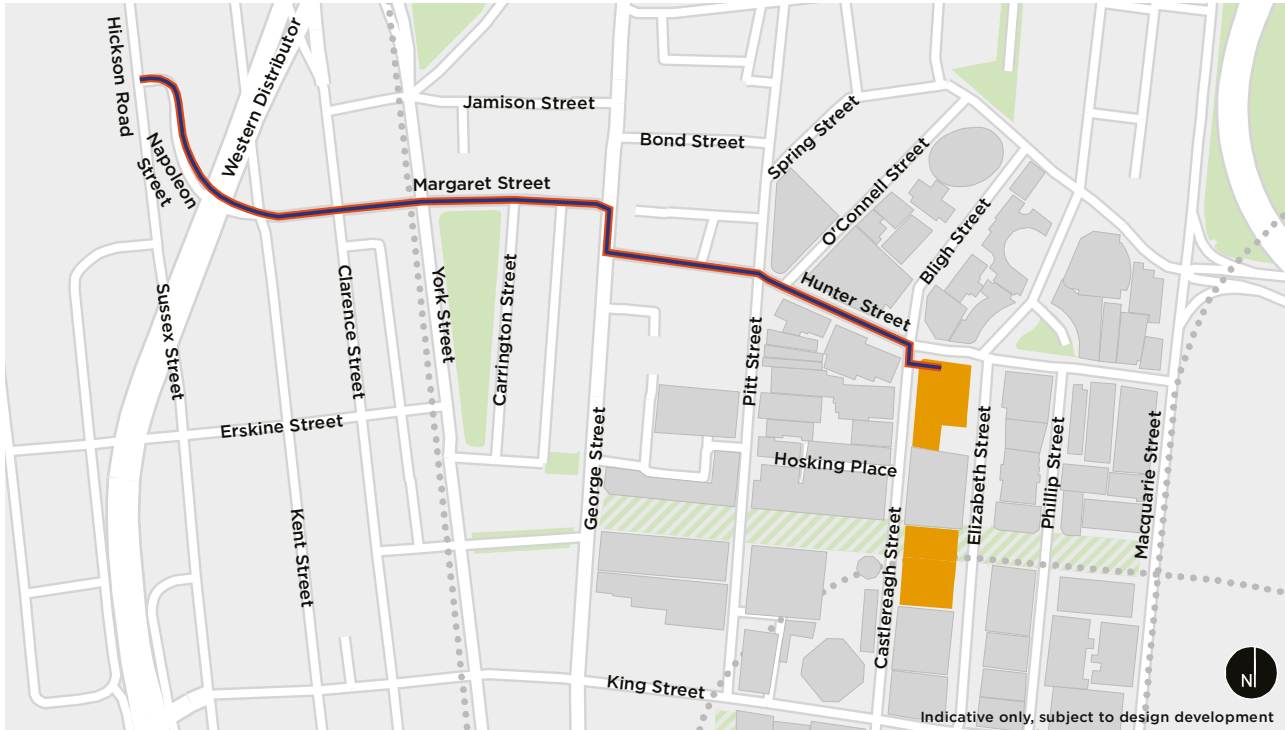
Based on the above assessment, are the impacts and scope of the proposed activity/modification consistent with the existing Approved Project?

- Yes The proposed activity/works are consistent and no further assessment is required.
- No The proposed works/activity is not consistent with the Approved Project. A modification or a new activity approval/ consent is required. Advise Project Manager of appropriate alternative planning approvals pathway to be undertaken.



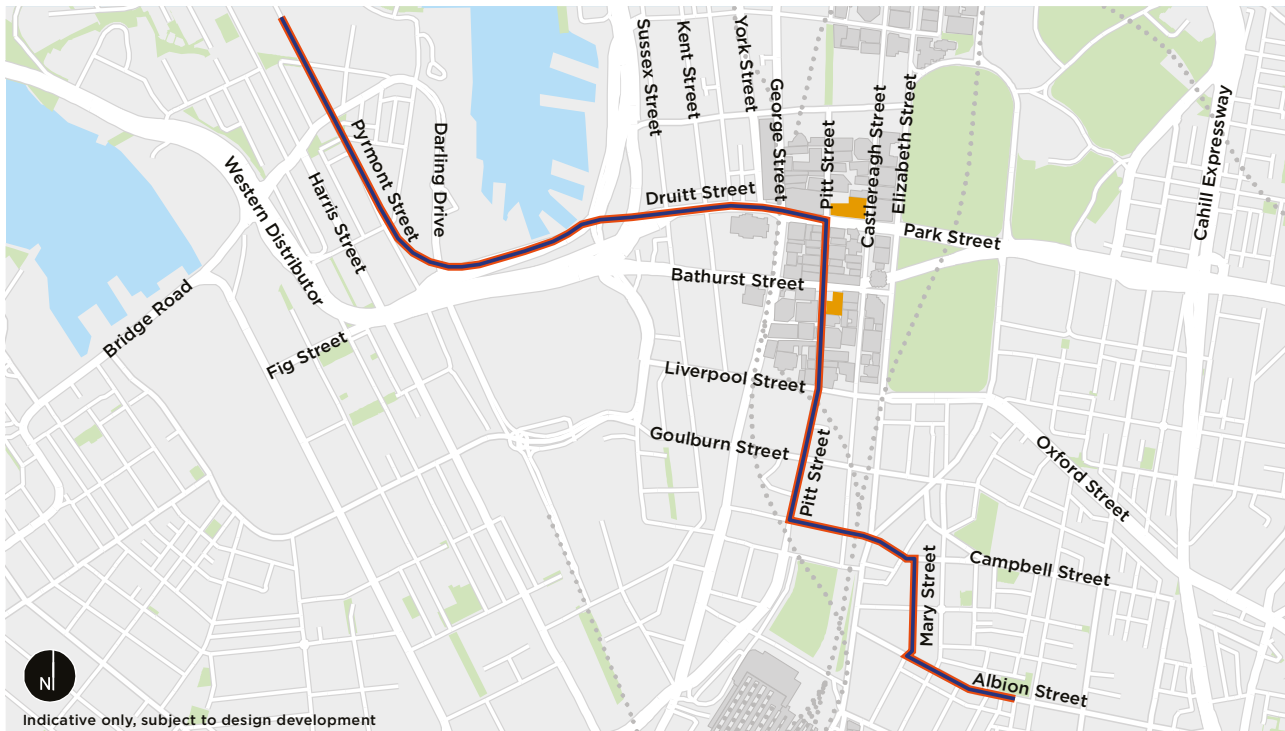
Endorsed by			
Name:	Fil Cerone	Date:	22/12/17
Title:	Principal City & Southwest, Sustainability, Environment & Planning	Comments:	Contractor to comply with approved Revised Environmental Mitigation Measure NAH12
Signature:			

Appendix A – Map of utility works assessed in the Sydney Metro City & Southwest Environmental Impact Statement



KEY
 Proposed construction site area
 Power supply route
 Existing suburban rail

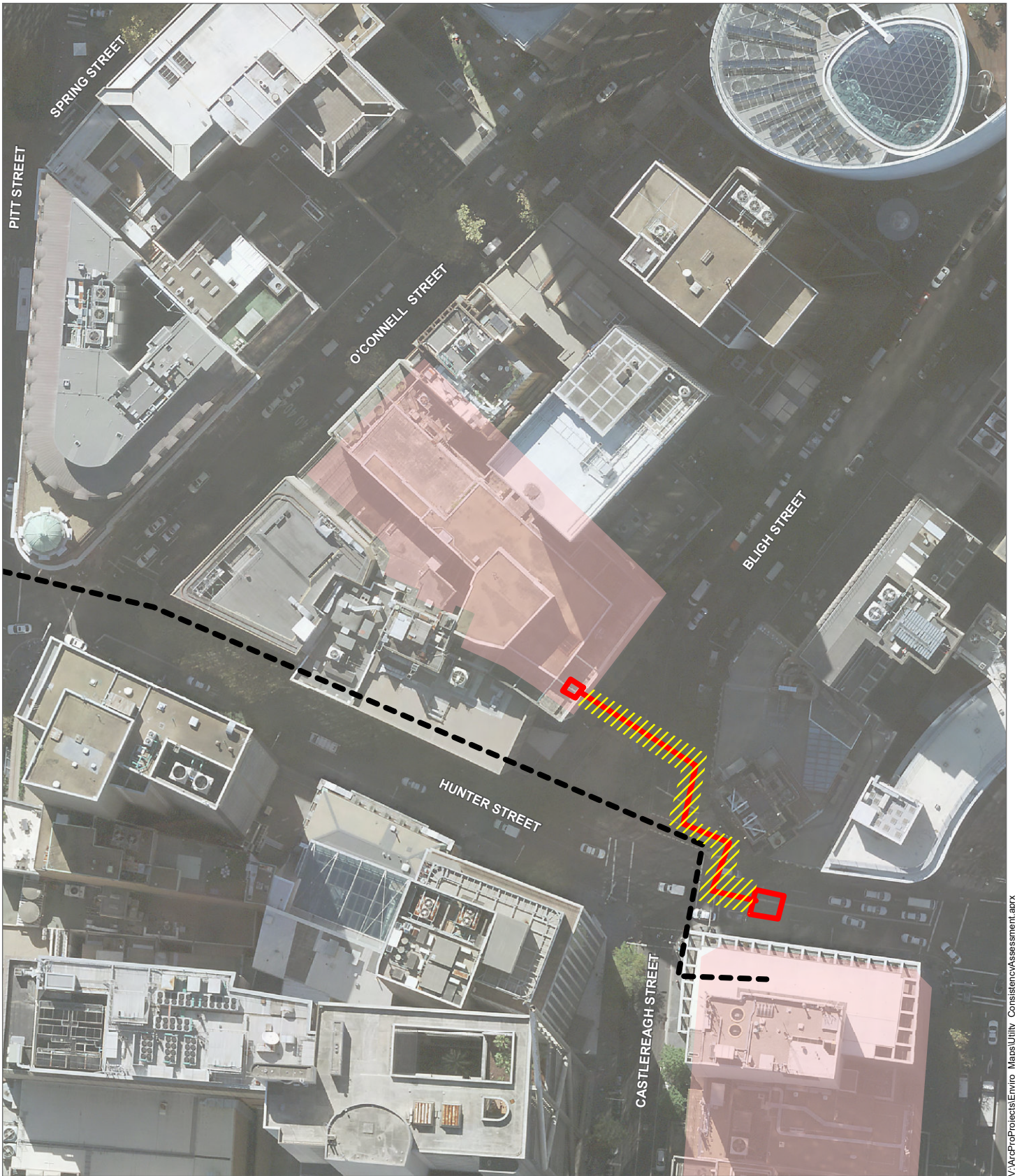
Figure 7-27 Martin Place Station – power supply route



KEY
 Proposed construction site area
 Power supply route
 Existing suburban rail

Figure 7-28 Pitt Street Station - power supply route

Appendix B – Map of rationalised utility works – Martin Place, Bligh Street and Pitt Street



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Legend



- Indicative Site Boundary
- Trenching
- HV/LV Power Supply Route EIS Proposed
- HV/LV Power Supply Route Relocation

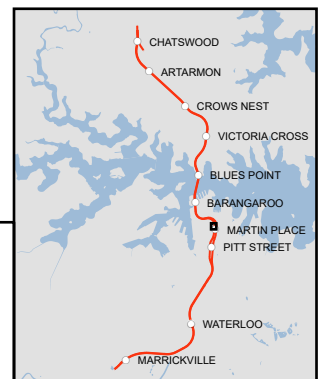
SYDNEY METRO CITY & SOUTHWEST -TSE WORKS

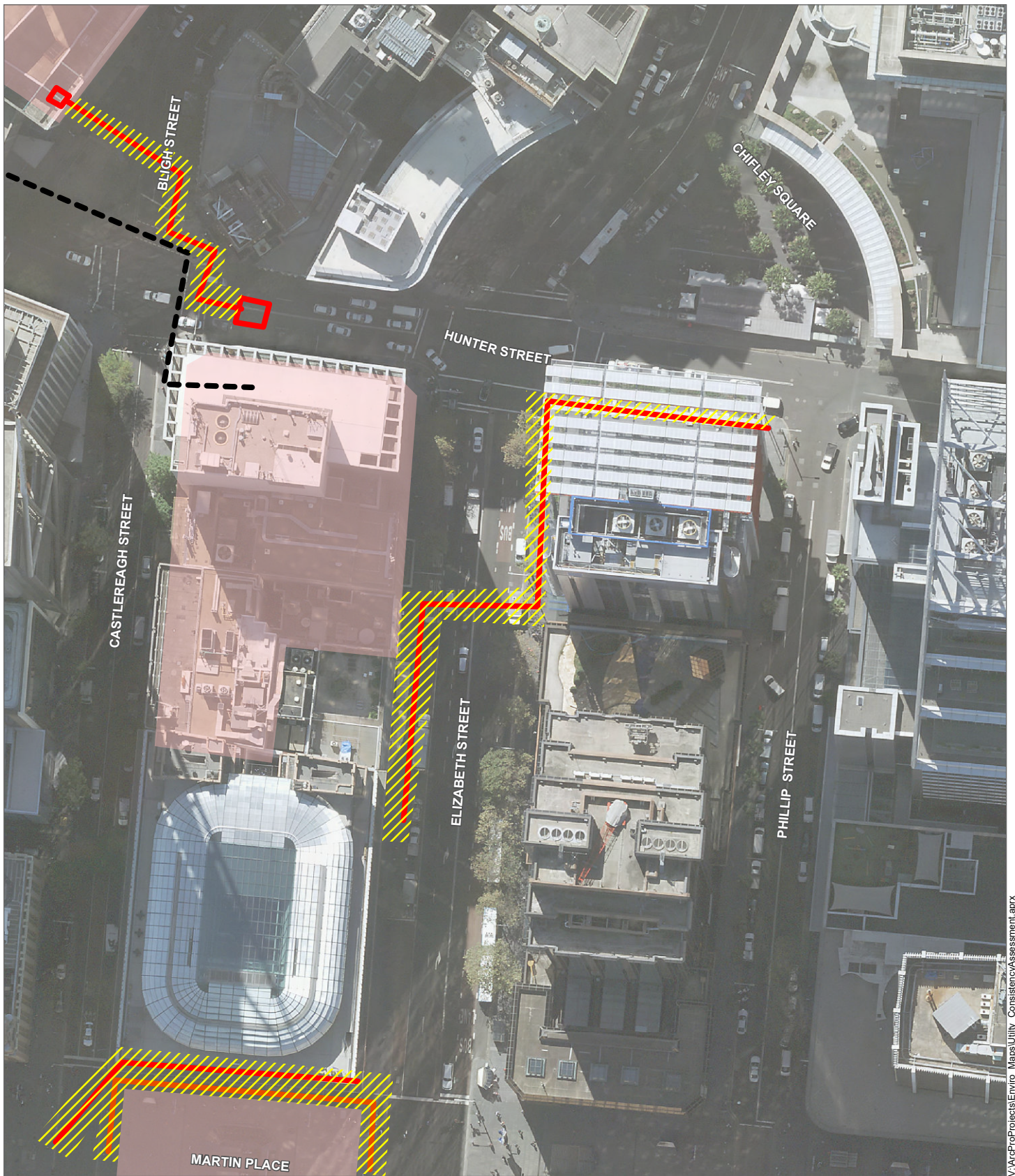
Utility Works- Consistency Assessment

Martin Place Bligh Street Worksite



Sketch 8 of 14





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Legend



- Indicative Site Boundary
- Trenching
- Combined Services
- HV/LV Power Supply Route EIS Proposed
- HV/LV Power Supply Route Relocation

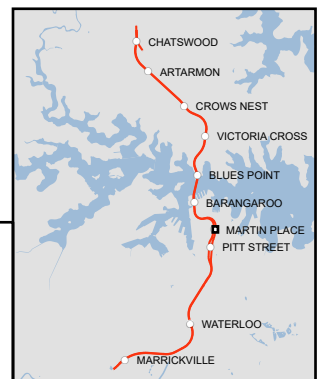
SYDNEY METRO CITY & SOUTHWEST -TSE WORKS

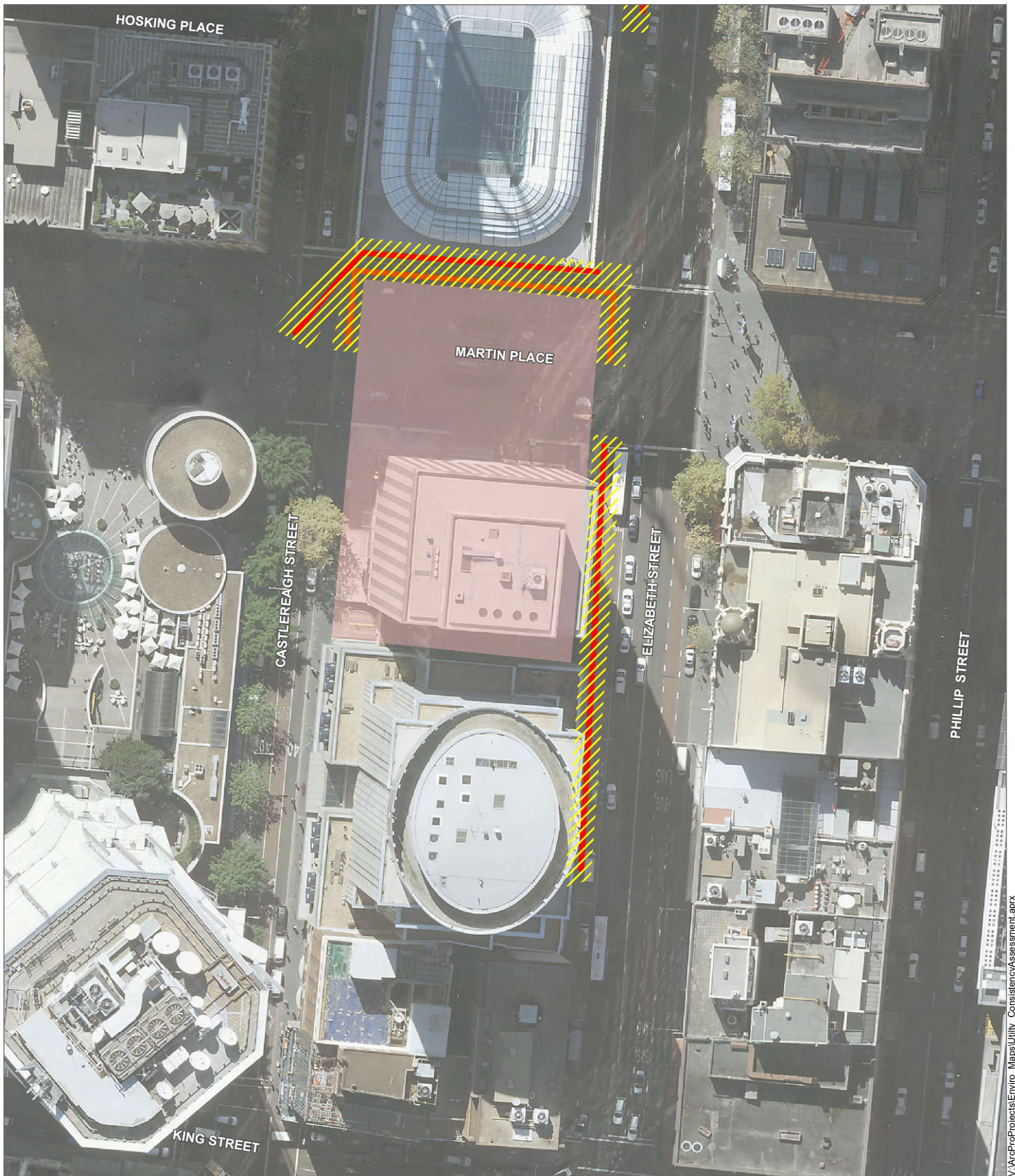
Utility Works- Consistency Assessment

Martin Place North Worksite



Sketch 9 of 14





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Legend



- Indicative Site Boundary
- Trenching
- Combined Services
- HV/LV Power Supply Route Relocation

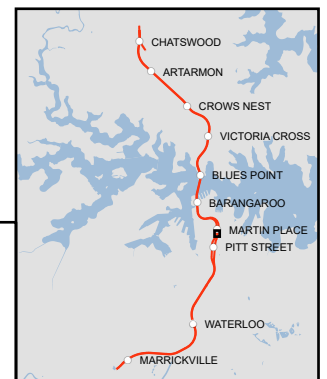
SYDNEY METRO CITY & SOUTHWEST -TSE WORKS

Utility Works- Consistency Assessment

Martin Place South Worksite



Sketch 10 of 14





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Legend



- Indicative Site Boundary
- Trenching
- HV/LV Power Supply Route EIS Proposed
- HV/LV Power Supply Route Relocation

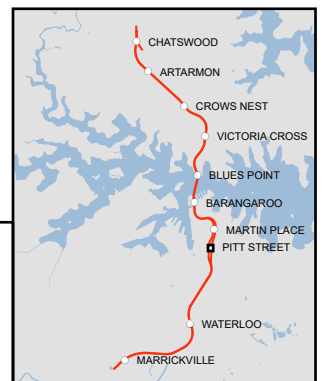
SYDNEY METRO CITY & SOUTHWEST -TSE WORKS

Utility Works- Consistency Assessment

Pitt Street North Worksite



Sketch 11 of 14





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Legend



Indicative Site Boundary

HV/LV Power Supply Route EIS Proposed



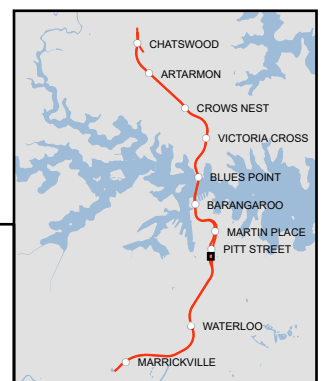
SYDNEY METRO CITY & SOUTHWEST -TSE WORKS

Utility Works- Consistency Assessment

Pitt Street South Worksite



Sketch 12 of 14



Appendix C – Heritage Listings within proposed Utility site alignments – Martin Place and Bligh Street

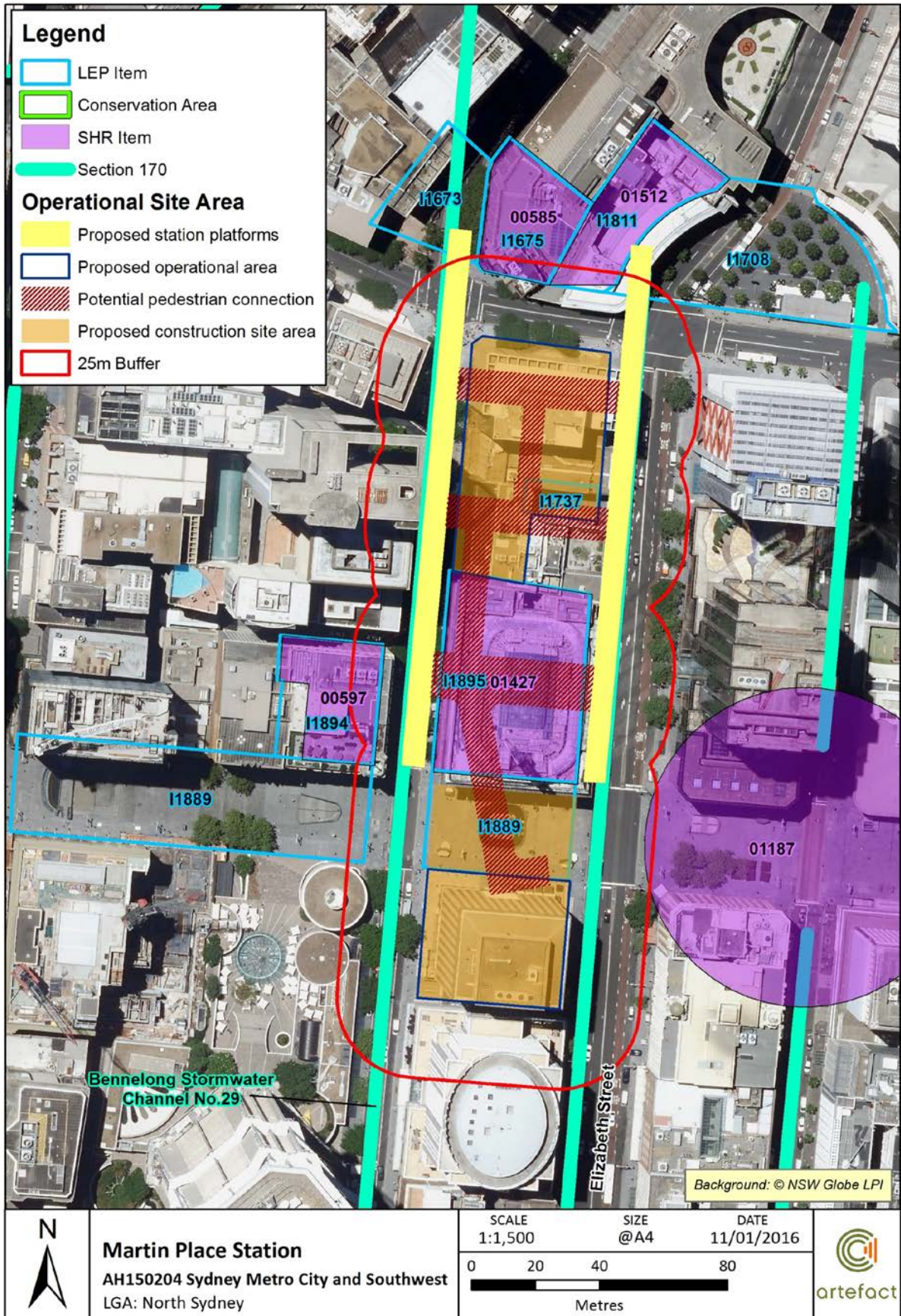
6.8.3 Heritage listed items

The following table outlines the heritage listed items within the study area which are shown in Figure 121.

Table 65: Overview of heritage items within the Martin Place study area

Heritage item	Register listings	Significance	Relationship to study area
Richard Johnson Square including monument and plinth	Sydney LEP 2012 I1673	Local	Partially within buffer zone
Former “MLC” building including interior	State Heritage Register 00597 Sydney LEP 2012 I1894	State	Partially within buffer zone
Former “City Mutual Life Assurance” building including interiors	State Heritage Register 00585 Sydney LEP 2012 I1675	State	Partially within buffer zone
Former “Qantas House” including interiors	State Heritage Register 01512 Sydney LEP 2012 I1811	State	Partially within buffer zone
Chifley Square	Sydney LEP 2012 I1708	Local	Partially within buffer zone
Commonwealth Bank of Australia including interior	State Heritage Register 01427 Sydney LEP 2012 I1895	State	Adjacent to construction area, within buffer zone
Flat building including interior	Sydney LEP 2012 I1737	Local	Within construction area
Martin Place	Sydney LEP 2012 I1889	Local	Partially within construction area and buffer zone
Martin Place Railway Station	State Heritage Register 01187 Sydney LEP 2012 I1891 Sydney Trains S170	State	Partially within construction area
Bennelong Stormwater Channel No.29	Sydney Water S170	Local	Partially within buffer zone

Figure 121: Heritage items within Martin Place study area.



**Appendix D – AMBS Historic Heritage Memo – Bennelong Stormwater Channel
No 29**

MEMO



DATE: 28 September 2017

AMBS Ref: 16314M Bennelong (170928)

TO: Caitlin Richards, Approvals, Environment & Sustainability Manager, JHCPBG

FROM: Jennie Lindbergh, AMBS Director Historic Heritage

SUBJECT: Bennelong Stormwater Channel No 29

The Sydney Metro City & Southwest Chatswood to Sydenham Metro was approved as a State Significant Development (SSD) on 7 January 2017. The Minister's Conditions of Approval (CoA) that are relevant to the protection of heritage is:

E10 *The Proponent must not destroy, modify or otherwise physically affect any Heritage item not identified in documents referred to in Condition A1.*

The Martin Place Station comprises two sites, north and south. An underground tunnelling support site is to be located at 33 Bligh Street with a tunnel decline constructed into the Martin Place station tunnels (Figure 1). Tunnelling for the station will be at a depth of 24–32m below street level and rock bolting will be required to ensure the stability of the tunnel walls. The route of the tunnels from Bligh Street will be near to the heritage curtilage of the Bennelong Stormwater Channel (SWC) (Figure 2). The Bennelong SWC was constructed 1856–1857 and is listed on the Sydney Water Heritage & Conservation (Section 170) Register as local heritage item 4570854. The rock bolts and/or spiling bars will be 3m–5m in length with a diameter of between 24mm and 32mm grouted into 45mm–60mm diameter holes. The rock bolts and/or spiling bars will be spaced and aligned out of the tunnel such that some will be horizontal to the Bennelong SWC within an approximate distance of 1.5m–2.0m (Figure 3).

Currently the detailed design has not been finalised and scoping investigations are continuing. Once engineering and predicted effects have been determined, JHCPBG will consult with all relevant parties, including Sydney Water and AMBS to ensure that the structural integrity and fabric of the Bennelong SWC is protected from impacts.

JHCPBG is aware of the heritage constraints associated with the proposed work and will ensure compliance with Condition E10.

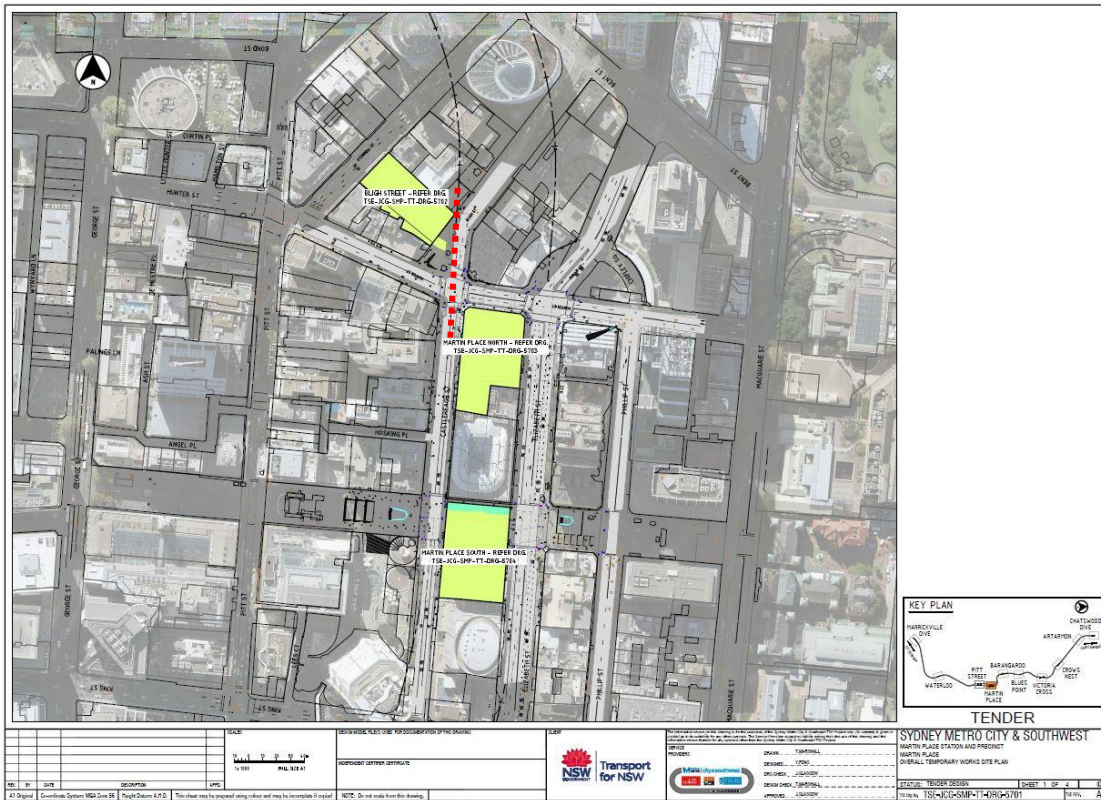


Figure 1 Martin Place Station: Overall Temporary Works Site Plan. The relevant section of the Bennelong SWC is indicated in red



Figure 2 The physical curtilage plan for Bennelong Stormwater Channel No.29 (Sydney Water Heritage Inventory). Bligh Street is arrowed.

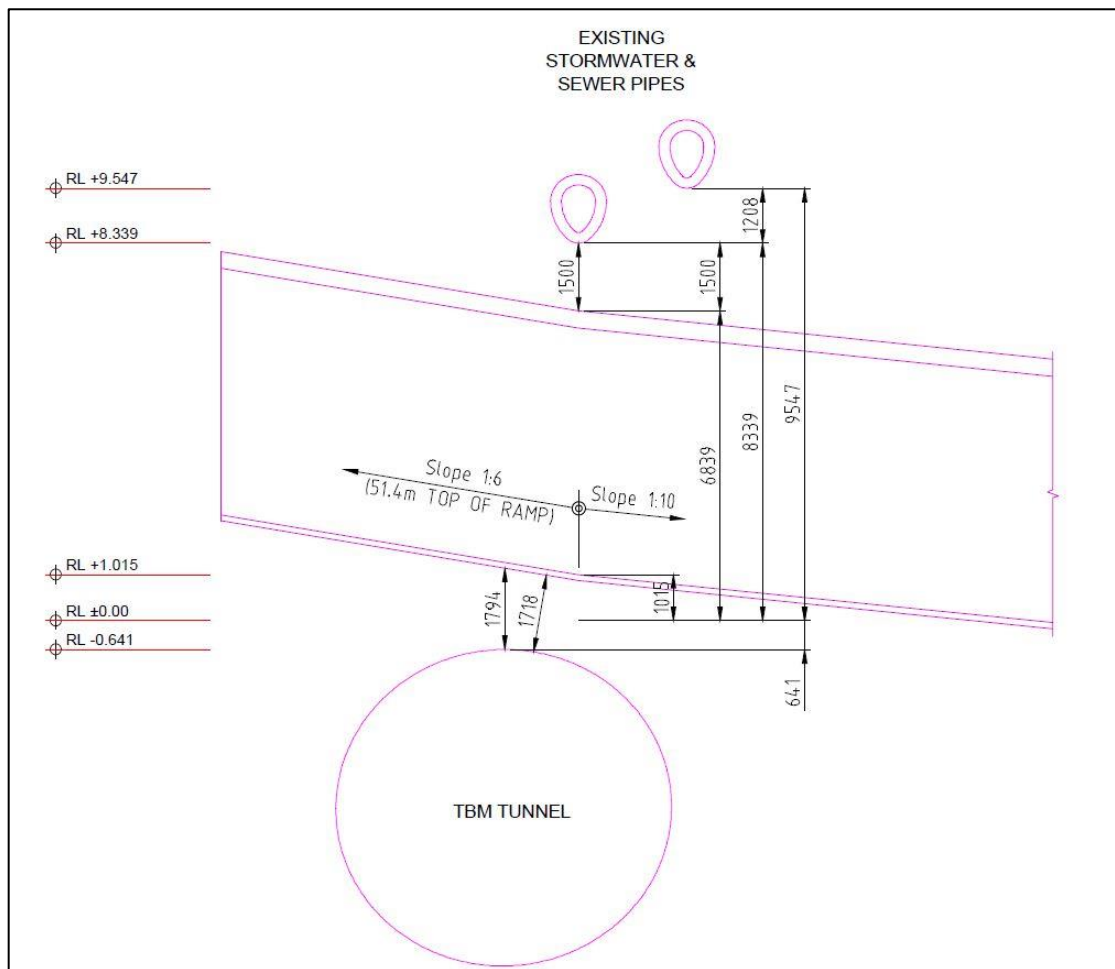


Figure 3 Schematic illustrating the likely relationship between the TBM tunnel and Bennelong SWC.

Appendix E – AMBS Historic Heritage Memo – Bligh Street HV

MEMO



DATE: 19 December 2017

AMBS Ref: 16314M Bligh Street

TO: Robert Muir, Senior Environment Coordinator Sydney Metro JHCPBG JV

FROM: Jennie Lindbergh, AMBS Director Historic Heritage

SUBJECT: Bligh Street HV electrical

The Sydney Metro City & Southwest Chatswood to Sydenham Metro was approved as a State Significant Development (SSD) on 7 January 2017. The Minister's Conditions of Approval (CoA) that is relevant to the protection of heritage is:

***E10** The Proponent must not destroy, modify or otherwise physically affect any Heritage item not identified in documents referred to in Condition A1.*

The Martin Place Station comprises two sites, one of which is at 33 Bligh Street (Figure 1). An underground tunnelling support site is to be located at 33 Bligh Street with a tunnel decline constructed into the Martin Place station tunnels. The 33 Bligh Street site has been a deep excavation for some time, having been earmarked for development by Ausgrid.

High Voltage Electrical Cabling

The PIR assessed and approved an additional underground pedestrian link which brought in the 33 Bligh Street site. Whilst not shown on a layout, utilities, including high voltage (HV) electrical cables, are required to be provided for the excavation of the shaft and underground pedestrian link. Existing abandoned electrical cables will be removed and new HV cables connected to the existing kiosks on Bligh Street and the corner of Bligh and Hunter Streets (Figure 2). Trenches, where existing abandoned cables are to be removed, will be re-used for new cables, in addition all cables will be laid in existing asbestos conduit bank where they will be replaced with modern PVC equivalents (Figure 3).

The area around Bligh and Hunter Streets is within early colonial Sydney with a street layout by 1807, which had however, changed by 1822 when it reflects the modern layout (Figure 4 and Figure 5). Re-using existing trenches means that there is negligible potential that relics associated with early housing will be exposed. As such, the Unexpected Heritage Finds procedure should be implemented.

JHCPBG is aware of the heritage constraints associated with the proposed work and will ensure compliance with Condition E10.

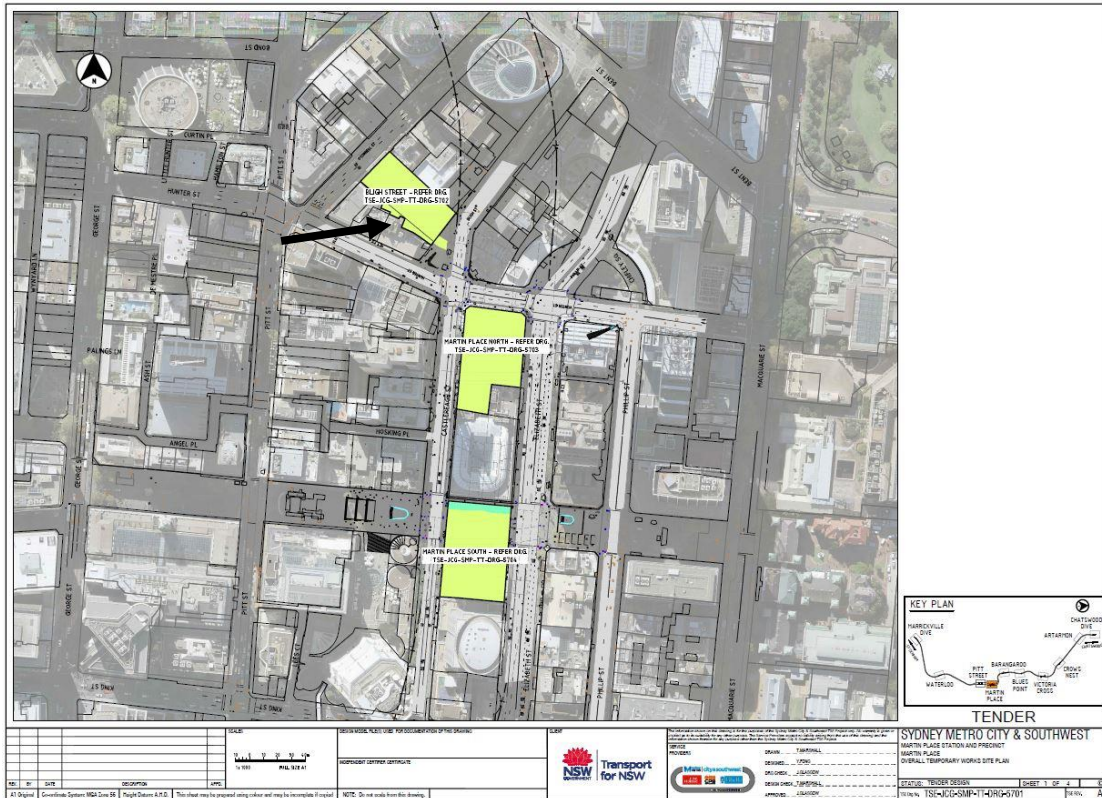


Figure 1 Martin Place Station: Overall Temporary Works Site Plan. The 33 Bligh Street site is arrowed.

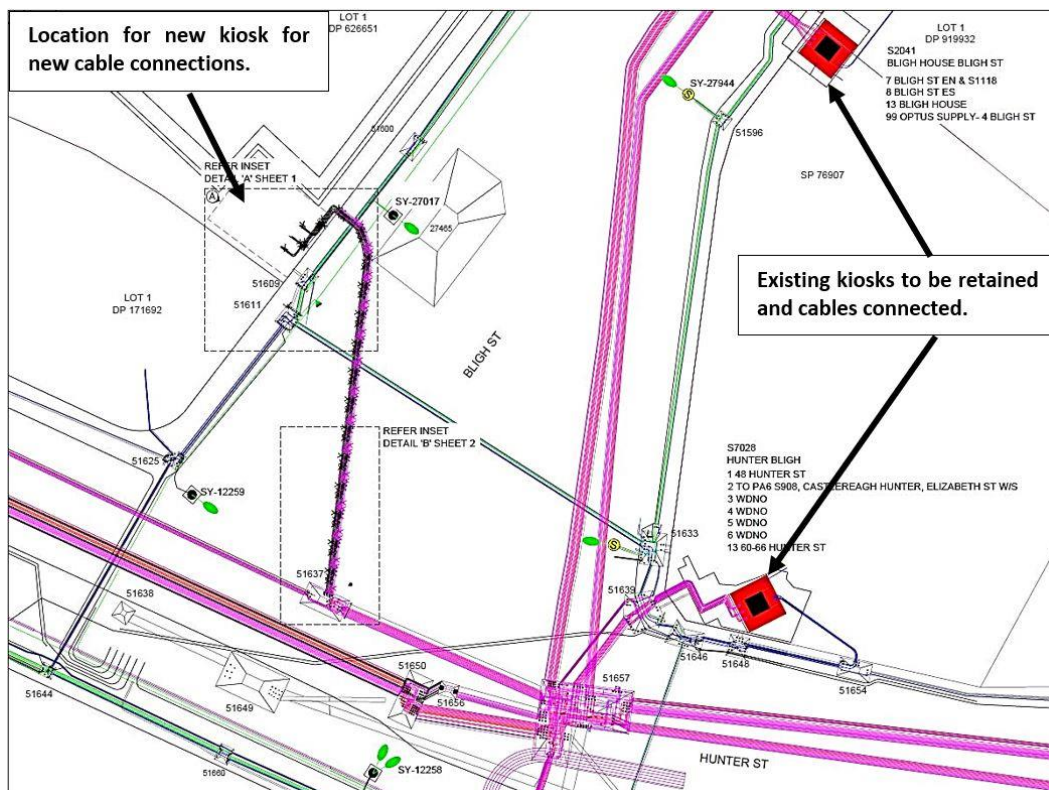


Figure 2 Layout plan for the HV cabling. Black identifies existing abandoned cables and new cables are pink.

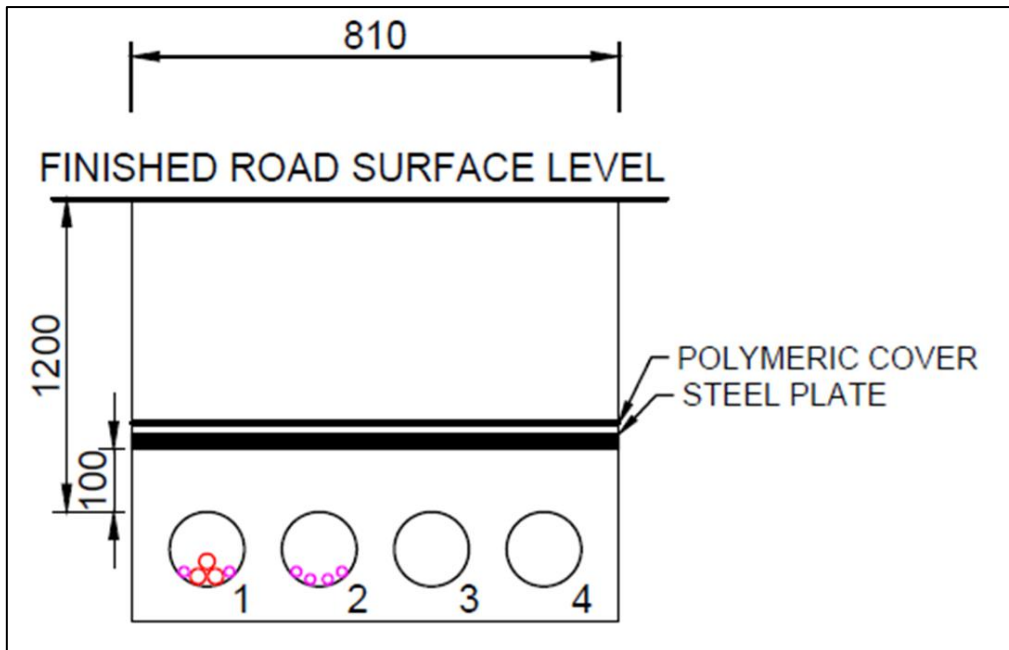


Figure 3 Indicative elevation of cable trench. The maximum depth approximately 1400mm.

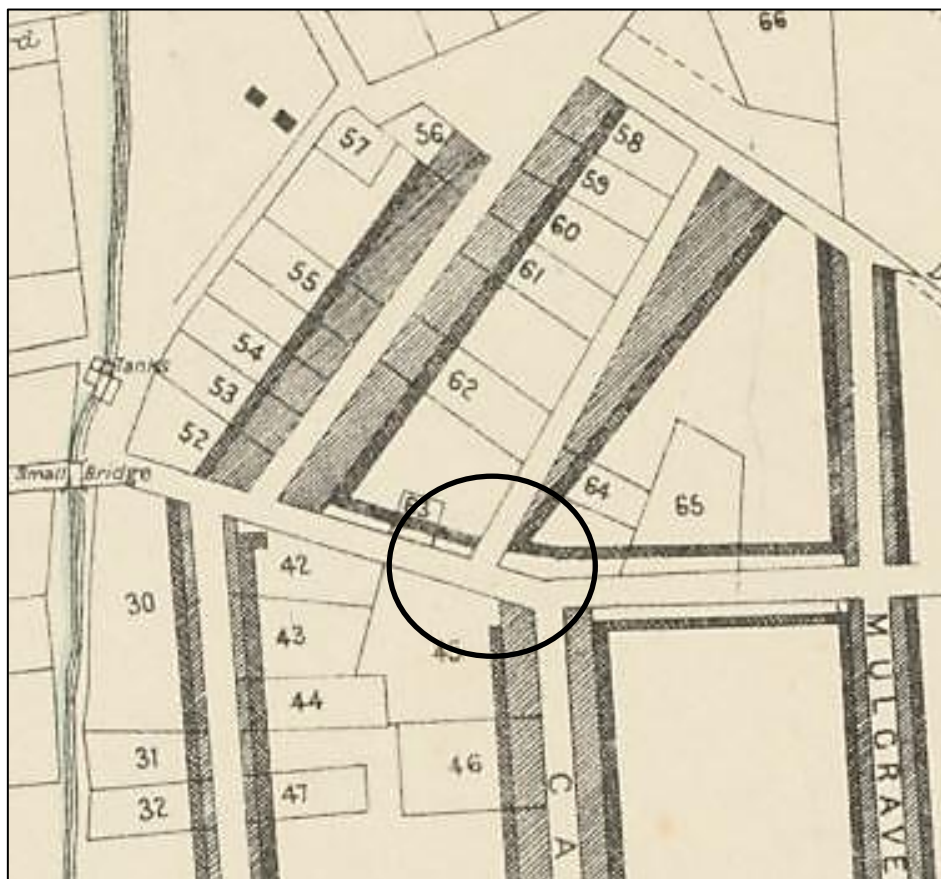


Figure 4 Detail from James Meehan's 1807 Plan of the town of Sydney in New South Wales. The intersection of Bligh and Hunter Streets is within the circled area (<https://nla.gov.au/nla.obj-229911438/view>).



Figure 5 Detail from Harper's 1822 Plan of the town and suburbs of Sydney (<https://nla.gov.au/nla.obj-229911701/view>).

Appendix F – Road opening permit Guide – Reinstatement

Road Opening Permit Guide

About this form

The Road Opening Permit Guide is an explanation of the conditions placed on road opening works occurring on council roads/footpaths in the City of Sydney Council local government area.

IMPORTANT NOTE: The Road Opening Permit Guide needs to be read in conjunction with the Road Opening Permit application form.

What is a Road Opening Permit?

A Road Opening Permit (ROP) in accordance with Section 138 of the Roads Act is used to apply for a permit to carry out any intrusive digging in the public road or footpath. The type of works covered by this application:

- Installation, maintenance, repairs/replacement or upgrading of utilities (water, gas, electricity or telecommunications)
- Any type of stormwater or sewer connection works and repairs
- Providing a temporary driveway/layback to premises for residential or construction vehicle access
- Upgrading the road, kerb & gutter associated with a development site approved by the City of Sydney Council.

This application form is **not** used for:

- Adding permanent driveways or replacing redundant driveways with new footpaths, these require a 'Driveway Application' form be completed.

Download a [Driveway Application form](#) from the City's website.

Who can apply?

Anyone who needs to carry out works can apply for a Road Opening Permit, including home owners or trades people. If the permit is approved, only fully qualified and licenced trades people can carry out the works. All qualification and licence details must be included in the application.

All financial transactions associated with this approval will be conducted between the City of Sydney and the 'Billable Party' listed on the application and the permit.

Things to know before applying for your permit

- Applications must be submitted a minimum of 48 hours (2 working days excluding weekends & public holidays) prior to the proposed start of works.
- A single application can be used for multiple frontages **of the same site**.
- All works being carried out under this approval must be carried out by qualified and licenced trades people whose registration / accreditation details are essential and must be provided at the time of submitting your application.
- **Works during December** - works during the month of December are restricted due to extended trading hours & school holidays leading up to Christmas & New Year. View a map showing exclusion zones and the applicable dates at <http://www.cityofsydney.nsw.gov.au/development/building-and-construction-approvals/construction-regulation> All applications submitted for works during December will be treated on a case by case basis.
- All approvals issued by the City of Sydney Council are subject to the standard conditions for road opening works. These conditions can be found on page 3 of this guide.
- In addition to the standard conditions mentioned above, site specific conditions based on location and type of works required will be included. These may include but not be limited to:
 - * Approved work days and hours of work
 - * Traffic & pedestrian controls required for the works
 - * Notification for the adjoining premises
 - * The type and number of vehicles allowed at site.

Definitions

- Billable Party** - An individual or company who will pay all deposits and whose name will appear on the receipt issued by the City of Sydney Council. The Billable Party will also be the only person/party to receive restoration invoices and/or refunds associated with these works.
- Agent / Licenced Contractor** - An individual or company engaged to carry out and complete the works i.e plumber, electrician etc. This person / party will not receive invoices or refunds associated with the works.

The application and approval process

1. Read & complete the Road Opening Permit Application. Ensure all accompanying documentation is completed and attached to your application otherwise your application may be delayed.
2. Please submit your application via email to cru@cityofsydney.nsw.gov.au or in person. Please note credit card payment details are not accepted via email for privacy reasons.
3. Your application will take two business days to assess and issue.
4. You must be in possession of your Road Opening Permit before road opening works can begin.
5. Once your works are completed:

Temporary restoration - once works are completed the party / parties carrying out the works must temporarily restore the area so the work site is safe and trafficable for pedestrians and vehicles. Temporary restoration specifications can be found in the notes at the back of this guide.

Notification to the Asset Inspector - contact the Asset Inspector (details on the permit) when you're about to start your works. At the completion of your works the Asset Inspector will arrange a site visit to measure the site for permanent restoration.

Permanent restoration of the site - All permanent restoration works will be carried out by the City of Sydney Council using the deposit paid on collection of the approved permit. If the cost of restoration is less than the deposit paid, a refund can be issued to the Billable Party. Subsequently, if the cost of restoration is higher than the deposit paid the Billable party will be advised and an invoice issued.

Permanent restoration requested to be completed by Applicant - if you are proposing to do your own permanent restoration once your permitted works have been completed, you must seek approval from the City of Sydney's City Infrastructure Maintenance Unit's Project Coordinator prior to any permanent works being commenced. Customers can contact the Project Coordinator via email: restorations@cityofsydney.nsw.gov.au

Fees

All charges are in accordance with the City of Sydney Schedule of Fees & Charges for the current financial year.

NOTE: Permits must be paid for and collected before the end of the financial year (30 June).

IMPORTANT NOTE: As per NSW State Legislation, these fees do not attract a GST. Please do not add GST to these fees, they are GST free.

All charges listed are in accordance with the City of Sydney's Schedule of Fees & Charges for the current financial year.

All charges for the permanent reinstatement of the work site are the responsibility of the Billable Party listed on the Road Opening Permit Application form and whose name is listed on the receipt for payment of this application.

Additional documentation may be required

- Where required, a full traffic/pedestrian control plan, in accordance with AS1742.3 & the RMS manual for traffic control at work sites drawn by a red or orange ticket RMS accredited person.
- Dial before you Dig cover sheet including the sequence numbers for Telstra & Ausgrid/Energy Australia and City of Sydney Asset Location Response confirmation.
- Where required by a site specific DA consent condition, provide proof that a City of Sydney Works Zone application has been submitted. Proof is considered to be a City of Sydney Council issued receipt showing payment of the Works Zone application fee.
- A copy of the applicant's current public liability insurance certificate (minimum of \$10 million) indemnifying the City of Sydney Council, must be attached to this application.
- A safe work method statement &/or Safety Data Sheets for proposed works.
- Where required, list authorisation numbers and attach copies of all external approval/acknowledgements or permits i.e. Roads & Maritime Services, NSW Police, Sydney Buses.
- For public domain upgrade works, provide a copy of the Public Domain approval and evidence of bonds being held by the City of Sydney Council.
- The City requires photographic evidence of the state of the footway/road prior to commencement of your works. A minimum of three photo's showing a close up of the work area and two long shots from either end of the works are required.
- City of Sydney Asset Location documents from the Dial before you dig searches

Standard conditions of approval for Road Opening Works

1. **Approval documents** - The person/company carrying out works associated with this permit must have all approval documents and conditions from the respective consent authorities i.e. City of Sydney, NSW Police, RMS or Sydney Buses on site at all times during works and these documents must be available when requested by an authorised City of Sydney Officer, NSW Police or RMS personnel.
2. **Traffic control & pedestrian management plans** - All traffic, pedestrian management and control plans must be in accordance with AS1742.3 and the RMS manual for traffic control at work sites. All plans must be approved by the City of Sydney's Construction Regulation team and cannot be modified without prior written consent of the City of Sydney. Approved plans must be on site during all works. All traffic controllers involved in the works must have current RMS accreditation and must comply with all requirements of 'Section 8 - Traffic controllers and speed zones' of the RMS manual.
3. **Indemnity for the City of Sydney Council** - The holder of this approval shall indemnify and keep indemnified the City of Sydney Council against all claims, demand, suits, actions damages & costs incurred by or charges made against the City of Sydney Council in respect to death or injury to any person or damage in any way arising from this approval.
4. **Public Liability Insurance** - The applicant must provide a certificate of currency for a Public Liability policy of not less than \$10 Million Dollars with an insurer licenced by the Australian Prudential Regulatory Authority to operate in Australia OR with an investment grade rating from an industry recognised rating agency such as Moodies, Standard & Poors or Bests.
5. **Identifying service utilities & notifying interested parties** - The permit holder occupies the area identified at its own and sole risk and acknowledges that it is responsible for identifying any service utilities in the area of the opening and any damage caused to these services. If adjacent properties or businesses will be affected, and the work is not an emergency repair, then the adjacent property owners or businesses must be given 5 working days written notification before work starts, giving details of: Scope of works, commencement date and time, duration of works, what alternative access provisions will be available and a 24 hour contact name and phone number.
6. **Notification of commencement & completion of works** - Prior to commencement of their work, the permit holder is required to contact the City of Sydney Asset Inspector for the site area, nominated on the first page of the permit. When works are completed, the permit holder must temporarily restore the affected area and contact the asset inspector to arrange for a site visit to measure the site and determine the scope and cost of the permanent restoration to be carried out by the City.
7. **Temporary restoration** - The permit holder is responsible for the temporary restoration of the road/footpath and these works must be carried out and maintained in accordance with the City of Sydney's specifications.

8. **Permanent restoration** - The permit holder must not carry out any permanent restoration of the public way, without the prior written approval of the City of Sydney's Infrastructure Maintenance Unit's Restoration Coordinator. The Restoration Coordinator can be contacted via email address restorations@cityofsydney.nsw.gov.au .
9. **Costs** - The permit holder will pay all costs associated with temporary and permanent restoration of the road (including any services located in the road) and / or any repairs caused by or resulting from the works. The restoration will be completed to the standard identified in the City of Sydney Council specifications for the relevant area and surface type. If work is carried out by or on behalf of the City of Sydney Council, then unless otherwise notified in writing, it will be charged at the overtime rates set out in the Schedule of Fees and Charges in force at the time of application. The specifications and the Schedule of Fees and Charges are available on the City's website (www.cityofsydney.nsw.gov.au) or from any of our Neighbourhood Service Centres.
10. **Unauthorised permanent restoration** - Any permanent restorations work done without formal written approval will be excavated and reconstructed by the City and all costs charged to the permit holder in accordance with the City of Sydney Council Schedule of Fees and Charges. The City may deduct any costs associated with the rectification and reinstatement of the road (calculated in accordance with clause 3) from the bond, or deposit monies supplied at the time the permit is approved. If the amount expended exceeds that bond or deposit monies, then the City may recover any shortfall from the permit holder as a debt due and owing.
11. **Pre-existing surface treatments** - All pre-existing surface treatments matching the surrounding finishes are to be reinstated in accordance with this permit and the relevant specification for the area. The specifications are available by contacting the City of Sydney's Infrastructure Maintenance Unit's Restoration Coordinator via email address restorations@cityofsydney.nsw.gov.au .
12. **Minimum area of restoration** - The permit holder acknowledges that the minimum area for restoration is 1 square metre.
- Footpath:** works may extend beyond the actual opening (by 1 square metre / 1 lineal metre) to the nearest joint or other point reasonably necessary to reinstate the integrity of the footpath affected by the work. .
- Road:** All restorations require 200mm overlap to all sides of all trenches.
13. **Workcover requirements and protection of the public** - During the performance of these works the permit holder is fully responsible for the site, including meeting all Workcover Authority requirements and protecting the public from all hazards which may arise from the opening. Work must not be carried out at a time or in a manner that will cause undue inconvenience to the public or adjacent property owner and businesses.
- The work must be completed as soon as possible within the hours listed in the special conditions.
 - The gutter / drainage system must not be obstructed in any way.
 - Work must not interfere with or damage any public utility services.
 - Excavated stone or pavers may be stored at the City's Bay St depot provided 24 hours notice is given to the City's Storeman on phone number (02) 9265 9565. Material will only be accepted if it is clean, in appropriate condition for re-use, is labelled and stored on pallets.
 - No material other than that to be used on the day of work is to be stored on site.
 - The public way is to be left in a clean and tidy state with no obstruction to pedestrian or vehicle traffic on completion of work each day. During non-work periods (day or night) any openings must be covered by adequately secured, skid resistive high strength steel plates to comply with the relevant Australian Standards.
 - All new services are to be laid to at least the minimum depth required by the relevant Australian Standard or responsible Public Authority.
 - The permit holder will promptly comply with any directions given by the City or the NSW Police.
 - All spoil, waste material & plant equipment to be removed from the road upon completion of works each day.
14. **Damage to the public way** - The permit holder undertakes to reimburse the City of Sydney for the cost of repair of any damage caused to the public way as a result of the activities associated with this approval.

Standard conditions of approval for Road Opening works - continued

15. **Stormwater gully protection** - Stormwater gully protection is to be in place at all times. The public way is to be maintained in a clean and tidy state at all times.
16. **Breach of conditions** - This approval may be withdrawn or modified without notice for safety issues or breaches of conditions.
17. **STA Associated costs** - Any costs associated with the rerouting of Sydney Transit Authority service and/or relocation of bus stops during works will be the responsibility of the permit holder.
18. **Noise** - All works under this approval are subject to the requirements of the City of Sydney's noise code.

Temporary Restoration specifications and requirements

At the completion of the works the party undertaking the works associated with the road opening permit is required to complete a temporary restoration to the works area, in accordance with the following specifications to ensure the area is in a trafficable state for pedestrians and/or vehicles.

- The edges of all asphalt, paved or concrete surface layers and all base layers are to be prepared with a straight clean square edge prior to temporary restoration.
- The excavation or trench is to be backfilled to the base of the surrounding pavement structure with pre-mixed sand and cement using a 14:1 ratio.
- The backfill material is to be placed in layers not exceeding 200mm deep with each layer being compacted to ensure the temporary pavement base is uniformly compacted 15mm below the finished surface level in a footway, and 300mm below finished surface level in a roadway or shared zone.
- Any unbound pavement material is to be laid in accordance with the RMS QA Specification R35 (as amended) to form a new level and uniformly compacted temporary pavement base 15mm below the finished surface level in a footway and 300 below finished surface level in a roadway or shared zone.
- The unbound pavement material must have a minimum thickness of 85mm in a footway and 250mm in a roadway.
- A wearing surface of asphaltic concrete complying with RMS specification part R116 is to be laid. AC5 is to be used for footway and AC10 or AC14 for roadways and shared zones.
- The asphalt surface is to be laid and uniformly compacted to match the finished surface levels of the surrounding pavement surface and is to be free of any irregularities that could lead to pedestrian or traffic hazards.
- The party undertaking the works/temporary restoration must action requests from the Asset Inspector to repair temporary restorations within 4 hours of the request.
- The party undertaking the works is to maintain all temporary restorations to a safe standard until permanent restoration of the openings is completed by the City of Sydney Council or its approved contractors.