

**UPDATED
PITT STREET SOUTH
DESIGN GUIDELINES**

APPENDIX A





Pitt Street South Over Station Development

Design Quality Guidelines

November 2018





Purpose of the Document

The purpose of this document is to guide the design of the Sydney Metro Pitt Street South over station development (OSD) and provide a reference document for the assessment of design outcomes.

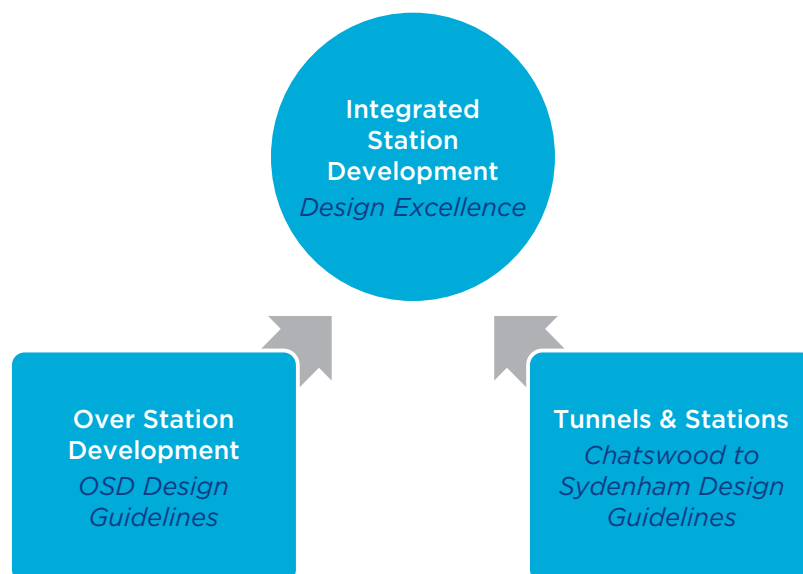
These design guidelines provide a set of overarching principles and outcomes capable of interpretation irrespective of final land use. They have been formulated to ensure delivery of design excellence across all responses to the site.

Design parameters are included for built form, heritage, integration with the public domain and Sydney Metro station, movement and connectivity and legacy outcomes of the development. These have been prepared with reference to:

1. Final Design Report Over Station Development, Pitt Street South, GHDWoodhead, June 2018
2. 'Sydney Metro City & Southwest: Chatswood to Sydenham Design Guidelines' (Sydney Metro CSW Design Guidelines), June 2017
3. 'Sydney Metro City & South West Pitt Street South Over Station Development: Heritage Impact Statement'.

Separate approval pathways for the station and the over station development have required the preparation of separate design guidelines for each component. These guidelines build on those accompanying the 'Sydney Metro City & Southwest: Chatswood to Sydenham Design Guidelines' and as such should be read in conjunction with them.

A key focus of these guidelines is to set clear design objectives for the OSD elements that integrate with the station, and to ensure consistency in the design approach for both elements. It is intended that the guidelines also inform and complement the future Station Design and Precinct Plan and Interchange Access Plan and are to be considered as part of the Sydney Metro Design Excellence Strategy for the site.

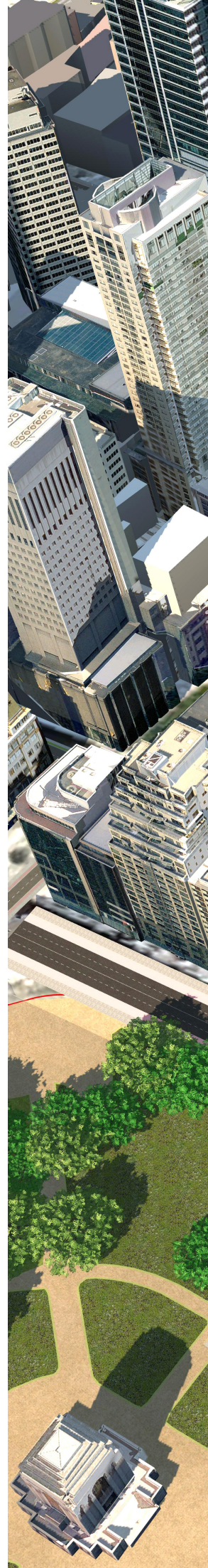


Vision

The Pitt Street South OSD will be transformational for the precinct and create and catalyse significant enhancement of the public realm, ground plane and urban setting. It will become a landmark element, integrated and associated positively with the Metro Station and be an integral part of its legacy.

Exceptional design will reflect the local context, acknowledge its prominent location within Central Sydney and address city-making and place-making principles. A distinctive built form will mark the site's location on Bathurst Street, one of Sydney's significant east-west avenues between Hyde Park, the Town Hall Civic Precinct and Darling Harbour, and capitalise on unobstructed views of Sydney Harbour and Hyde Park.

The OSD will seamlessly integrate with all building elements across the site, including the public domain, station and entrances and transfer elements.



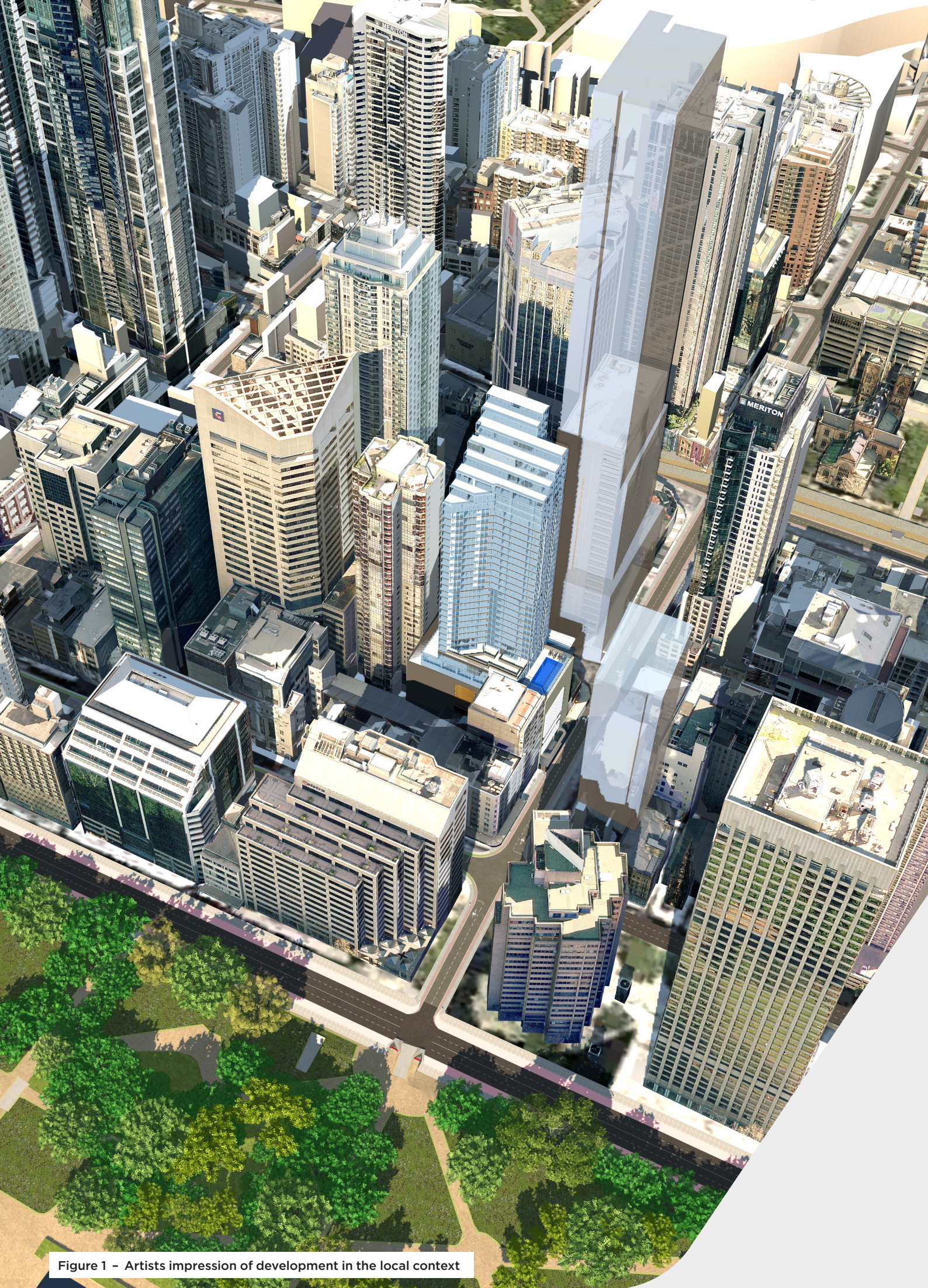


Figure 1 - Artists impression of development in the local context

Site Overview

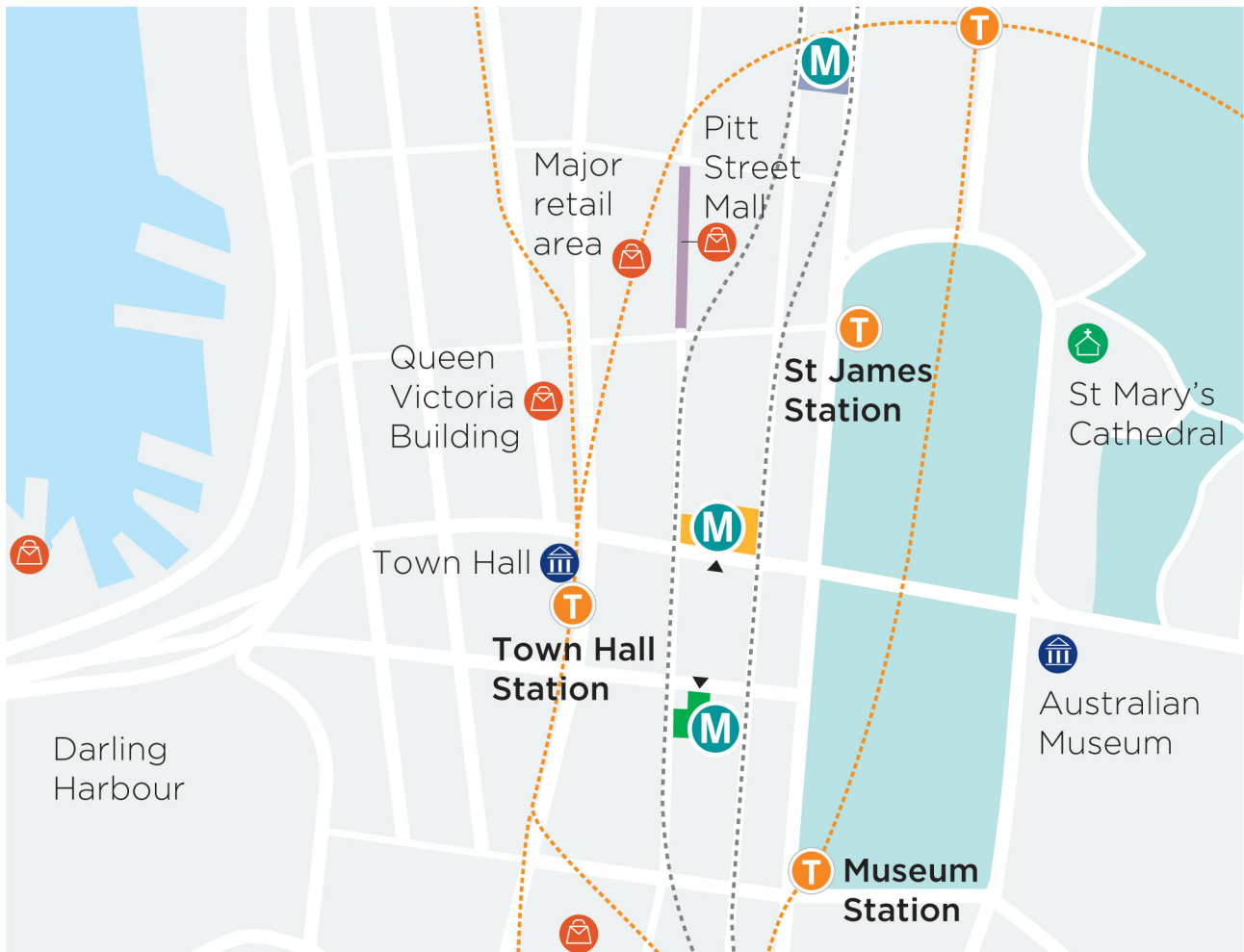


Figure 2 - Site Map

The proposed Pitt Street South OSD site is located between Pitt & Bathurst Streets and is one of two entries to the Pitt Street Metro Station (See Figure 2: Site Map). The site has an area of 1,708sqm and features street frontages of 24 metres to Bathurst Street and 32 metres to Pitt Street. The Edinburgh Castle Hotel on the corner of Pitt and Castlereagh Streets is excluded from the boundaries of the site, resulting in an irregular shaped site, with the corner frontages removed from the develop-able ground plane.

Site Context

Significant features of the site and its context include:

1. A central location on Bathurst Street, one of Sydney's significant east-west avenues connecting Hyde Park to the Town Hall Civic Precinct and Darling Harbour.
2. Located within 400m walking distance of key city features such as Hyde Park, World Square, Town Hall and the QVB, as well as major convenience stores such as Woolworths, creating significant pedestrian movement through the precinct.
3. A wide range of transport options are available in the immediate vicinity including bus and rail and the future light rail and Metro.
4. A number of significant heritage items are located to the north, south and west of the site, with close proximity to Sydney Town Hall, the QVB, and Hyde Park.

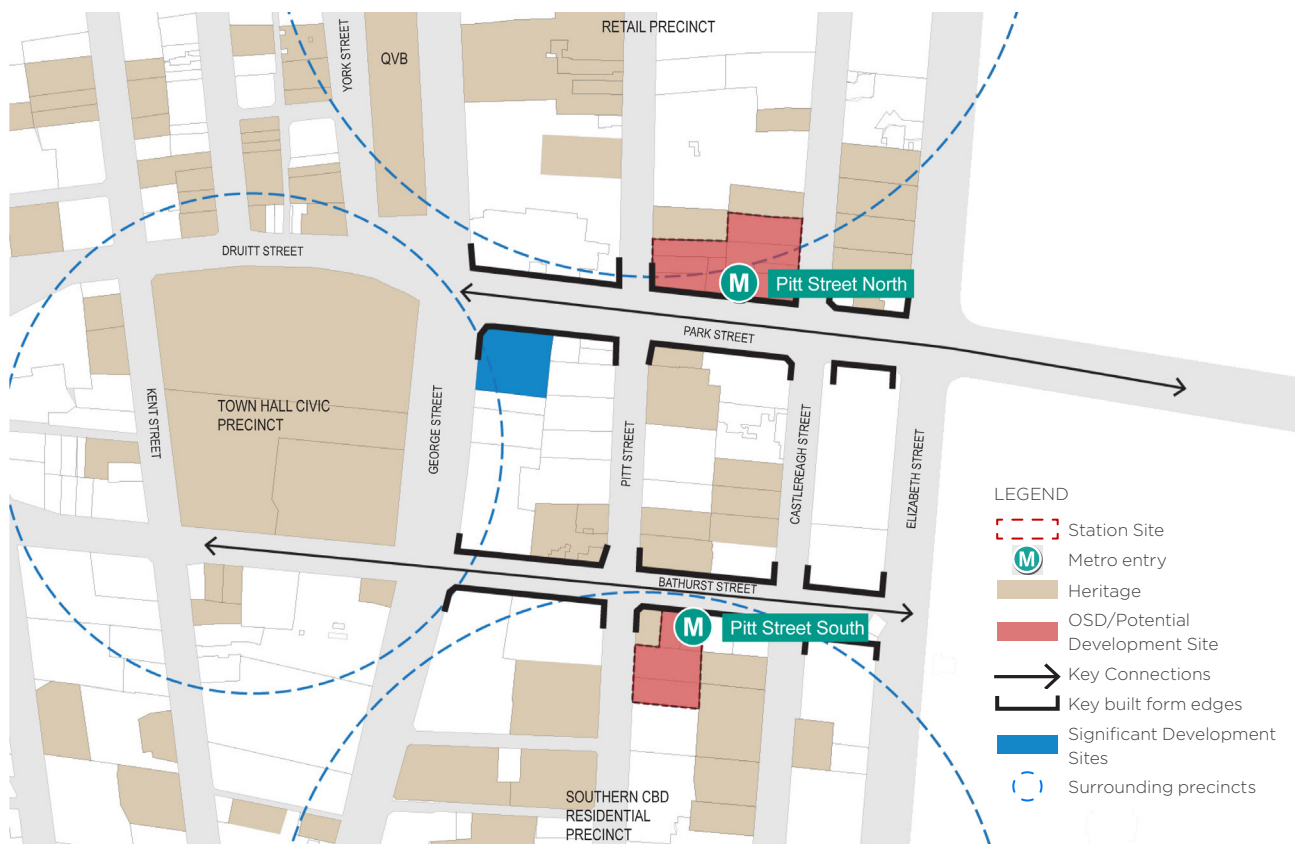


Figure 3 - Site Context

5. The current streetscape on both Bathurst and Pitt Streets is a broad mix of use, scale and materiality organised around the heritage architecture of the Edinburgh Castle Hotel and dominated by the heritage façade of the Primus Hotel (old Water Board building) in Pitt Street
6. The immediate precinct is undergoing a major upgrade with the Greenland Tower and 116 Bathurst Street developments under construction, and the recent completion of the Primus Hotel. This emerging context fundamentally alters the skyline and provides an opportunity for a productive visual dialogue with the built form above the podium.
7. The Edinburgh Castle Hotel on the corner of Pitt and Bathurst Streets adjoins the site and is a 3 storey heritage building. This building has a major influence on planning for the Station and the OSD for the site.
8. The parapet line of the Edinburgh Castle Hotel along Pitt Street is continued by the façades of the Princeton apartments to the south, with no immediate reference to the 45m podium control, and well below the 20m minimum in the DCP.
9. Along Bathurst Street, the 45m podium height is strongly reinforced by the two apartment blocks to the east, creating a challenge as to how to mediate between the Bathurst Street frontage and the much lower parapet line of the Hotel on the corner.
10. The majority of the ground plane is allocated to the station concourse and associated functions, with this limiting uses requiring large loading or lobby spaces such as hotels or commercial.
11. A variety of opportunities exist to enhance the public domain & the broader urban context through an improved and activated pedestrian environment, contextual and human scale design, integrated landscaping and quality finishes.

Sydney Metro Design Objectives

The design outcomes for the Pitt Street South OSD are underpinned by the design objectives for all Sydney Metro projects.



Figure 4 – Sydney Metro project

Designs for the station, station precinct and the over station development must deliver on the following:

Ensuring an easy customer experience.

Sydney Metro places the customer first. Stations are welcoming and intuitive with simple, uncluttered spaces that ensure a comfortable, enjoyable and safe experience for a diverse range of customers.

Being part of a fully integrated transport system.

Sydney Metro is a transit-oriented project that prioritises clear and legible connections with other public and active transport modes within the wider metropolitan travel network that intersect with this new spine.

Being a catalyst for positive change.

Sydney Metro is a landmark opportunity to regenerate and invigorate the city with new stations and associated development that engage with their precincts, raise the urban quality and enhance the overall experience of the city.

Being responsive to distinct contexts and communities.

Sydney Metro's identity is stronger for the unique conditions of centres and communities through which it passes. This local character is to be embraced through internationally benchmarked high quality station architecture and public domain that is well integrated with the valuable inherited urban fabric of existing places.

Delivering an enduring and sustainable legacy for Sydney.

Sydney Metro is a positive legacy for future generations. A high standard of design across the corridor, stations and station precincts, that sets a new benchmark, is vital to ensuring the longevity of the Metro system, its enduring contribution to civic life and an ability to adapt to a changing city over time.

Sydney Metro City And South West Chatswood To Sydenham Design Guidelines

The Chatswood to Sydenham (C2S) design guidelines form part of the environmental impact statement (EIS) for the C2S Critical State Significant Infrastructure approval and establish design standards to guide the interface outcomes between stations and their surrounding locality.

The C2S design guidelines provide the following key design drivers and urban design strategies for Pitt Street station. As a component of the approved C2S EIS, these drivers and strategies must inform the design response for the Pitt Street South site.

Key design drivers

1. Provide space for customers in a busy pedestrian environment by extending the public domain into the station entries.
2. Integrate with the Sydney City Centre Access Strategy and other CBD planning strategies.
3. Anticipate connections to a future Town Hall Square and other nearby developments.
4. Extend the transport focus along Park Street, near Pitt Street.

Urban design strategies

Linking Hyde Park to the Civic Precinct

As increasingly important pedestrian streets, Park Street and Bathurst Street will require public domain improvements.

A Street-grid of Interchange

The entrances to the new Metro station address Park and Bathurst Streets. These two streets will be key to interchange movements, especially to the bus and light rail services that run along the north-south streets of the city.

Frontages to east-west streets

The primary address of both Metro entries will be to the east-west connectors, reinforcing the importance of these streets and facilitating interchange between transport modes.

Extending the materiality and character of the surrounding public domain into the station entries creates the opportunity for a seamless experience.

Optimising development over stations

The entrances to the station provide an opportunity to facilitate renewal. Future development above these spaces should reflect the context of the locality and positively contribute to the built form and character of the area.

DESIGN GUIDELINES

Built form

Respond to the existing urban fabric and built form context of this mid-town location through a finer, textured-grain and human scale podium design and a simple, refined over station design, reflecting both the significant heritage architecture of the locality and the evolving nature of the precinct.

Ensure the design responds appropriately to final land use choice and directly integrates connections between station and OSD elements, including rooftops, and aligns materiality and scale with the adjacent heritage fabric.

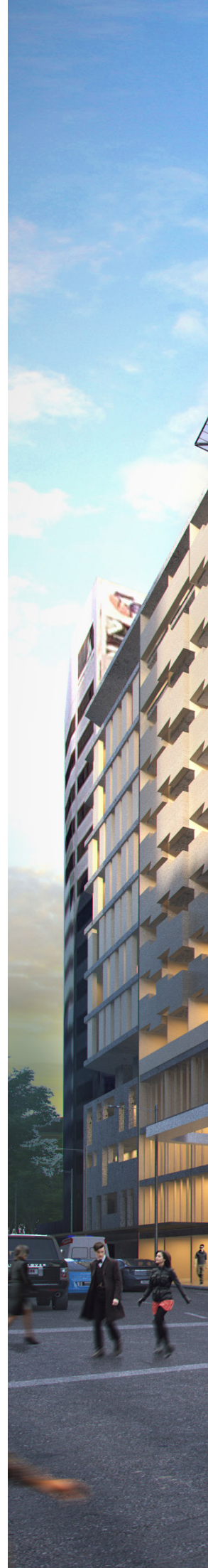




Figure 5 - Indicative design of Pitt Street South over station development built form

Podium and Street Wall

Podium form & articulation should demonstrate strong heritage & contextual sensitivity, with scale and massing that relates well at the human scale, whilst acknowledging the evolving nature of this area of Central Sydney. Design excellence, articulation and finish are delivered irrespective of end use and capture opportunities for varied responses accordingly.

This is to be achieved through:

1. Recognising the surrounding streetscape scale and providing an enhanced interface with adjacent heritage buildings, with direct reference to the height and articulation of these buildings, including:
 - a) Seamless integration of station and over station development in the podium within a multi-scaled and visually noisy streetscape (see Figure 6: Indicative Integrated Massing Strategy).
 - b) Mitigating the impacts of scale and massing on existing heritage items through the provision of a modulated podium and setbacks and responding to the built form context (See Figure 7: Indicative Street Wall Response – Bathurst Street Elevation and 8: Indicative Street Wall Response – Pitt Street Elevation).
 - c) Providing an intermediate reference element along Pitt Street, referencing the lower Edinburgh Castle Hotel parapet line, the Princeton Apartments façade and the more dominant scale of the Primus Hotel opposite.
 - d) Retaining the prominence and landmark character of the Edinburgh Castel Hotel through:
 - (i) Exploring opportunities to seamlessly integrate the hotel into the OSD
 - (ii) Addressing the scale difference between the established 45m podium height along Bathurst Street and the lower parapet line of the Edinburgh Castle Hotel,
 - (iii) Design of vertical street walls above the hotel, especially where the footprint of the over station development wraps around the building, to prevent large, blank walls from dominating the building.
 - (iv) Materiality and façade articulation of the podium responding to the hotel to better integrate the two sites and to activate the facades.
 - e) Provision of a maximum podium height of RL 71.0, being approximately 9 storeys or 47 metres above ground level.
 - f) Setbacks of:
 - (i) 0 metre to northern and eastern boundaries
 - (ii) 3 metres to southern boundary
 - (iii) A minimum 4.87 metres to western boundary, referencing the Princeton Apartments, 304-308 Pitt Street.
2. Maximising natural light to OSD uses within the podium.
3. Alignment of over station development with established building alignments at lower levels, with lobbies provided from Pitt Street.
4. Provision of landscaping throughout the podium design, laying spaces of relief & activation and referencing landscaping of the precinct.

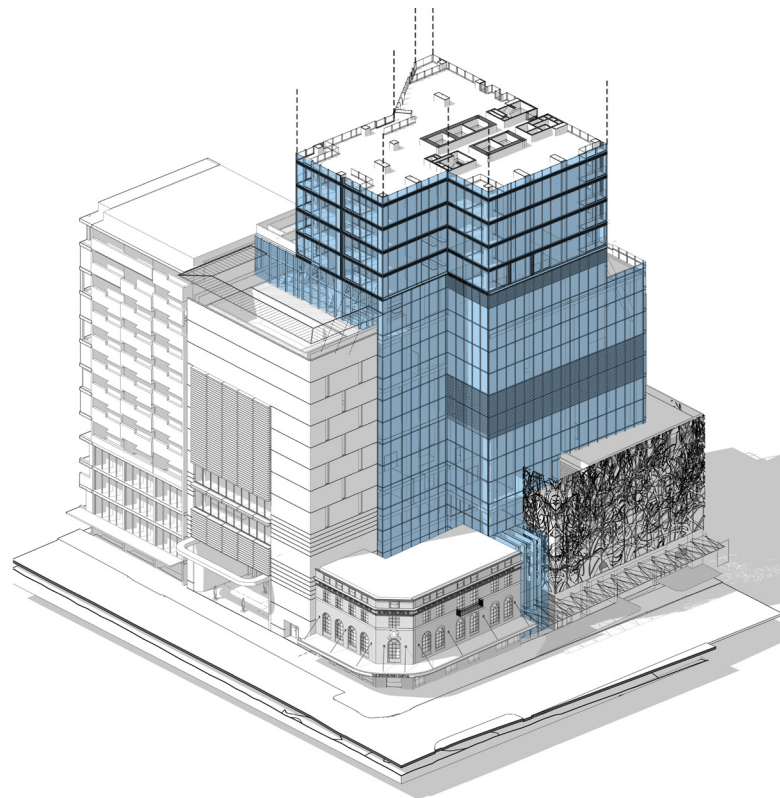


Figure 6 - Indicative Integrated Massing Strategy

Source: Pitt Street South OSD - Built Form and Urban Design Report, GHD Woodhead, June 2018

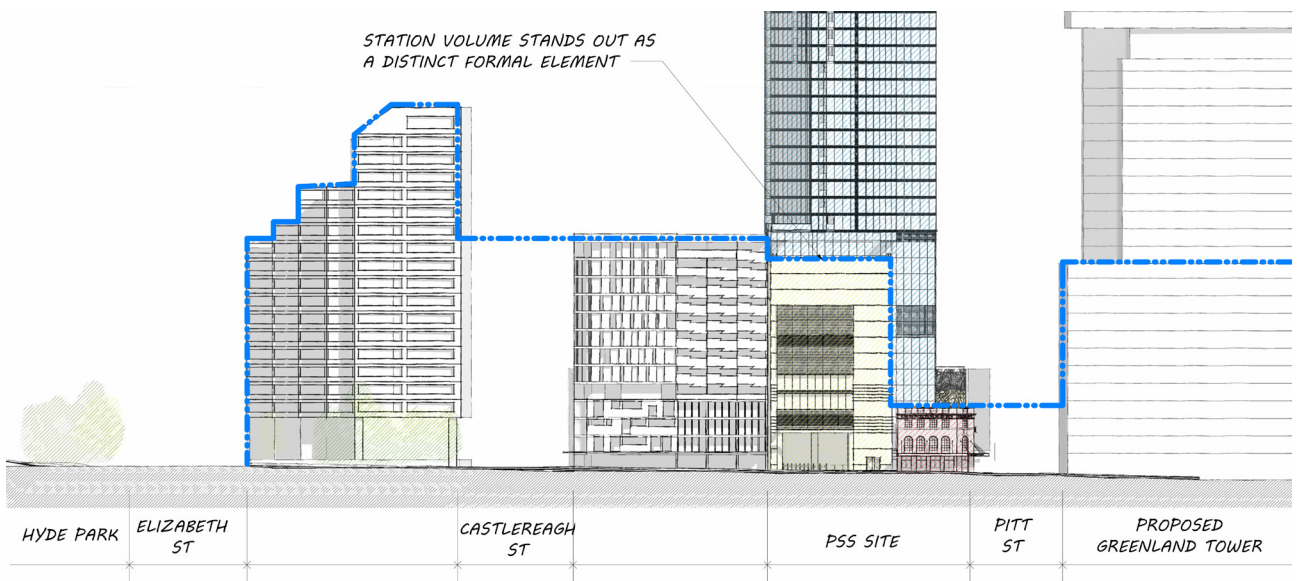


Figure 7 - Indicative Street Wall Response - Bathurst Street Elevation

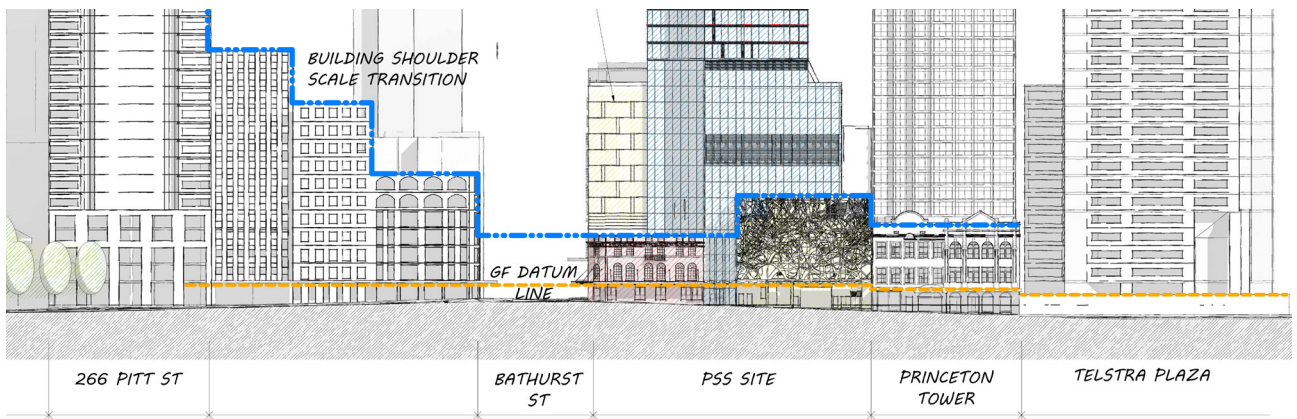


Figure 8 - Indicative Street Wall Response - Pitt Street Elevation

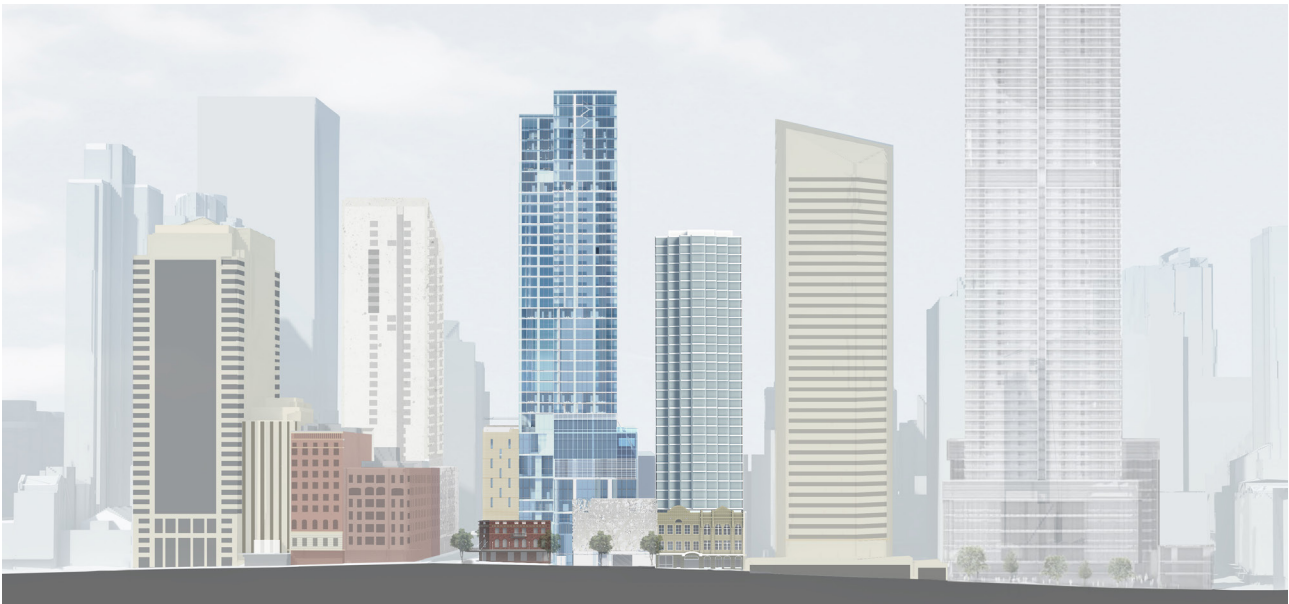


Figure 9 – Indicative Over Station Development Design Response - Pitt Street Elevation

Built Form above the Podium

The built form above the podium will leverage the evolving development context to create an exceptional and prominent urban marker that is complementary and sympathetic to the local context, creating a considered and transitional composition on the skyline. Design excellence, articulation and finish are delivered irrespective of end use and capture opportunities for varied responses accordingly. Design will ensure protection of the public domain, especially solar access to Hyde Park, and consideration of impacts on neighbouring uses.

This is to be achieved through:

1. Recognition of the contextual relationship with surrounding heritage listed items.
2. Integration of the over station design to enhance podium articulation and improve legibility of the station entrance (See Figures 9 to 11: Indicative Over Station Development Design Response).
3. Creating a built form transition between Greenland Tower and other adjacent developments, particularly Telstra Building (320 Pitt Street) and 116 Bathurst Street
4. Maximising solar access to the public domain, through:
 - a) Design and articulation of the built form above the podium to ensure no additional overshadowing to Hyde Park on June 21st, between 12pm and 2pm (required by SLEP 2012 Sun Access Plane controls).
 - b) Creation of opportunities to protect solar access to surrounding pedestrian environments.
5. Optimising views from the development to Hyde Park and Sydney Harbour.
6. Consideration of privacy implications to surrounding residential buildings, including the Princeton Apartments and 135-137 Bathurst Street.
7. Where practicable, preserve sunlight access and views to the north for neighbouring properties.
8. Street setbacks above the CSSI Transfer level (RL 58.25) of:
 - a) 4 metres to Bathurst Street.
 - b) 3 metres to the eastern boundary adjacent to 137-139 Bathurst Street.
 - c) minimum nil setback to the remainder of the eastern boundary.
 - d) 12 metres to the southern boundary, in recognition of windows in northern wall of Princeton Apartments.
 - e) 5.9 metres to Pitt Street, to align with setbacks for the Princeton Apartments.
9. Use of materials that reflect the function of elements above the podium, distinguishing them from the surrounding context and providing a simple design resolution within the city skyline.
10. Provision of landscaping throughout the design, laying spaces of relief and referencing landscaping of the precinct.
11. Achievement of SEPP65 & ADG requirements.



Figure 10 - Indicative Over Station Development Design Response - Bathurst Street Elevation



Figure 11 - Indicative Over Station Development Design Response - Castlereagh Street Elevation

Source: Pitt Street South OSD - Built Form and Urban Design Report, GHD Woodhead, May 2018

Public Domain and Place

Contribute to a well-considered and articulated public domain that addresses the significance of the site and the complexity of high pedestrian activity in a relatively constrained location. Provide a strong relationship between Pitt Street Station North and South and pursue innovative opportunities to maximise activation of the spaces within the site and fronting the street network.

This is to be achieved through:

1. Enhancing the quality of the public domain, including provision of widened footpaths, new street trees, paving upgrades and public art, especially along Bathurst Street. A potential kerb extension at the station entry would add amenity to the public domain by allowing tree planting and urban furniture.
2. Providing space for customers in a busy pedestrian environment by recessing station entries to widen the pavement and provision of uncluttered movement corridors (See Figure 13: Design for efficient pedestrian access and demarcation of uses).
3. Reinforcing the importance of Bathurst Street as a primary City avenue by locating the main entry points to the Metro station on this street.
4. Providing a strong, well demarcated street address to each frontage through strong form modulation and well activated ground floors.
5. Innovative design solutions to maximise activation along all street frontages. Activation includes a mix of building entrances and retail uses.
6. Promoting a safe & user-friendly environment including weather protection, security measures & wayfinding etc. To include as a minimum:
 - a) Minimising opportunities for criminal and anti-social behaviour.
 - b) Incorporating awning cover that relates to surrounding buildings to create a continuous weather protection edge to all street frontages.
 - c) Seamless integration of all signage with the architectural character of the scheme and surrounding context, providing an elegant and uncluttered approach and coordinated with nearby public art. Signage must integrate with City of Sydney DCP 2005 - Signage and Advertising Structures.
7. Reinforcing the east west connection between Hyde Park, George Street and Darling Harbour. (see Figure 14: Significant East-West connections).
8. Provision of public art, integrated and cohesive with the design of the built form and potentially recognising former uses.

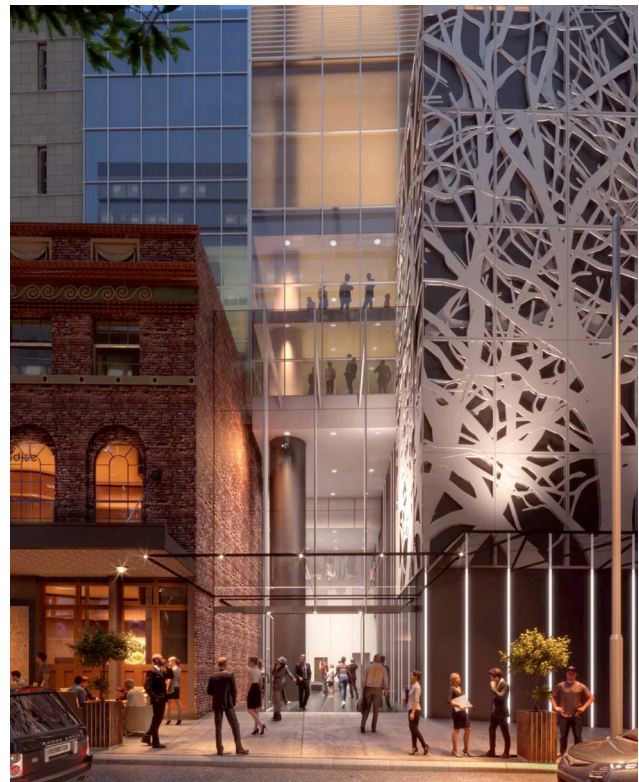


Figure 12 – Artists impression of Public Domain and Place

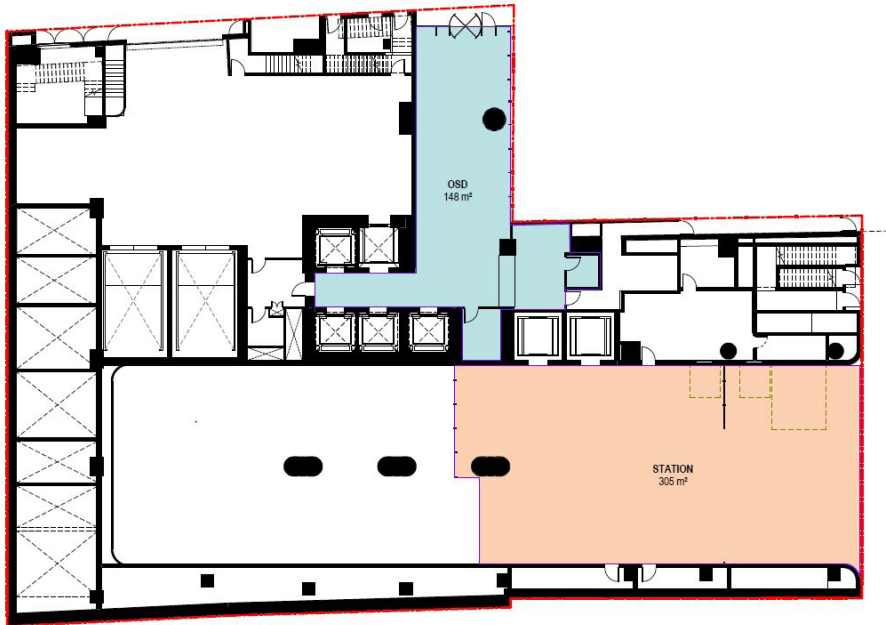


Figure 13 – Design for efficient pedestrian access and demarcation of uses

Source: Pitt Street South OSD - Built Form and Urban Design Report, GHD Woodhead, June 2018

LEGEND

- OSD GROSS FLOOR AREA
- STATION GROSS FLOOR AREA



Figure 14 – Significant East-West Connections

Source: Hassell

Movement and Connectivity



Figure 15 - Pedestrian activity around Pitt Street South

Acknowledge the important movement and interchange function of Bathurst Street. Prioritise pedestrian access, permeability and amenity within the development and across the precinct and facilitate legible, safe and convenient interchange opportunities across transport modes.

This is to be achieved through:

1. Mitigating pedestrian overcrowding through the use of additional footpath width along Bathurst Street, achieved through some kerb extensions.
2. Managing pedestrian flow at ground level through separation of over station development lobbies and Metro entries to different street frontages.
3. Clustering support services at ground level, including egress points, to simplify the articulation of the ground plane and ensure clarity between the various functions and lobbies.
4. Integrating with the Sydney City Centre Access Strategy.
5. Facilitating safe and adequate pedestrian space at adjoining road crossings and driveways, including provision of traffic management infrastructure as required.
6. Designing to minimise cyclist conflict points with vehicles and pedestrians.
7. Providing clear and legible interchange with all transport modes, including:
 - a) Town Hall and Museum Stations.
 - b) City and South East Light Rail on George Street.
 - c) Bus stops on Park Street, Bathurst Street, Castlereagh Street, and Elizabeth Street.
 - d) Bicycle parking facilities and the future cycle connection on Castlereagh Street.
 - e) Vehicle drop off and pick-up from Bathurst Street and Pitt Street and taxi bays on Pitt Street and Park Street.(See Figure 16 Interchange Opportunities)
8. Strengthening connections to Town Hall Civic Precinct and nearby developments.
9. Strengthening East West connections along Bathurst Street, including as connections to green space.
10. Retaining existing and incorporating new street trees to reduce the heat island effect and supplement existing avenue planting.



Figure 16 - Interchange Opportunities

LEGEND

- Station site
- M Metro entry
- T Existing Train Station
- Existing Bus Access
- B Existing Bus Stop
- L Future Light Rail Stop
- Future Light Rail Line
- 🚲 Proposed bike parking
- 🚲 Existing cycle route
- 🚲 Proposed cycle route
- 🚗 Proposed taxi bay
- K Proposed Kiss & Ride (existing short stay parking)
- ➔ Interchange movement

Integration and Legacy

Provide an OSD that seamlessly integrates all components of the development and is a positive legacy for future generations.

This will be achieved through:

1. Delivering a high standard of design and finish that promotes longevity and adaptability over time.
2. Functional integration of the various permissible uses with the Sydney Metro component should be seamless, simplifying the vertical division and coordination of services wherever possible.
 - a) Permissible uses should be functionally separated as much as possible at ground level to assist in pedestrian circulation and serviceability (See Figure 17: Functional Separation of Uses).
 - b) Back of house operations and services should be consolidated wherever possible while maintaining any required separation between the OSD and Sydney Metro (See Figure 18: Indicative Service Division).
 - c) Consider and allow for flexible future use of functional spaces & services coordination.

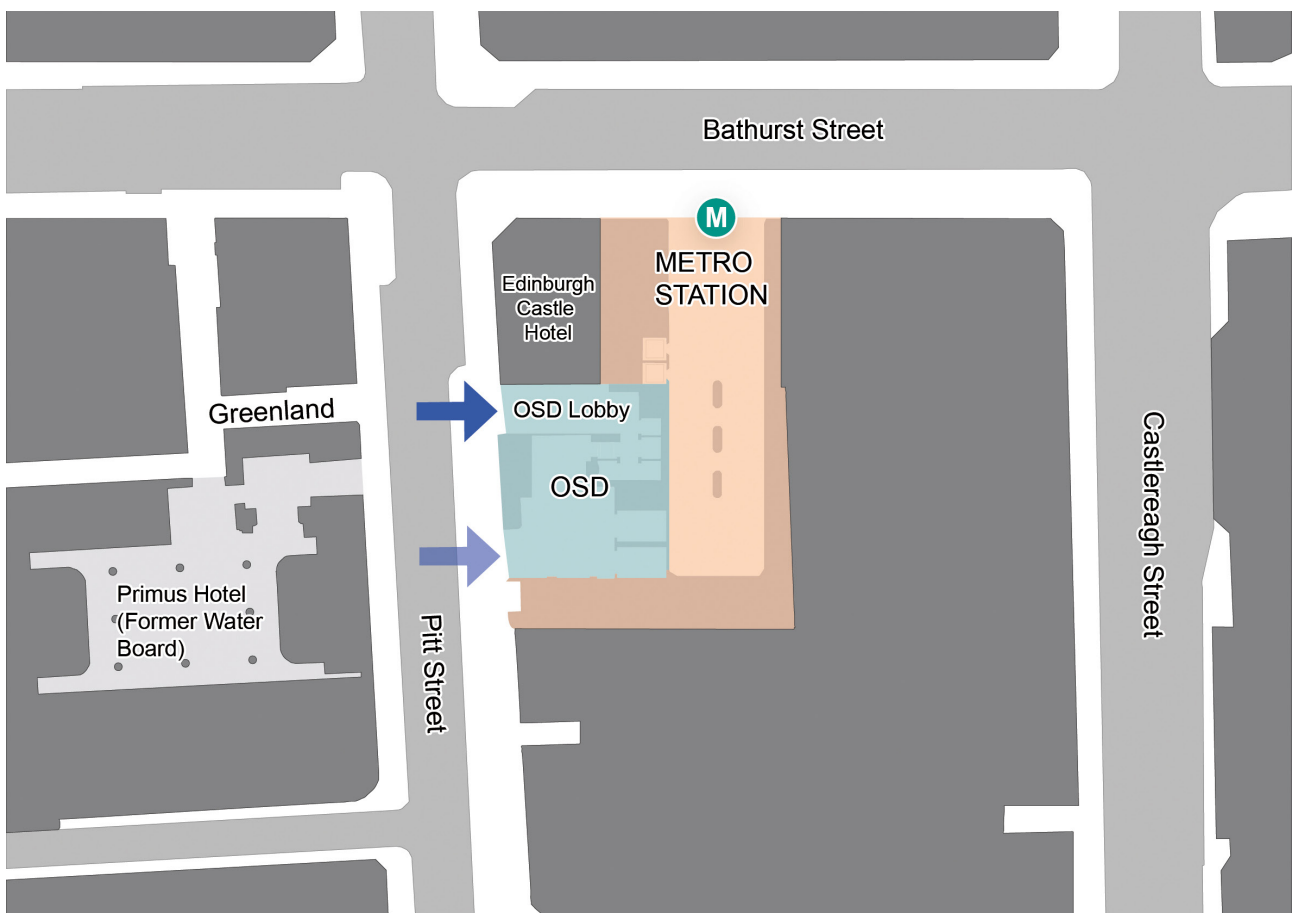


Figure 17 - Functional Separation of Uses

Source: Pitt Street South OSD - Built Form and Urban Design Report, GHD Woodhead, June 2018

3. Delivering an over station development that:
- a) Does not have any adverse impact on the design and/or operation of the metro Station.
 - b) Is capable of complete demolition and reconstruction, or major maintenance or modification, without significant interference to the operation of the metro Station.
 - c) Will allow independent access, servicing and maintenance from normal station activities and operation.
 - d) Integrates efficiently with the station structure.
 - e) Achieves unity in design through connecting the station entry, podium and over station development, as a single readable piece of architecture including to provide continuity and well considered transitions of bulk and scale between the station box and the over station development design.
 - f) Provides visual connectivity between the OSD lobby and the public domain.

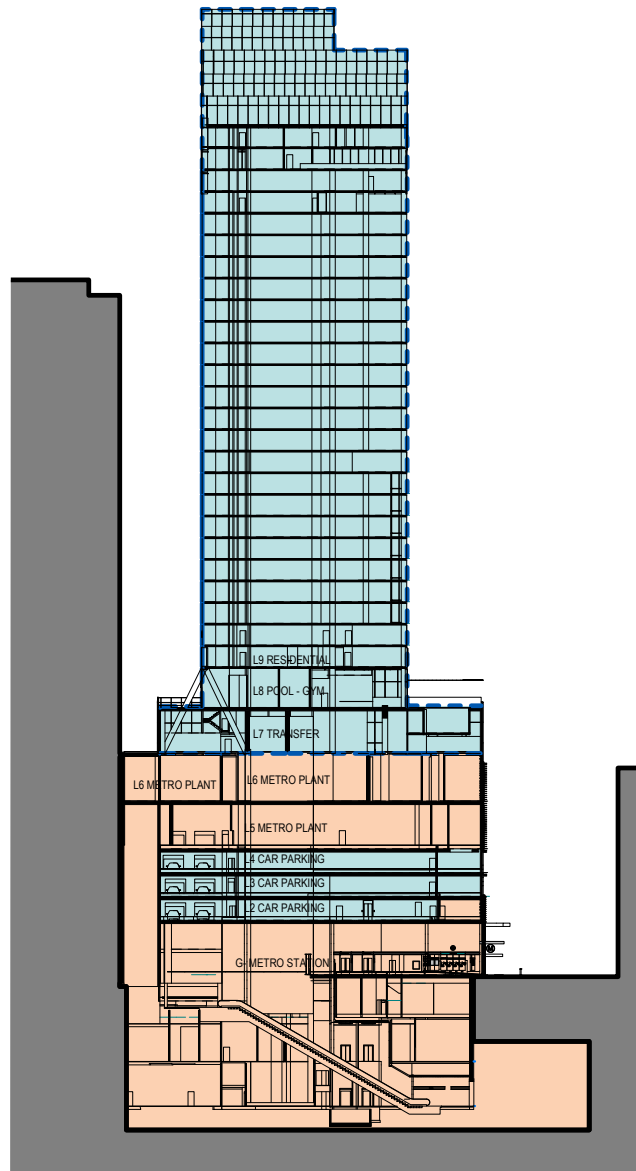


Figure 18 – Indicative Service Division

Source: Pitt Street South OSD - Built Form and Urban Design Report, GHD Woodhead, June 2018

Benchmarks

Sydney Metro has identified benchmark projects that demonstrate the design quality aspirations for the two Pitt Street Integrated station development sites.

These benchmarks have been selected to showcase the minimum quality expected in relation to:

1. Integrated design outcomes.
2. Built Form above the podium that showcase high quality design and contribute positively to the city skyline.
3. Architecture that responds to adjoining buildings and streetscape character and scale.
4. A design that provides a high quality public space that is integrated, connected, active, safe and comfortable for customers and pedestrians.
5. A design that fulfils the needs of a civic station entry and a high quality OSD entry with associated servicing.
6. Materials and finishes that are high quality and appropriate to the context.
7. Integration of joyful public art and public domain elements that contribute to a good experience of the place.
8. Well considered strategies in façade and services integration that contribute towards best practice sustainable outcomes.

Each benchmark has been chosen to endorse a variety of design outcomes as outlined in the table below. Further details of these projects are provided in the Pitt Street Design Quality Benchmarks and are to be used to guide design outcomes for the over-station development.

Benchmark	Wynyard Place 10 Carrington St, Sydney	Lumiere' Bathurst St, Sydney	AHL Headquarters + Hilton hotel 478 & 488 George St, Sydney	5 Martin Place	Upper House 520 Swanston St, Carlton, Melbourne	Nishi building 25 Edinburgh Ave, Canberra, ACT
Integrated design	✓	✓	✓	✓	✓	✓
Positive contribution to skyline	✓	✓	✓	✓	✓	
Streetscape character and scale	✓	✓	✓	✓	✓	
High quality public space	✓	✓		✓		
Civic station entry and high quality OSD entry	✓	✓		✓		✓
High quality materials and finishes	✓	✓	✓	✓		✓
Public art and public domain elements	✓	✓				✓
Best practice sustainability	✓			✓		✓



Contact us

For more information visit our website sydneymetro.info or contact us via:

-  **1800 171 386** 24-hour community information line
-  **sydneymetro@transport.nsw.gov.au**
-  **Sydney Metro**
Sydney Metro, PO Box K659,
Haymarket, NSW 1240
-  If you need an interpreter, call Translating and Interpreting Services National on **131 450** and ask them to call **1800 171 386**



Figure 19 - Artists impression of Pitt Street South envelope within the Sydney CBD skyline.



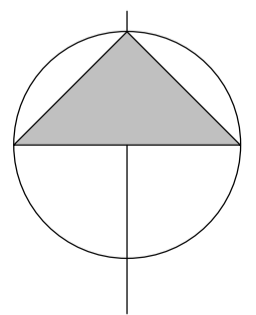
ADDITIONAL BUILDING ENVELOPE SECTIONS

APPENDIX B

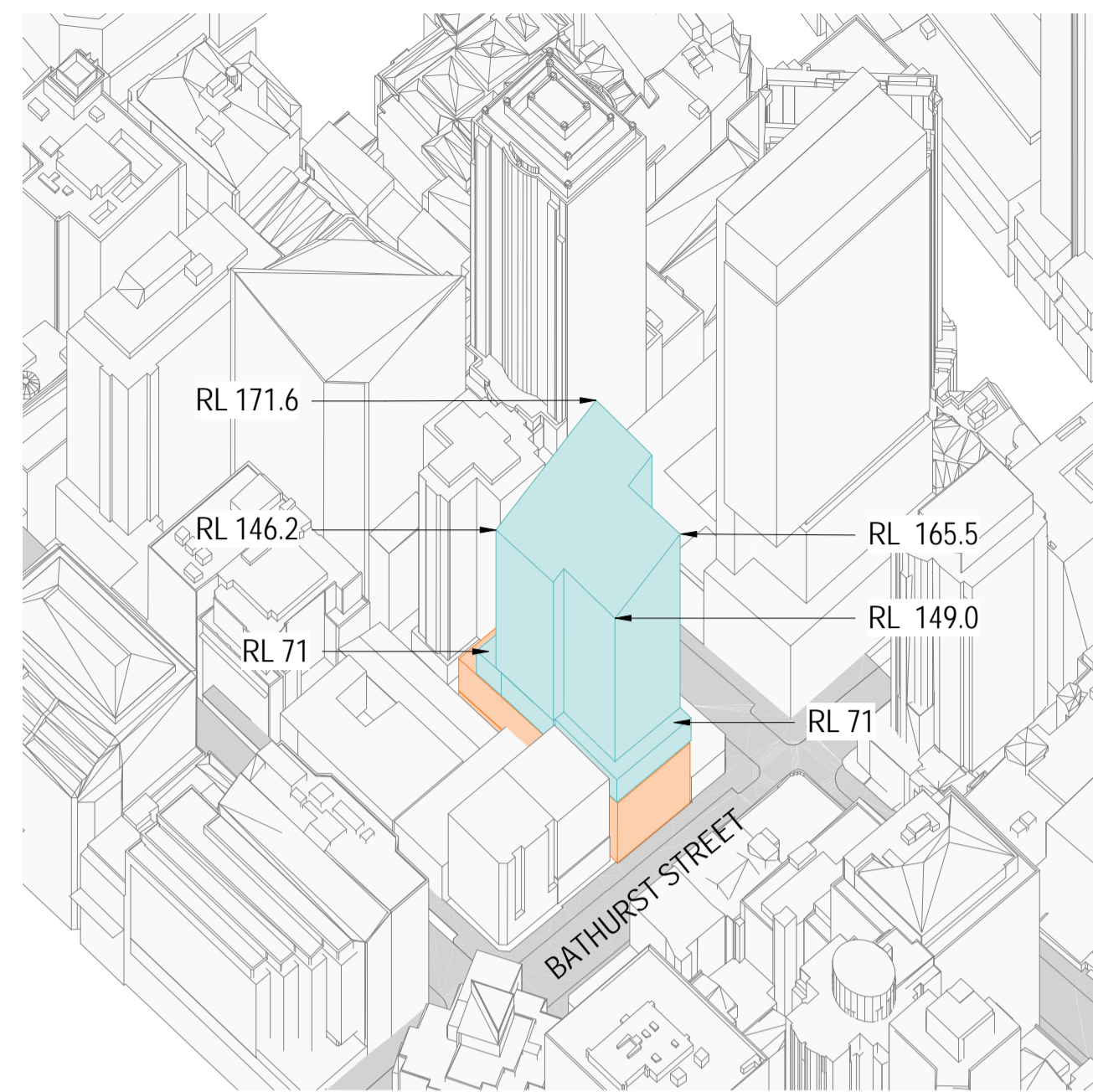




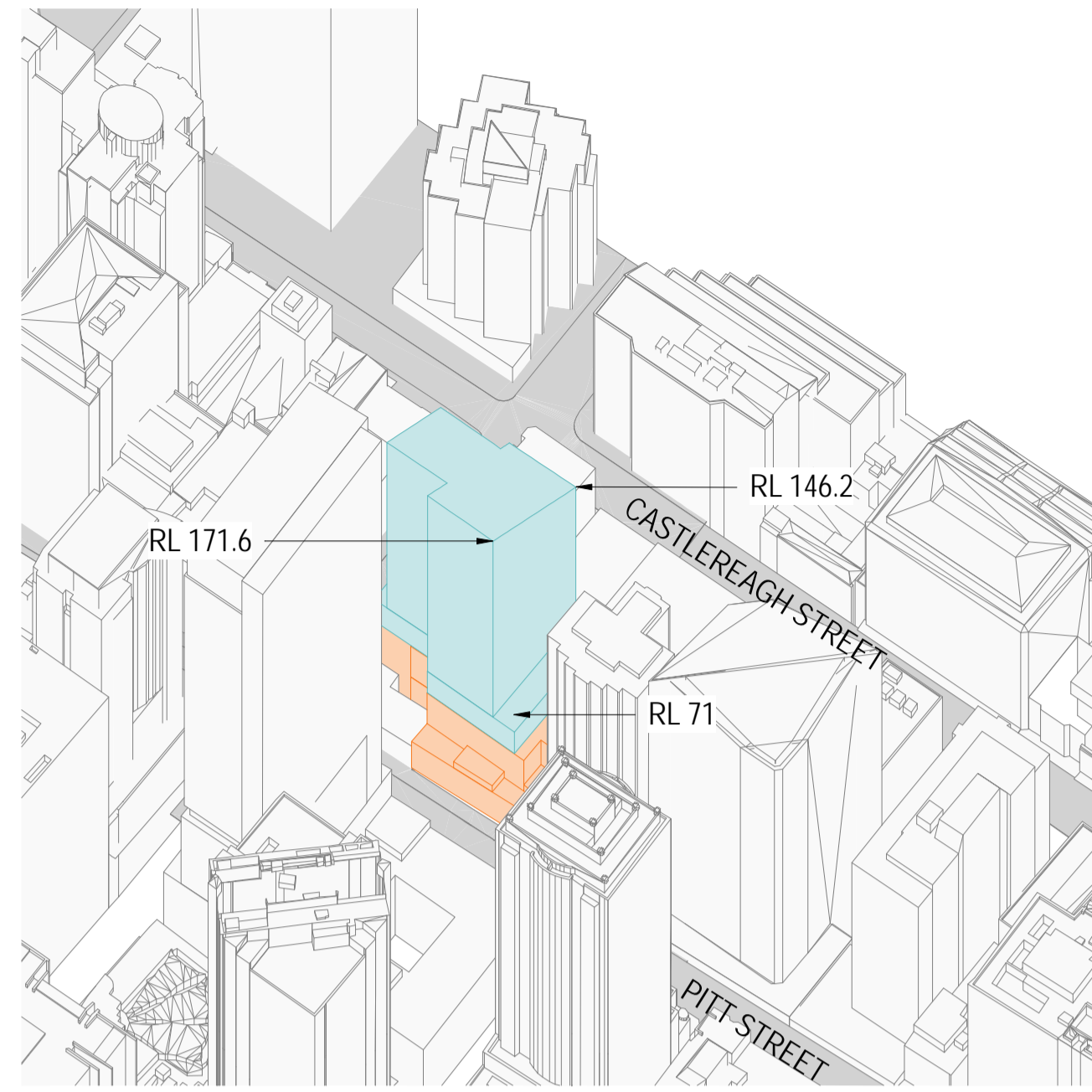
1 DCP SETBACKS ABOVE 45M STREET HEIGHT
1:500



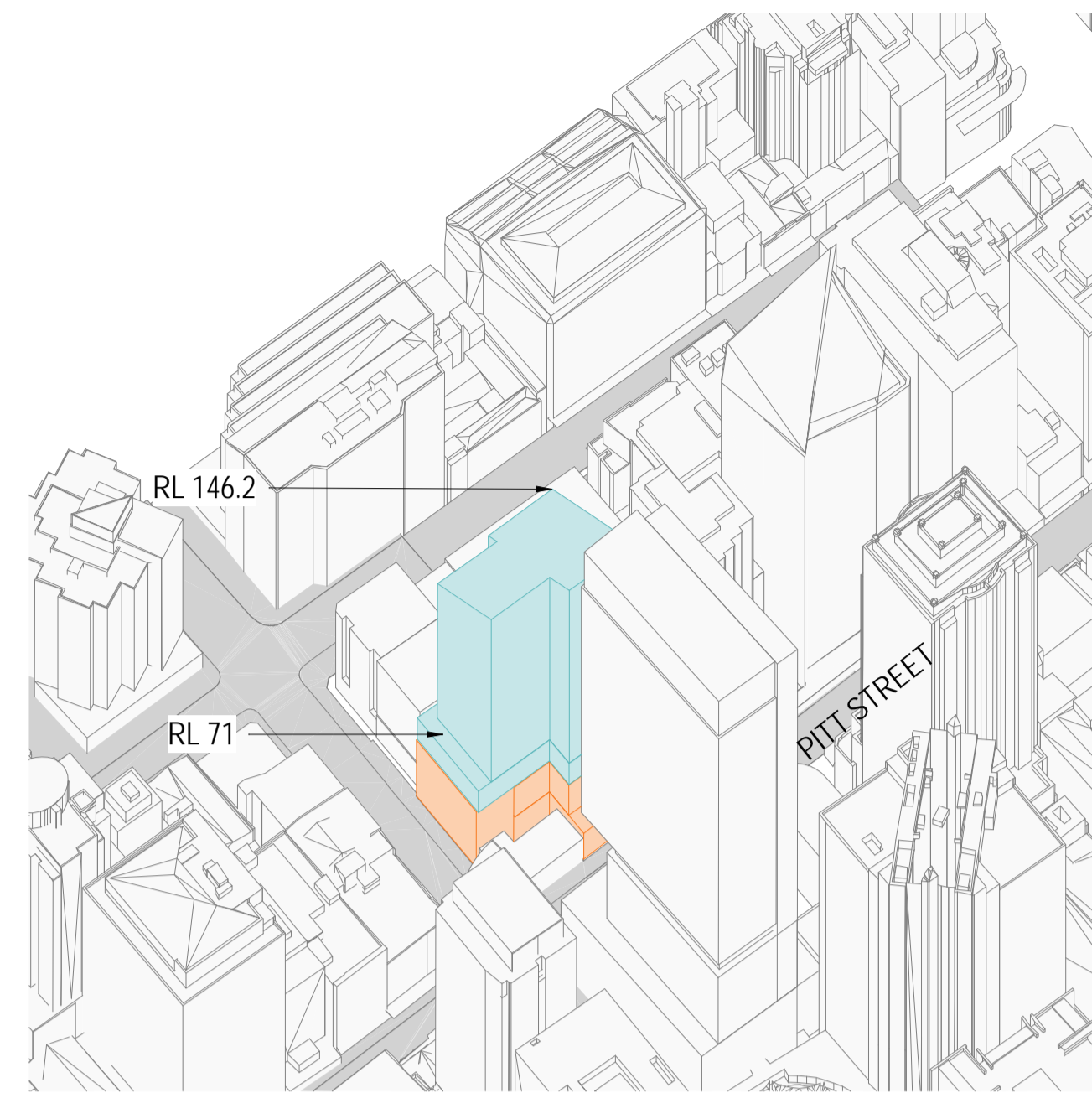
DRAFT



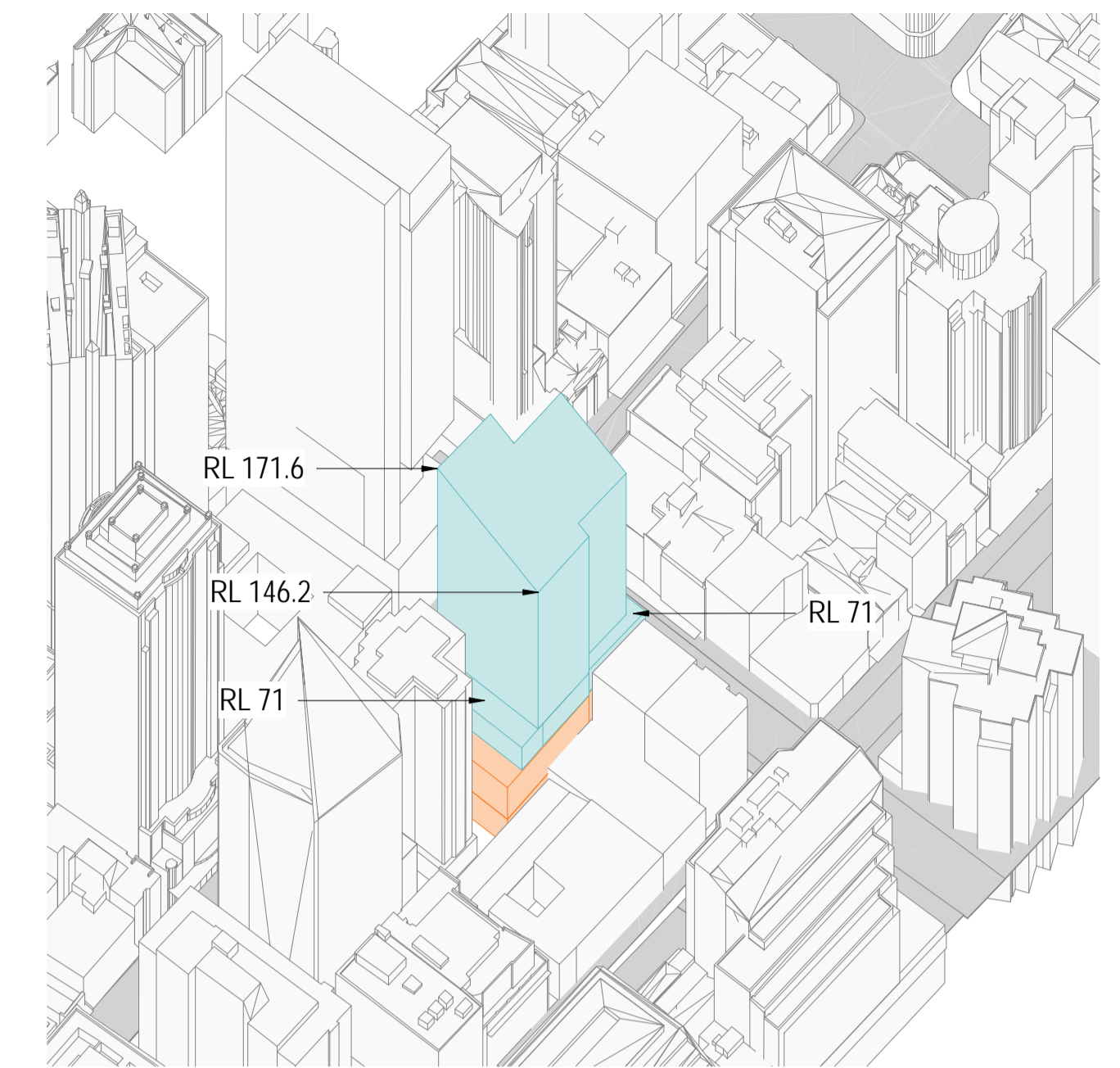
1 NORTH EAST



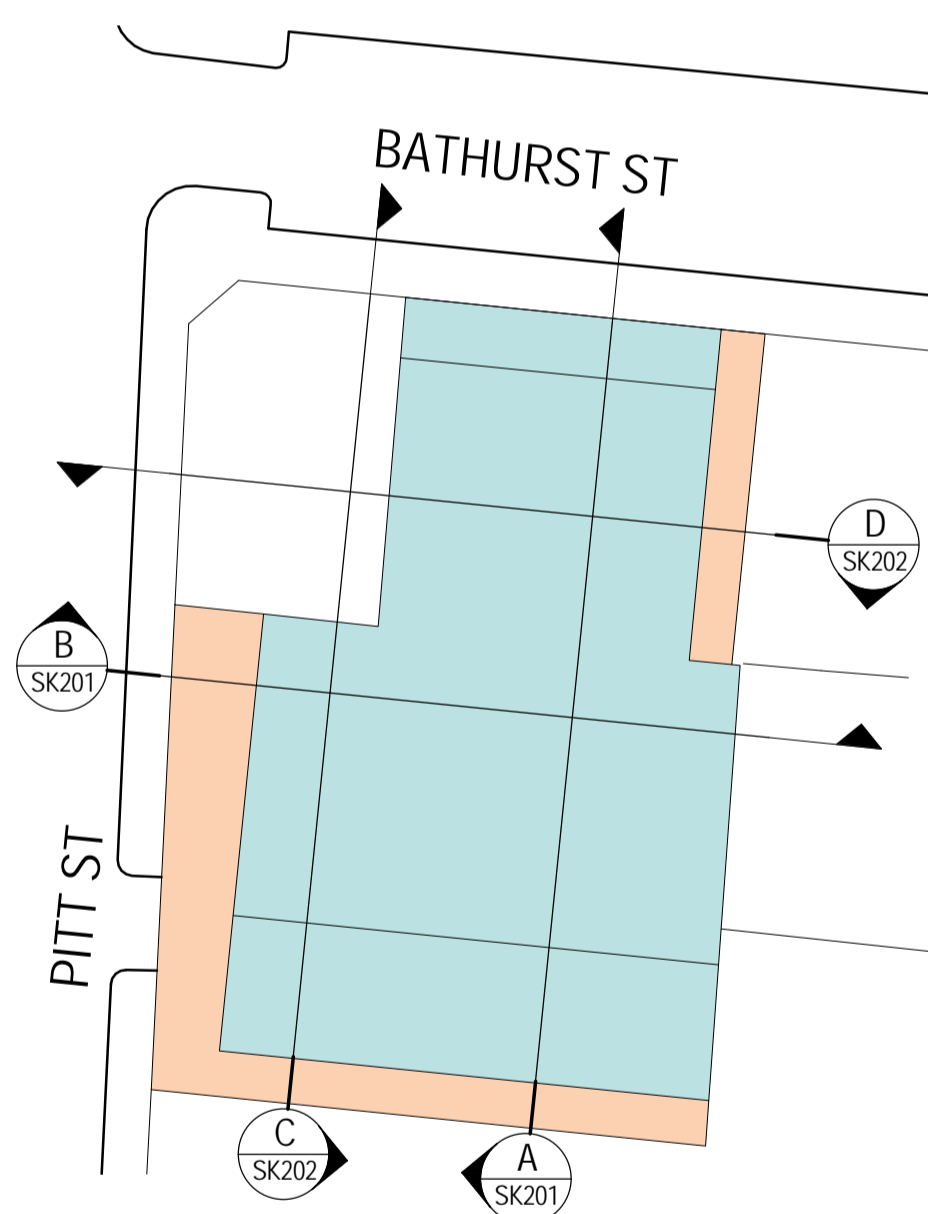
2 SOUTH WEST



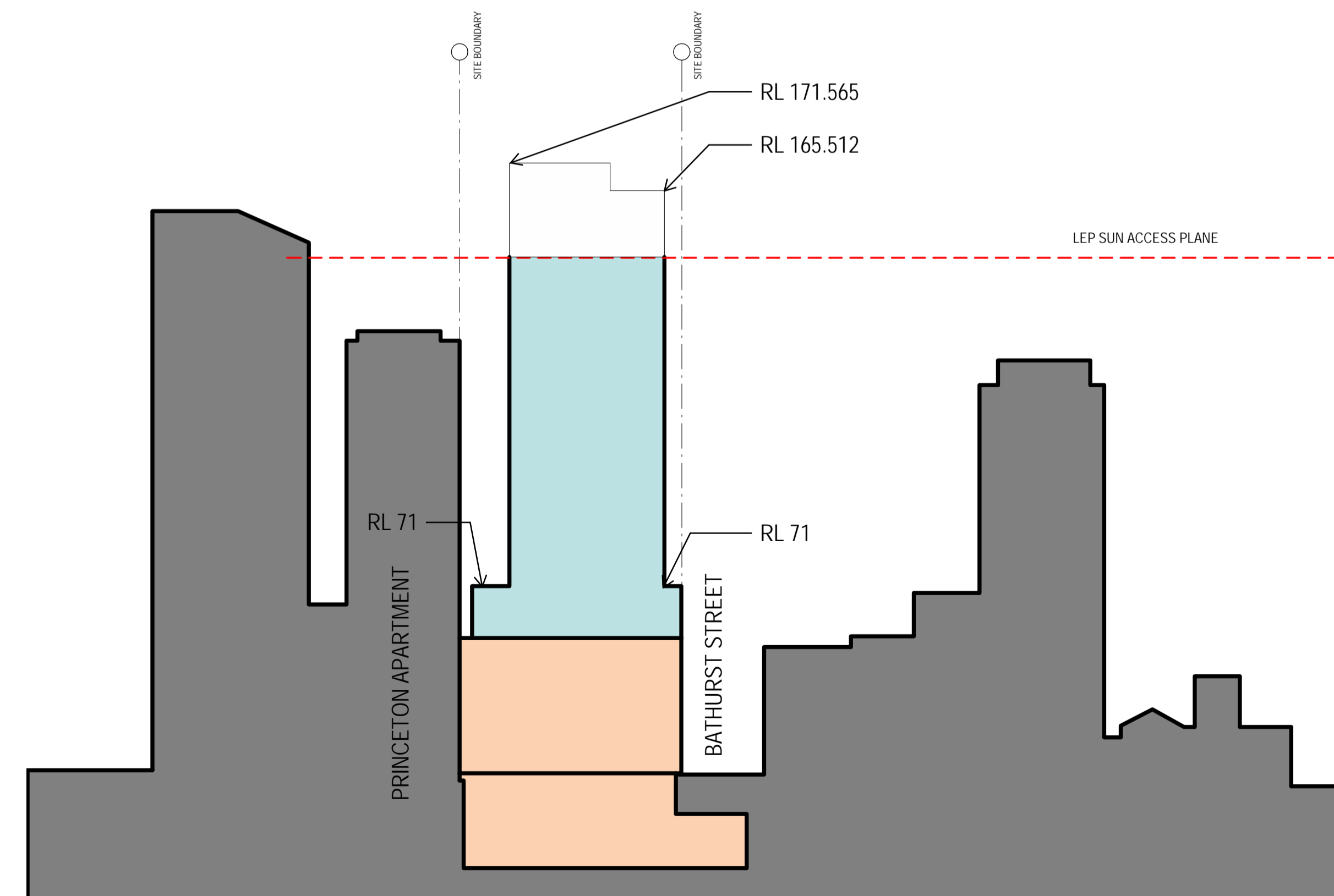
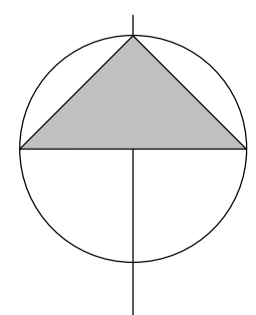
3 NORTH WEST



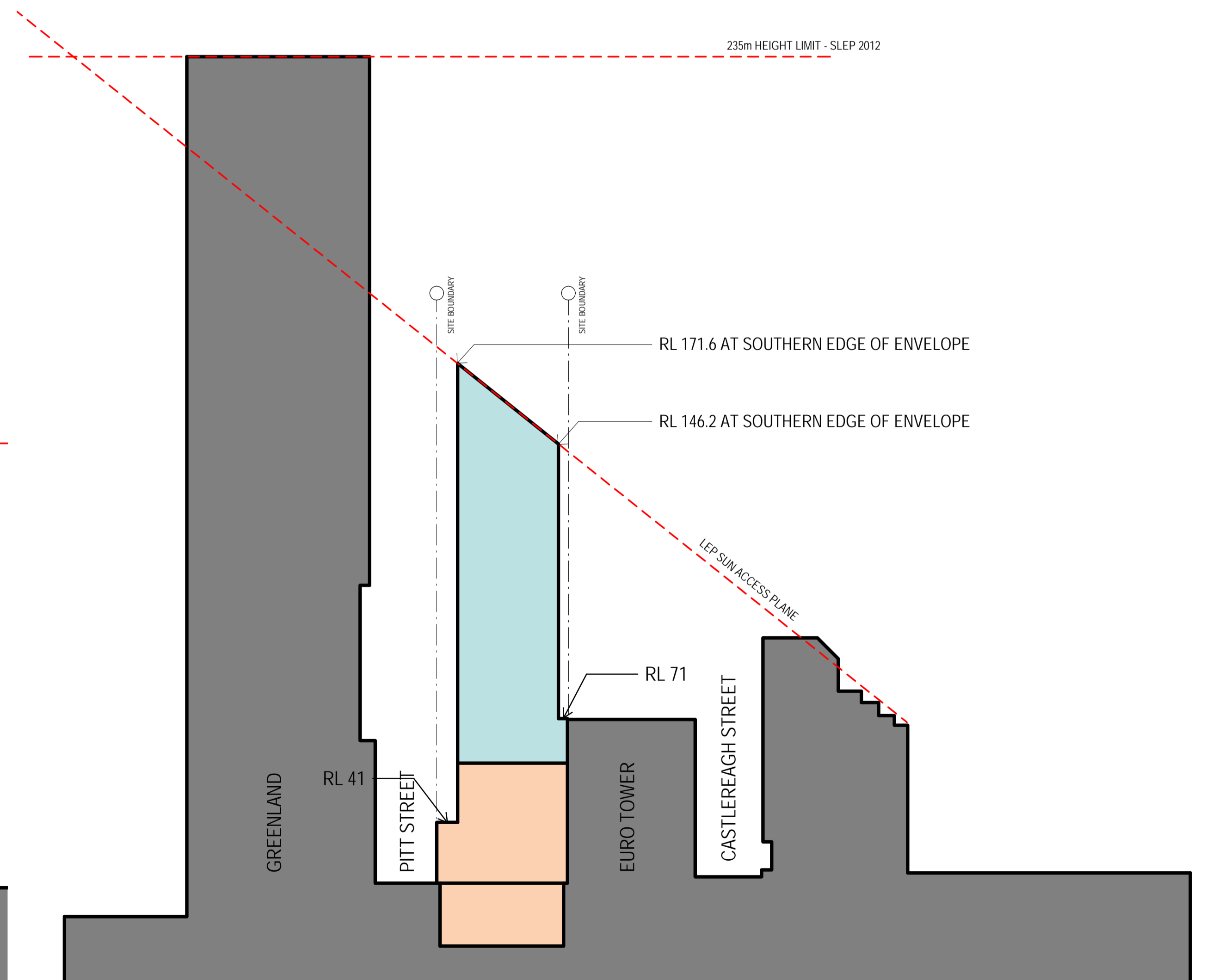
4 SOUTH EAST



- PITT STREET SOUTH METRO STATION CSSI
- OSD CONCEPT SSDA INCLUDES OSD AREAS INSIDE THE CSSI 'SHELL' BELOW GROUND AND IN PODIUM LEVELS

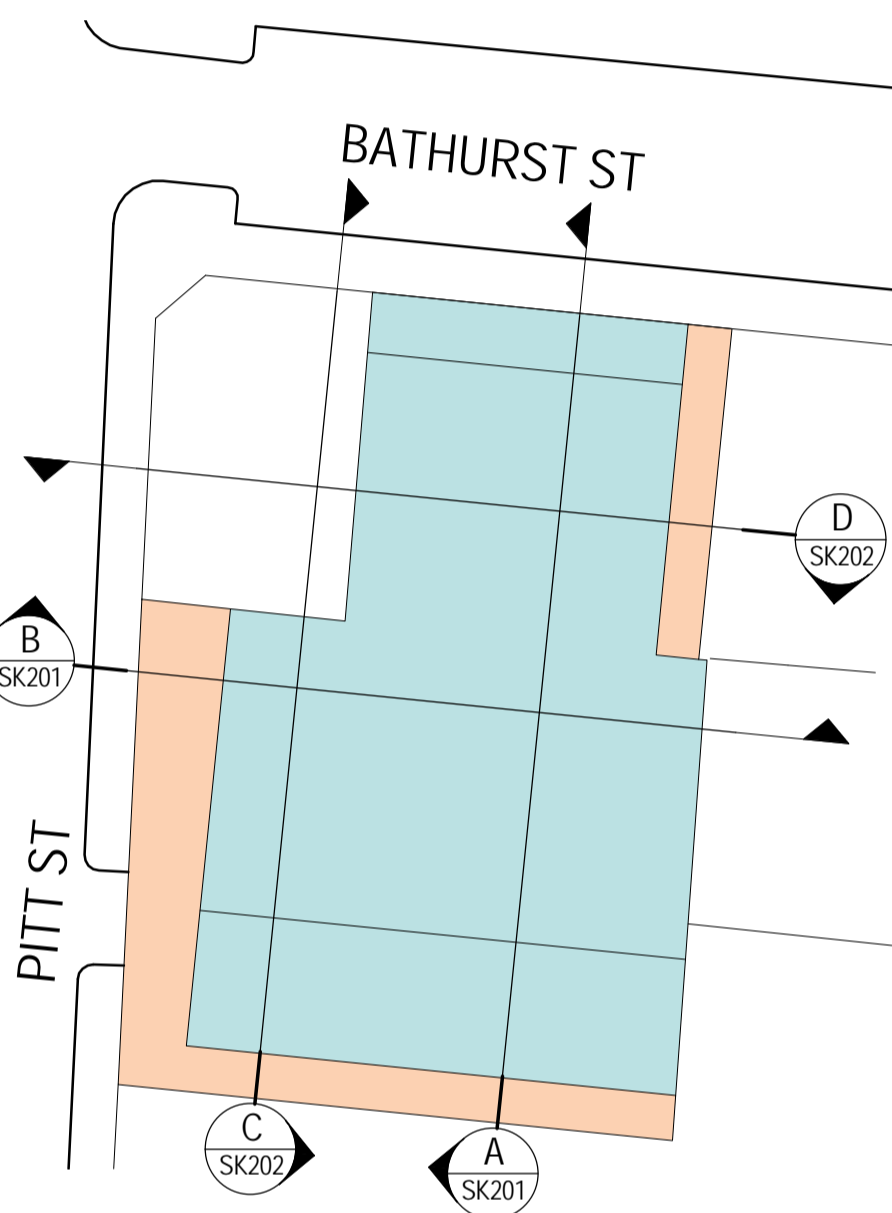


A SECTION A NORTH SOUTH
1:1000

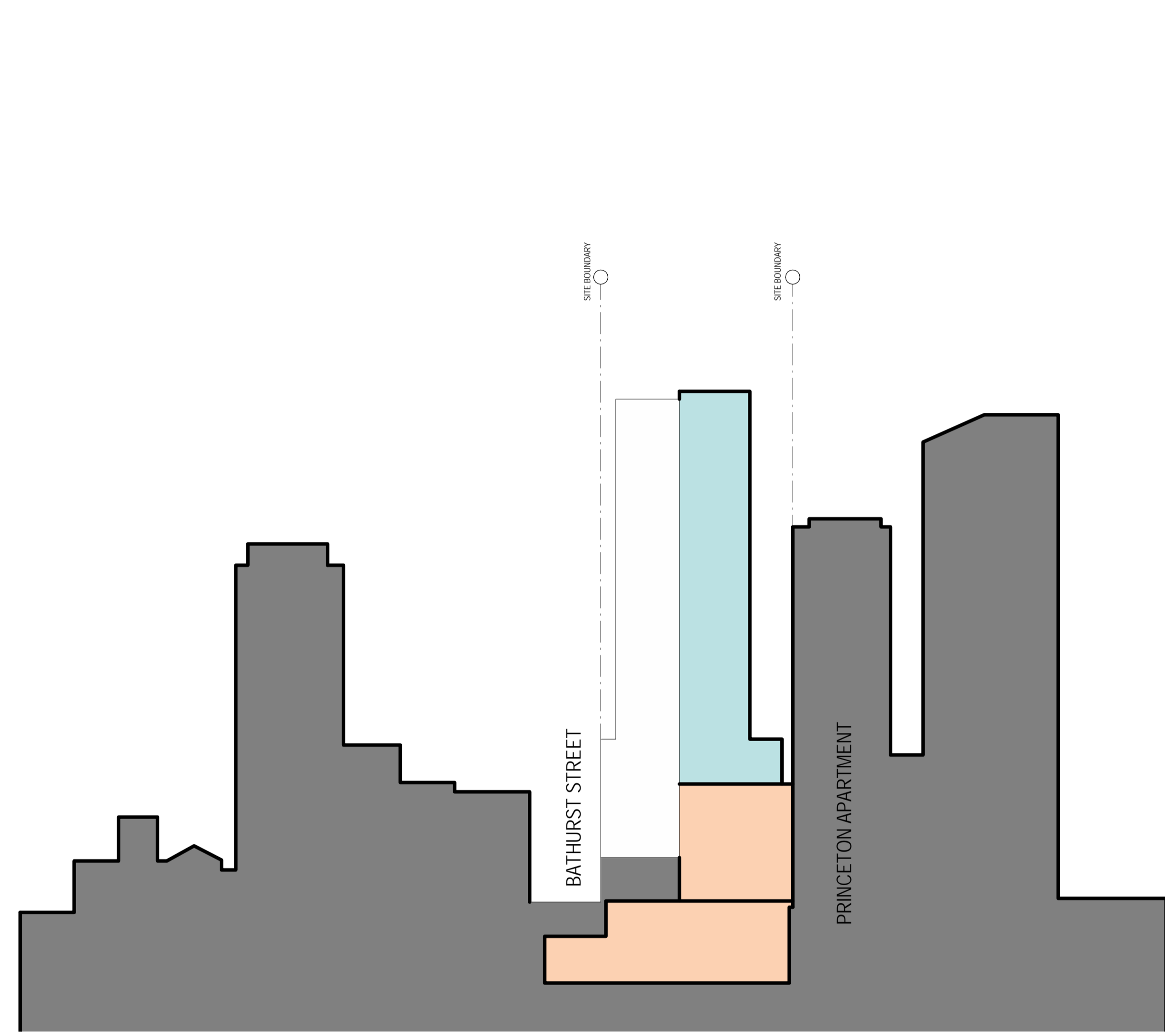


B SECTION B EAST WEST
1:1000

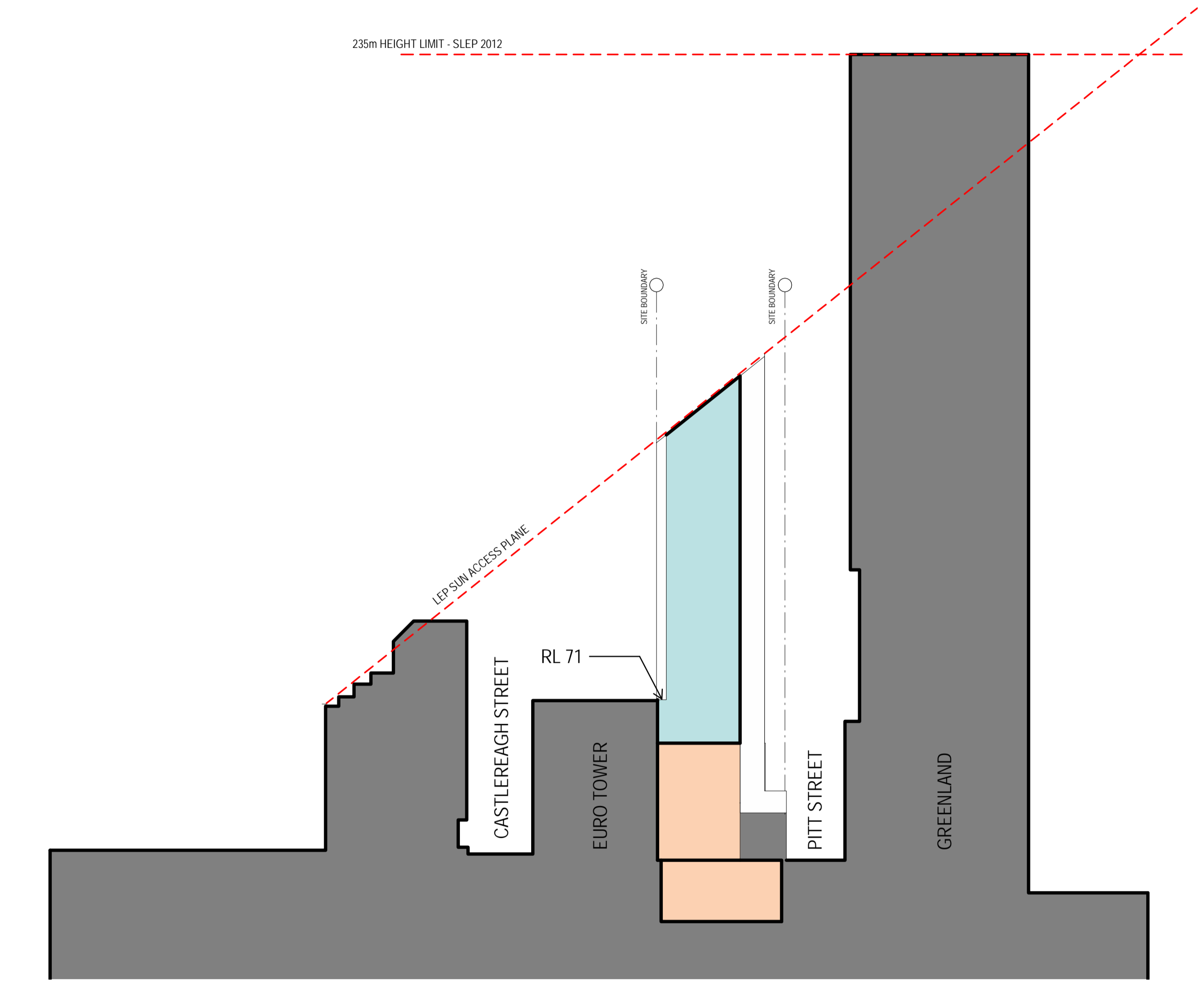
DRAFT



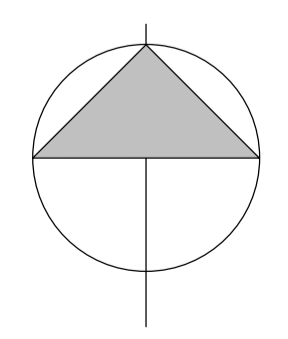
- PITT STREET SOUTH METRO STATION CSSI
- OSD CONCEPT SSDA INCLUDES OSD AREAS INSIDE THE CSSI 'SHELL' BELOW GROUND AND IN PODIUM LEVELS



C SECTION C NORTH SOUTH
1:1000



D SECTION D EAST WEST
1:1000



DRAFT

**COMMUNITY CONSULTATION
INFORMATION SESSION
CONTACT INFORMATION**





Pitt Street integrated station development

Better connecting our global city with new places to live, work and play in the heart of Sydney.

Sydney Metro is Australia's biggest public transport project.

This new standalone railway will deliver 31 metro stations and 66 kilometres of new metro rail, revolutionising the way Australia's biggest city travels.

The NSW Government has identified stations on the Sydney Metro system which can be better integrated with the areas around them and buildings above them, creating world-class places that will shape our city's future.

There are two concept proposals for above Pitt Street Station – one on the corner of Park and Pitt Street, referred to as Pitt Street North and one on the corner of Bathurst and Pitt Street, referred to as Pitt Street South.

The concept for Pitt Street North is a 43-storey mixed use building which could house a hotel, apartments and commercial office space. The concept for Pitt Street South is a 35-storey building which could house apartments or commercial office space.



sydneymetro.info
facebook.com/sydneymetro



Community information sessions

The Sydney Metro team will hold four community information sessions, while the concept State Significant Development applications for both Pitt Street North and Pitt Street South are on exhibition until 12 September 2018.

Expert members of the project team will be available to answer any questions you have. There is no need to make a booking – and you can call in at any time.

Tuesday, 28 August 2018 11am–2pm	Wesley Conference Centre 220 Pitt Street, Sydney
Thursday, 30 August 2018 4–7pm	Primus Hotel 339 Pitt Street, Sydney
Tuesday, 4 September 2018 11am–2pm	Primus Hotel 339 Pitt Street, Sydney
Thursday, 6 September 2018 4–7pm	Wesley Conference Centre 220 Pitt Street, Sydney

Better connecting our global city with new places to live, work and play in the heart of Sydney.

Sydney Metro is Australia's biggest public transport project. It will transform Sydney, delivering more trains and faster services for customers across the network.

The NSW Government has identified stations on the Sydney Metro system which can be better integrated with the area around them and buildings above them, creating world-class places that will shape our city's future.

There are two concept proposals for above Pitt Street Station - one on the corner of Park and Pitt Street, referred to as Pitt Street North and one on the corner of Bathurst and Pitt Street, referred to as Pitt Street South.

The concept proposal for the Pitt Street North site is a 43-storey mixed use building which could house a hotel, apartments and commercial office space.

The concept proposal for the Pitt Street South site is a 35-storey building which could house apartments or commercial office space.

Information sessions

The Sydney Metro team will hold four community information sessions while the concept State Significant Development applications for both Pitt Street North and Pitt Street South are on public exhibition until 12 September 2018.

There is no need to make a booking and you can call in at any time. Expert members of the project team will be available to answer any questions you have.

Community Information Sessions

Date and Time	Location
Tuesday, 28 August 2018, 11am–2pm	Wesley Conference Centre, 220 Pitt Street, Sydney
Thursday, 30 August 2018, 4–7pm	Primus Hotel, 339 Pitt Street, Sydney
Tuesday, 4 September 2018, 11am–2pm	Primus Hotel, 339 Pitt Street, Sydney
Thursday, 6 September 2018, 4–7pm	Wesley Conference Centre, 220 Pitt Street, Sydney

Better connecting our global city with new places to live, work and play in the heart of Sydney.

Sydney Metro is Australia's biggest public transport project. It will transform Sydney, delivering more trains and faster services for customers across the network.

The NSW Government has identified stations on the Sydney Metro system which can be better integrated with the area around them and buildings above them, creating world-class places that will shape our city's future.

There are two concept proposals for above Pitt Street Station - one on the corner of Park and Pitt Street, referred to as Pitt Street North and one on the corner of Bathurst and Pitt Street, referred to as Pitt Street South.

The concept proposal for the Pitt Street North site is a 43-storey mixed use building which could house a hotel, apartments and commercial office space.

The concept proposal for the Pitt Street South site is a 35-storey building which could house apartments or commercial office space.

Community information sessions

The Sydney Metro team will hold community information sessions while the concept State Significant Development applications for both Pitt Street North and Pitt Street South are on public exhibition until 12 September 2018.

There is no need to make a booking and you can call in at any time. Expert members of the project team will be available to answer any questions you have.

Community information sessions

Date and time	Location
Thursday, 30 August 2018, 4-7pm	Primus Hotel, 339 Pitt Street, Sydney
Tuesday, 4 September 2018, 11am-2pm	Primus Hotel, 339 Pitt Street, Sydney
Thursday, 6 September 2018, 4-7pm	Wesley Conference Centre, 220 Pitt Street, Sydney

**COMMUNITY
INFORMATION SESSION
A0 BOARDS**

APPENDIX D



Welcome to this information session

Our staff will be happy to assist you



Revolutionising how we travel



The customer is at the centre of the Sydney Metro rail product.

Key benefits

Metro delivers improved connectivity across multiple levels:

- moving more people faster than ever before
- fast and easy to change to trains, buses, ferries and light rail
- making it fast and easy to move around a growing city
- making a big city better connected
- aligning and integrating with other city-building transport projects
- opening up options for work, education, health care and much more
- taking the guesswork out of travel – no need to plan, just turn up and go.

Australia's biggest public transport project



sydneymetro.info
 1800 171 386

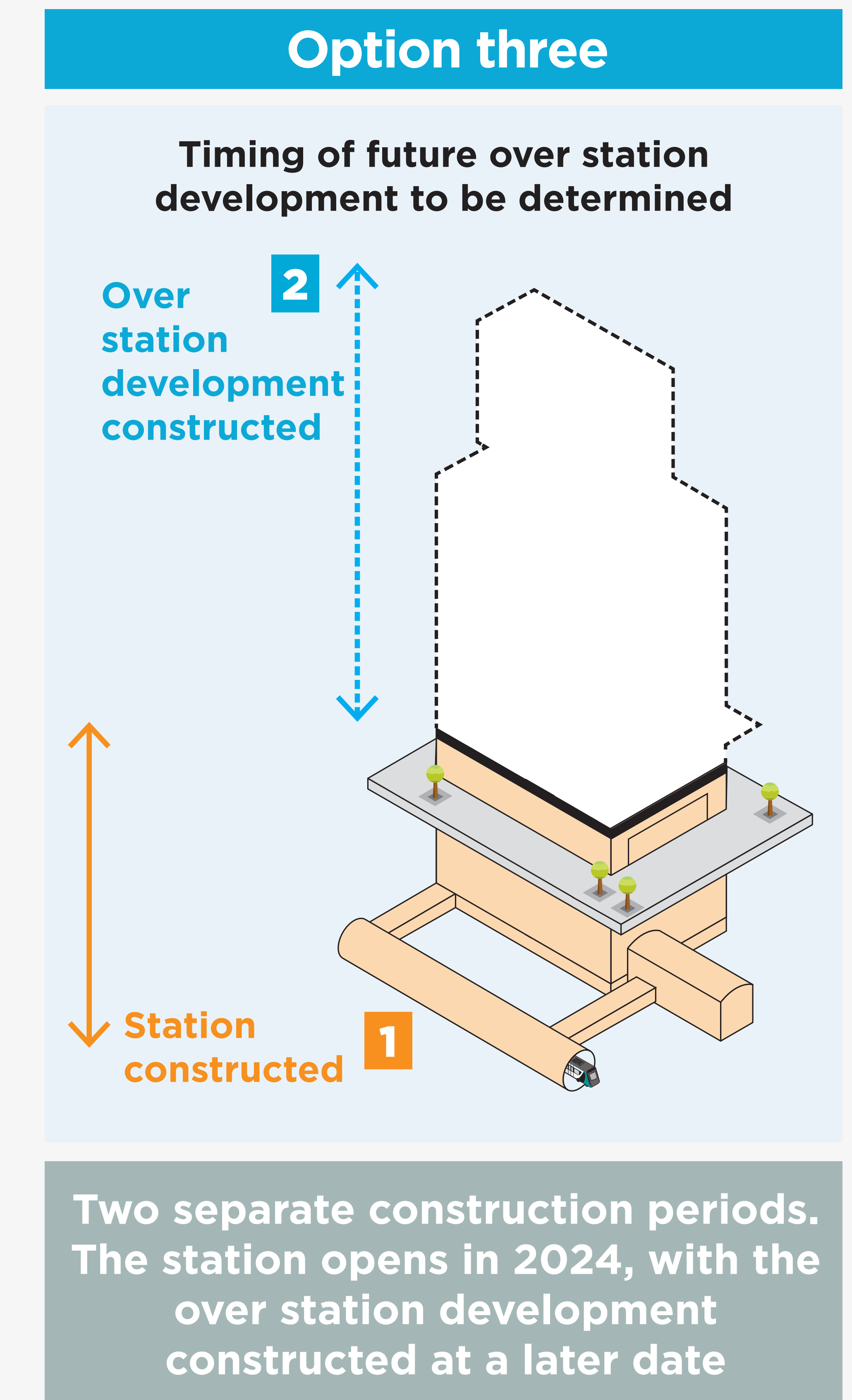
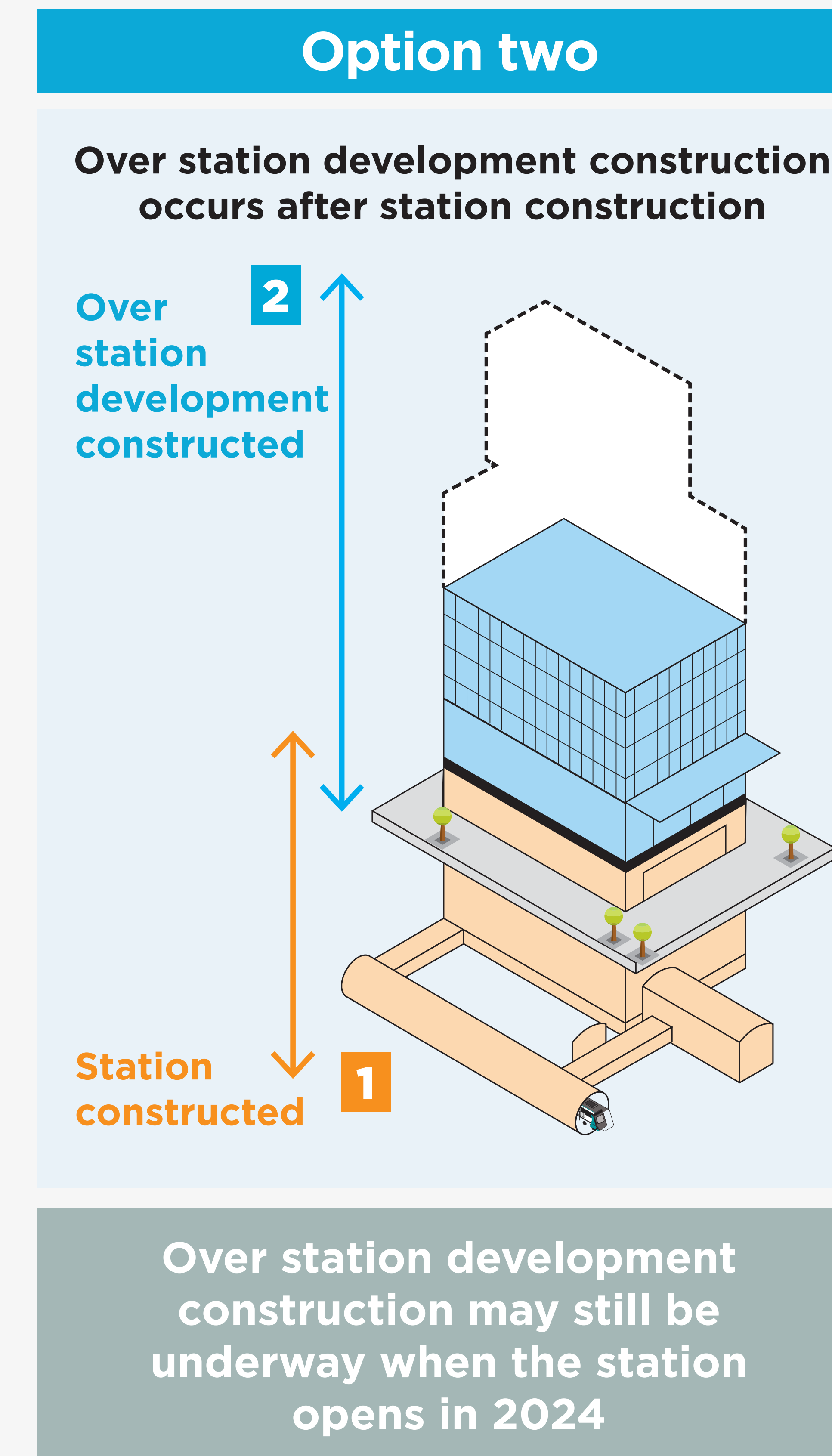
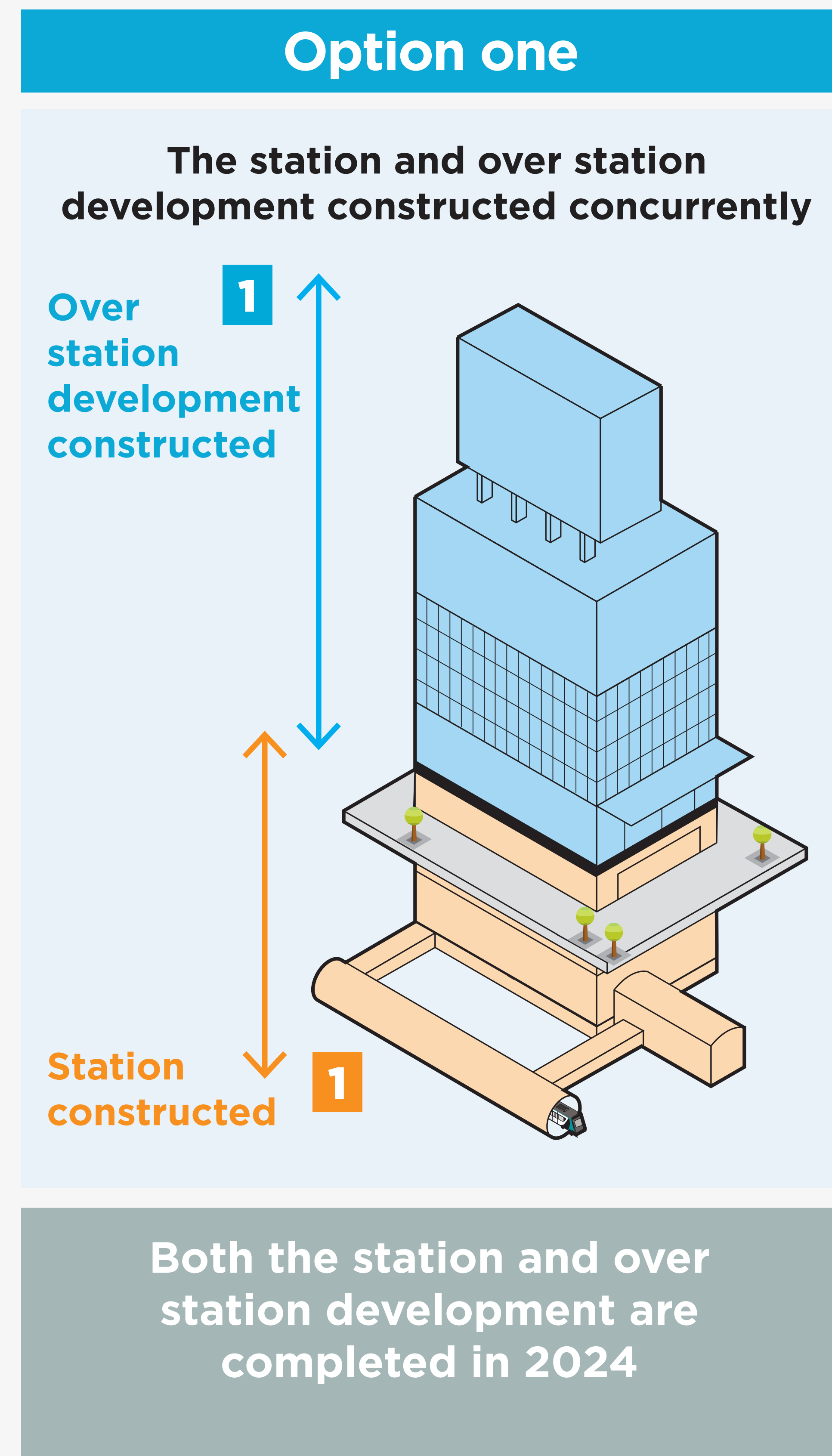
Integrated station developments

As the new metro stations are built underground, the over station developments can be built above them at the same time.

This helps reduce community impacts and the length of time required to deliver the outcomes of integrated station developments.

The metro stations have been designed so that work on the over station developments can start while the station construction is underway. This integrated approach means buildings can be completed close to when Sydney Metro City & Southwest services start in 2024.

Option one is preferred because it delivers the integrated development at the earliest date and minimises construction impacts.



Excellence in design



New city icons

- Development that shapes Sydney's growth and identity
- Contributing to Sydney's reputation for design excellence
- Leaving a lasting legacy



Vibrant public places

- Welcoming and inclusive places for social and cultural interaction
- Opportunities to create places and buildings that celebrate Sydney's culture and values
- Contributing to the vibrant and accessible streets and open space around stations



Integrated and inclusive

- Maximising opportunities for land use and transport integration
- Meeting the increasing demand for well-located residential, commercial, retail and community precincts
- Contributing to economic growth through appropriate land use outcomes
- Supporting opportunities for the expansion, improvement and activation of public open spaces



Designed for the future

- Buildings and surrounding spaces will be of the highest quality, exceeding community expectations now and into the future
- New infrastructure and public spaces will be durable, hard wearing and easy to maintain
- Sustainability will be embedded into initiatives



Safety and accessibility

- Day and night time activation will make precincts vibrant, including safe access
- A sense of security will ensure people can see and be seen

Reimagining the heart of the city

The Pitt Street integrated station development will better connect our global city, with new places to live, work and play in the heart of Sydney.



Artist's impression of the Pitt Street North integrated station development

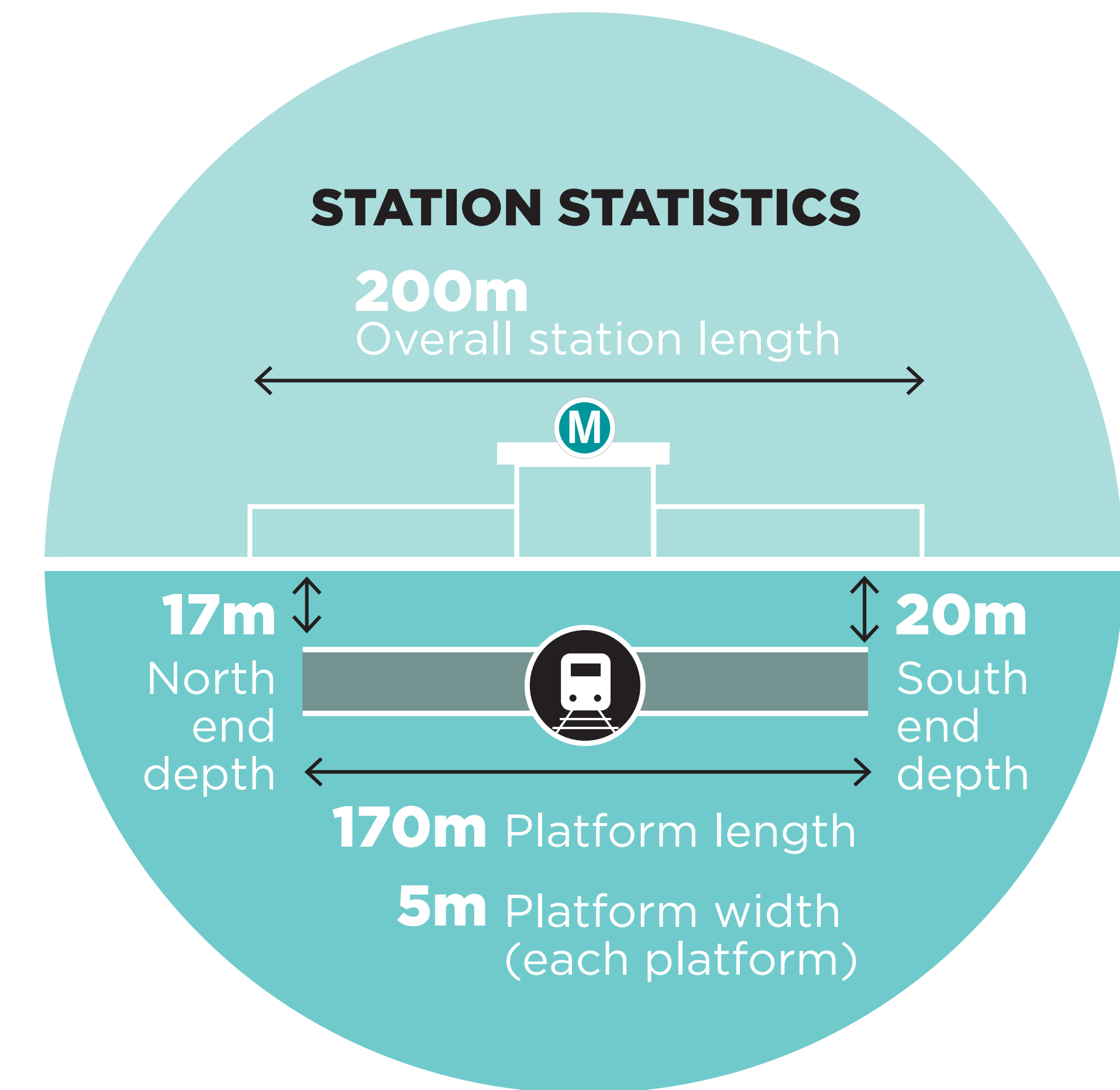


sydneymetro.info
1800 171 386

Pitt Street integrated station development

- 2** Minutes to **CENTRAL**
- 4** Minutes to **BARANGAROO**
- 7** Minutes to **VICTORIA CROSS**

00:04
A train every four minutes in the peak - with ultimate capacity for a train every two minutes in each direction



9400 PEOPLE
 will move through Pitt Street Station in the morning peak

UP TO 1800 PERMANENT JOBS

UP TO 600 NEW HOMES

UP TO 200 HOTEL ROOMS

Nearby transport interchanges



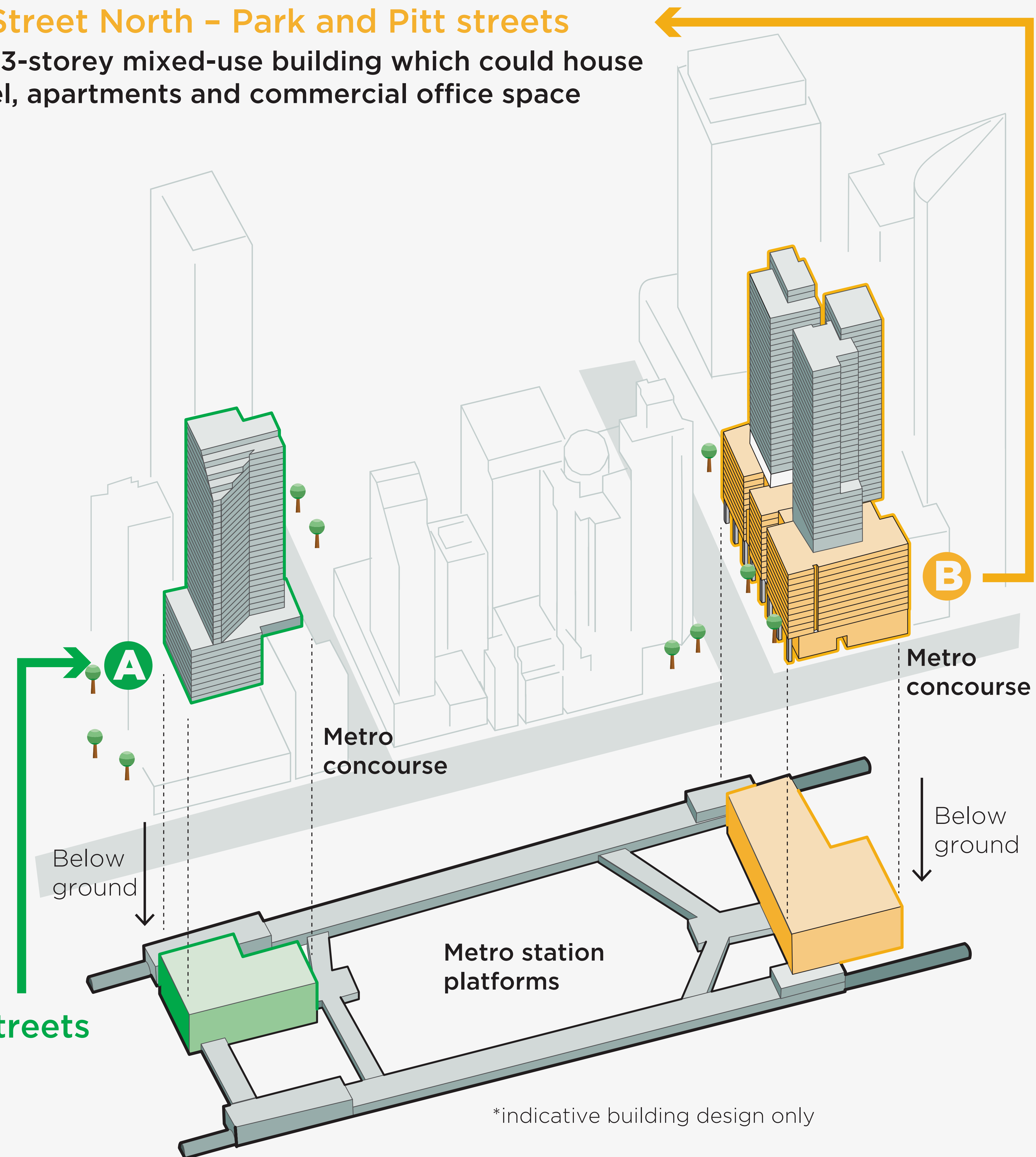
Pitt Street integrated station development

There are two concept proposals for above Pitt Street Station – one on the corner of Park and Pitt streets at the northern entry to Pitt Street Station and one on the corner of Bathurst and Pitt streets at the southern entry to Pitt Street Station.

These sites are referred to as Pitt Street North and Pitt Street South. Each concept proposal is being assessed separately and has its own State Significant Development application and Environmental Impact Statement.

Pitt Street North – Park and Pitt streets

One 43-storey mixed-use building which could house a hotel, apartments and commercial office space



Pitt Street South – Bathurst and Pitt streets

One 35-storey building which could house apartments or commercial office space

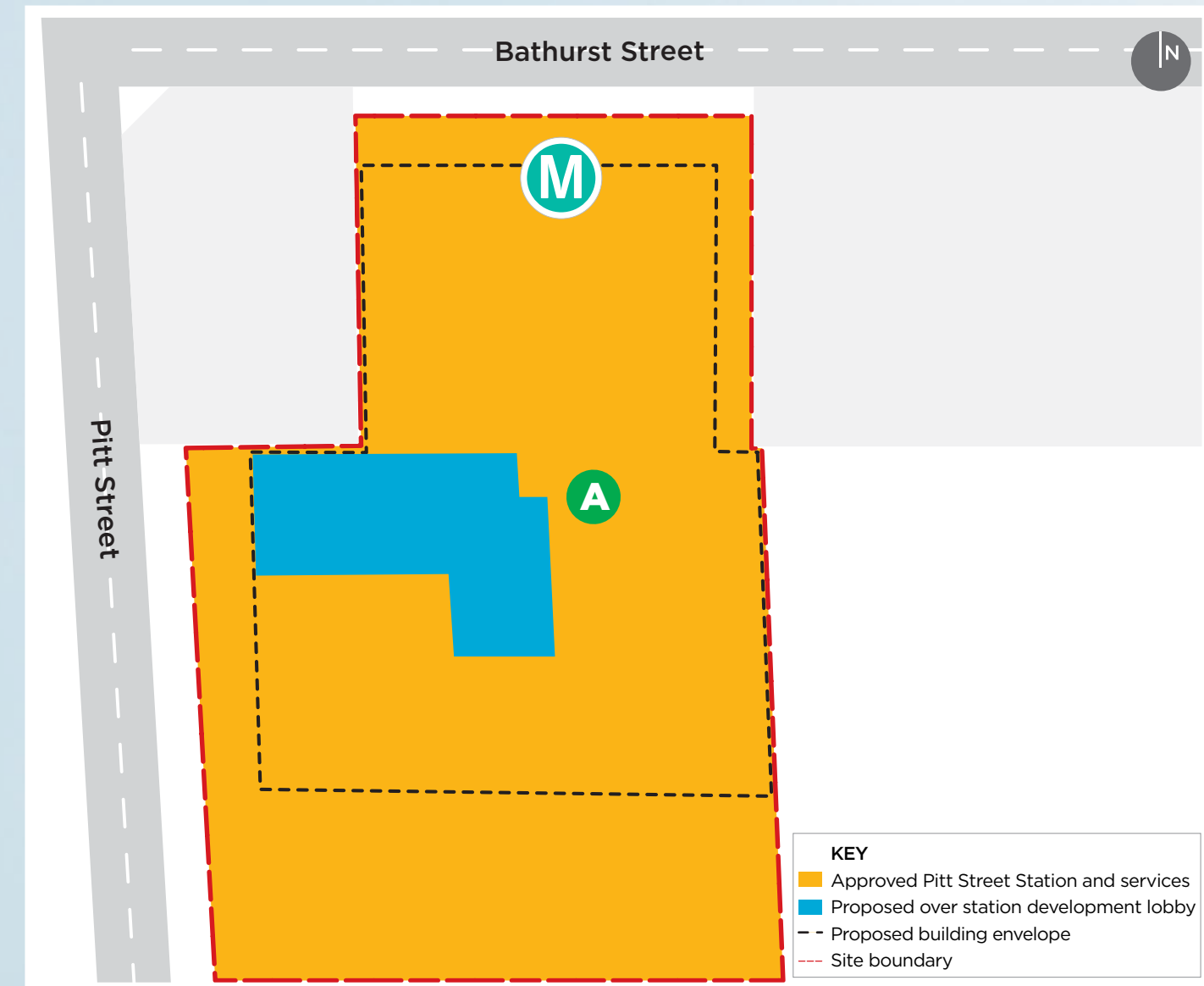
Pitt Street concept proposals



A

Pitt Street South

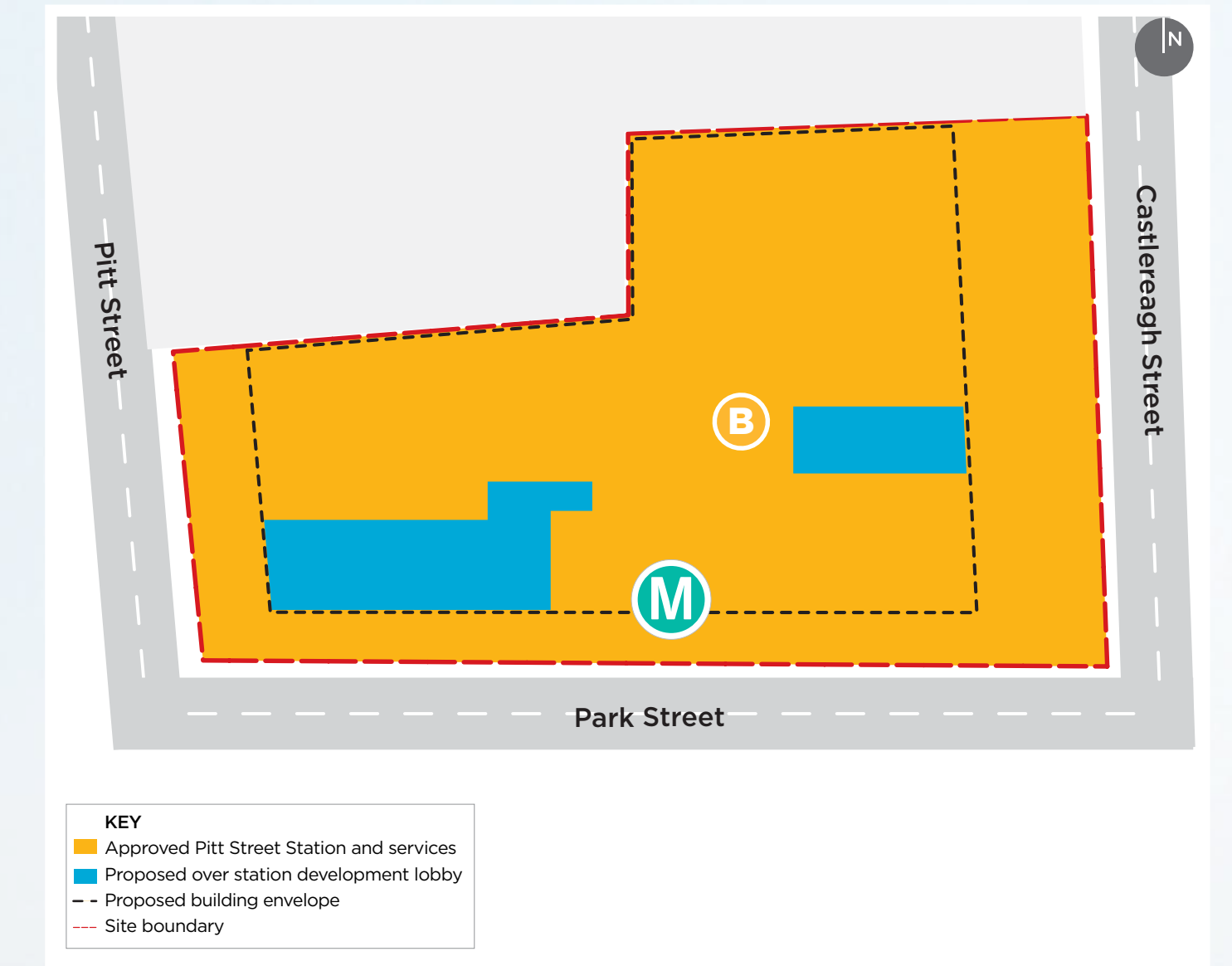
One 35-storey building which could house apartments or commercial office space



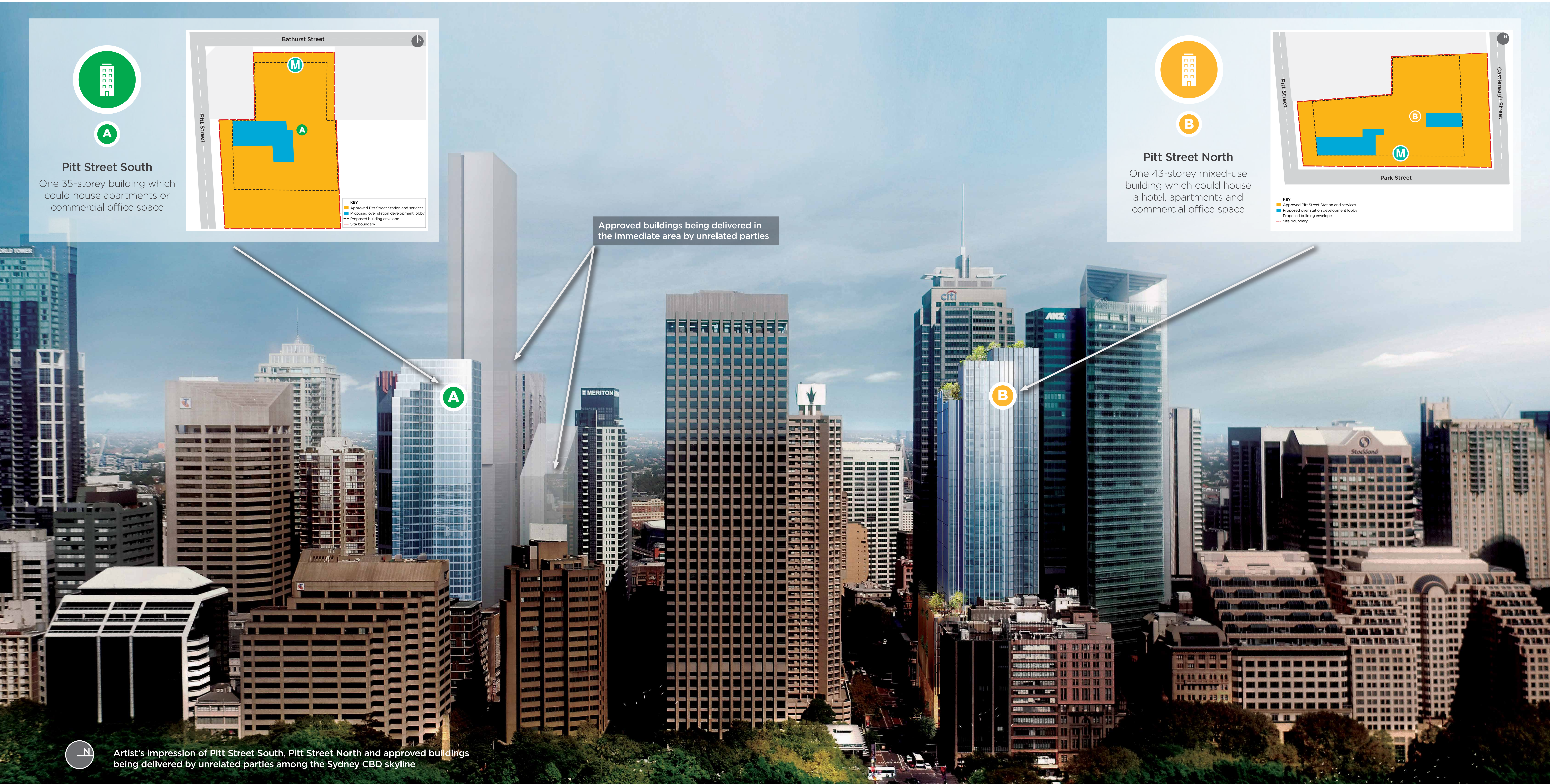
B

Pitt Street North

One 43-storey mixed-use building which could house a hotel, apartments and commercial office space



Approved buildings being delivered in the immediate area by unrelated parties



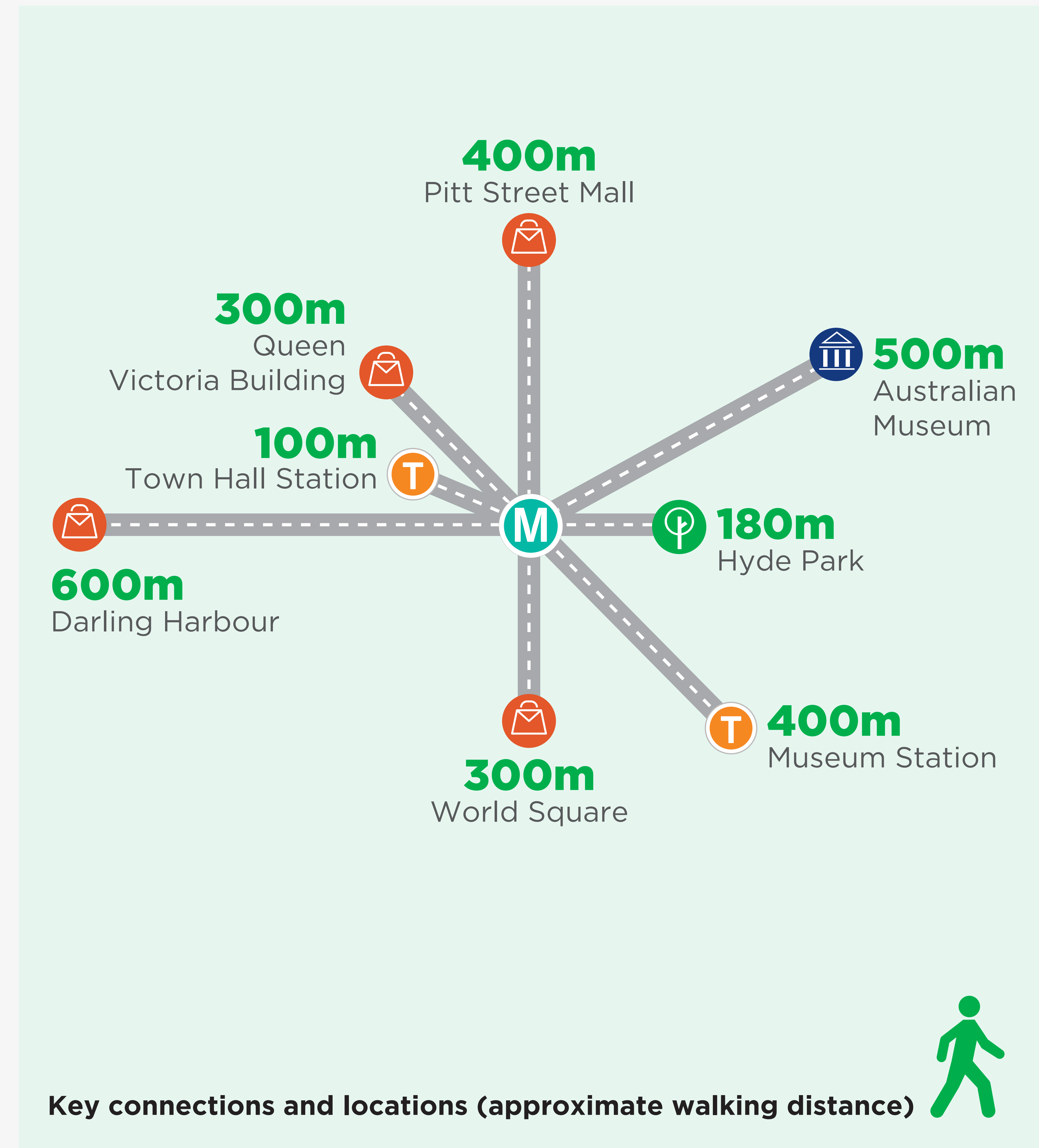
Artist's impression of Pitt Street South, Pitt Street North and approved buildings being delivered by unrelated parties among the Sydney CBD skyline



sydneymetro.info
1800 171 386

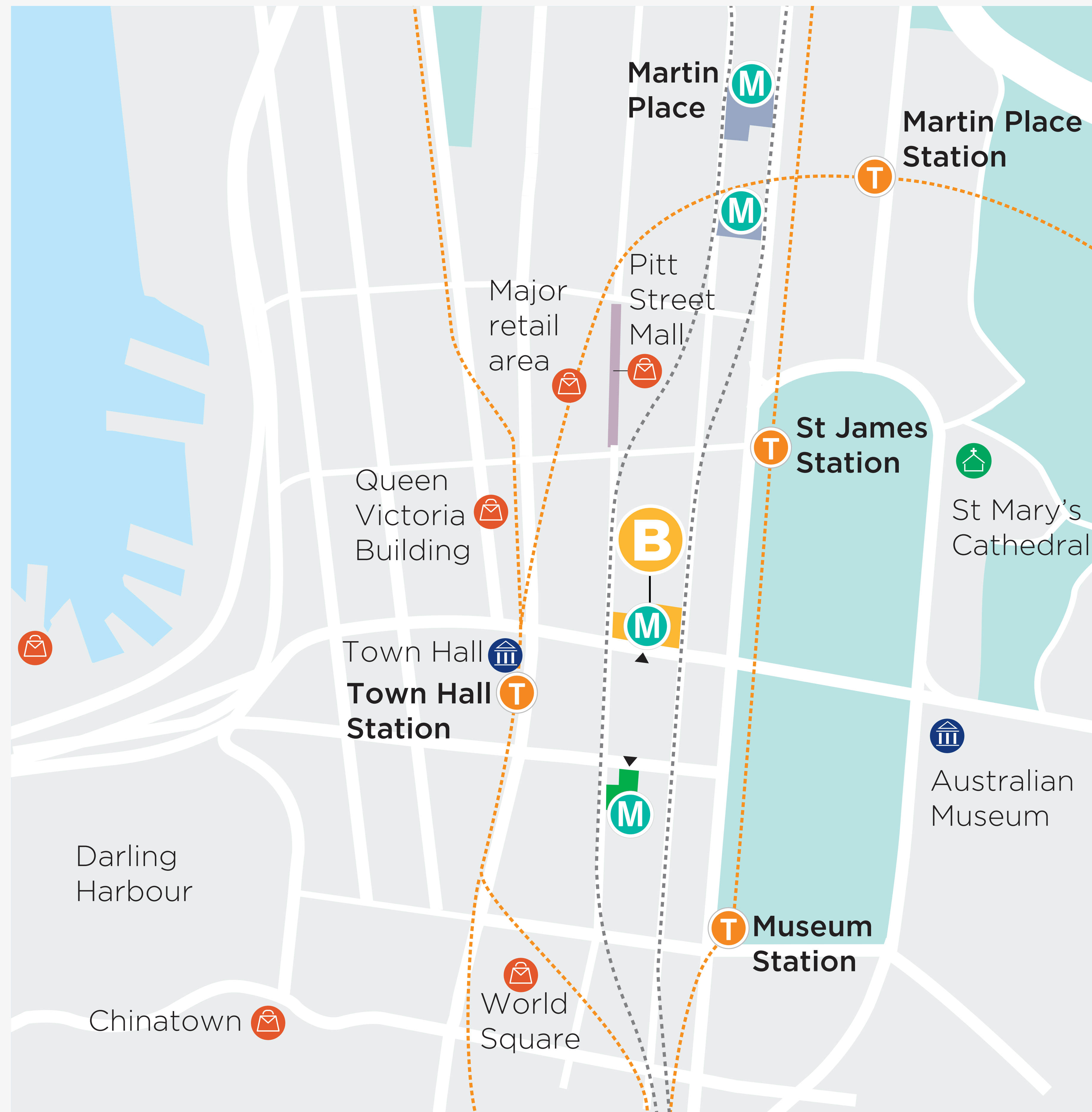
Pitt Street South

Bathurst and Pitt streets



Pitt Street North

Park and Pitt streets



Pitt Street North Environmental Impact Statement

The Environmental Impact Statement for Pitt Street North contains information about the proposed buildings above the new metro station at Pitt Street, as well as an assessment of possible environmental impacts. This is a summary of the key aspects of the Environmental Impact Statement.



The building

The concept includes a 43-storey mixed use building that will integrate with the metro station below. Building uses will include up to 1,500 square metres of office space, up to 300 apartments and up to 200 hotel rooms.



Views

The most prominent view of the Pitt Street North building will be from the east, where it will not be obscured by other buildings. This includes views of the building from Hyde Park. From most vantage points, the building is consistent with the city skyline and the high rise character of the Sydney CBD.

Two residential buildings to the south, 27 Park Street and 197 Castlereagh Street, will have changed views to the north and north-west as a result of the Pitt Street North building. Some apartments in 27 Park Street will see the top portion of the Pitt Street North building where they now see sky.

Views towards Hyde Park, Sydney Harbour or North Head from 27 Park Street will not be impacted. At 197 Castlereagh Street there will be reduced views of Piccadilly Tower, Sydney Tower and the MLC Centre.



Overshadowing

The building will not have any significant overshadowing impacts on the majority of surrounding residential apartments; however it will cause increased overshadowing to lower level residential apartments on the western side of the future building at 201 Elizabeth Street.

The building will also cause increased overshadowing to the northern side of the residential building at 329 Pitt Street and the north and western facing facades of 27 Park Street.

Pitt Street North Environmental Impact Statement



Traffic impacts

Traffic impacts for the Pitt Street North building will also be relatively minor, with provision for about 50 car spaces and loading and servicing facilities.

There are not expected to be any adverse impacts on the performance of the surrounding road network from traffic generation at the site.



Heritage

The Pitt Street North building will have heritage buildings nearby including the Criterion Hotel, the National Building and the Masonic Club.

Significant views to, from or between heritage items will not be reduced or obstructed when compared to the buildings previously on the site.



Mitigation measures

Mitigation measures have been outlined to manage the impacts for each stage of future design development and construction for the Pitt Street South and Pitt Street North buildings.

These mitigation measures include:

- adhering to the principles outlined in the design excellence strategy, including a design that demonstrates quality and best practice against international standards
- strategies to protect and enhance local heritage and ensure the development achieves a positive heritage outcome for the buildings



Hyde Park

- implementing best-practice sustainability initiatives during design and construction, which could include installation of acoustic treatments to internal and external elements of the buildings
- measures to minimise noise and vibration once the buildings are in operation, including reducing noise at the loading docks and any acoustic treatment required for plant and equipment
- wind-tunnel testing to model wind speeds and inform mitigation strategies
- an assessment of building façade treatments and their sun reflection potential on motorists and pedestrians
- development of strategies to mitigate potential privacy impacts on nearby residential apartments
- adherence to recommendations in the traffic impact assessments to mitigate any traffic impacts
- waste management strategies
- site accessibility provisions
- a Construction Environmental Management Framework to identify and mitigate construction impacts - this will include mitigation measures to manage noise, vibration, dust and traffic.

Pitt Street South Environmental Impact Statement

The Environmental Impact Statement for Pitt Street South contains information about the proposed buildings above the new metro station at Pitt Street, as well as an assessment of possible environmental impacts. This is a summary of the key aspects of the Environmental Impact Statement.



The building

The concept includes a 35-storey building that will integrate with the metro station below. The building will include up to 16,250 square metres of office space and 11 car spaces, or up to 159 residential apartments and 34 car spaces.



Views

The most prominent long-distance views of the proposed envelope will be from the east, where the building will not be obscured by other buildings. The building is well suited to the surrounding context and will not interrupt any key public view corridors across the Sydney CBD.

Some of the apartments in the Princeton Apartments, Century Towers and the future Greenland Centre residential apartment buildings will have changed views, mostly at the mid-rise levels of the buildings.



Overshadowing

The building will not have any significant overshadowing impacts on the majority of surrounding residential apartments; however it will cause increased overshadowing of some units on the northern side of the Princeton Apartments.

These impacts were addressed at the time of the Princeton Apartments development. The original building approval for the Princeton Apartments required that potential occupants with north facing windows be notified that views and sun access from those windows will be affected by any development on the proposed site.

Pitt Street South Environmental Impact Statement



Traffic impacts

Traffic impacts for the Pitt Street South building will be relatively minor, with provision for about 34 car spaces and loading and servicing facilities.

There are not expected to be any adverse impacts on the performance of the surrounding road network from traffic generation at the site.



Heritage

The Pitt Street South building will have heritage buildings nearby including the Edinburgh Castle Hotel, the Metropolitan Fire Brigade building, the former Speedwell House, the former YMCA building and the former Sydney Water building, now the Primus Hotel.

Significant views to, from or between heritage items will not be reduced or obstructed when compared to the buildings previously on the site.

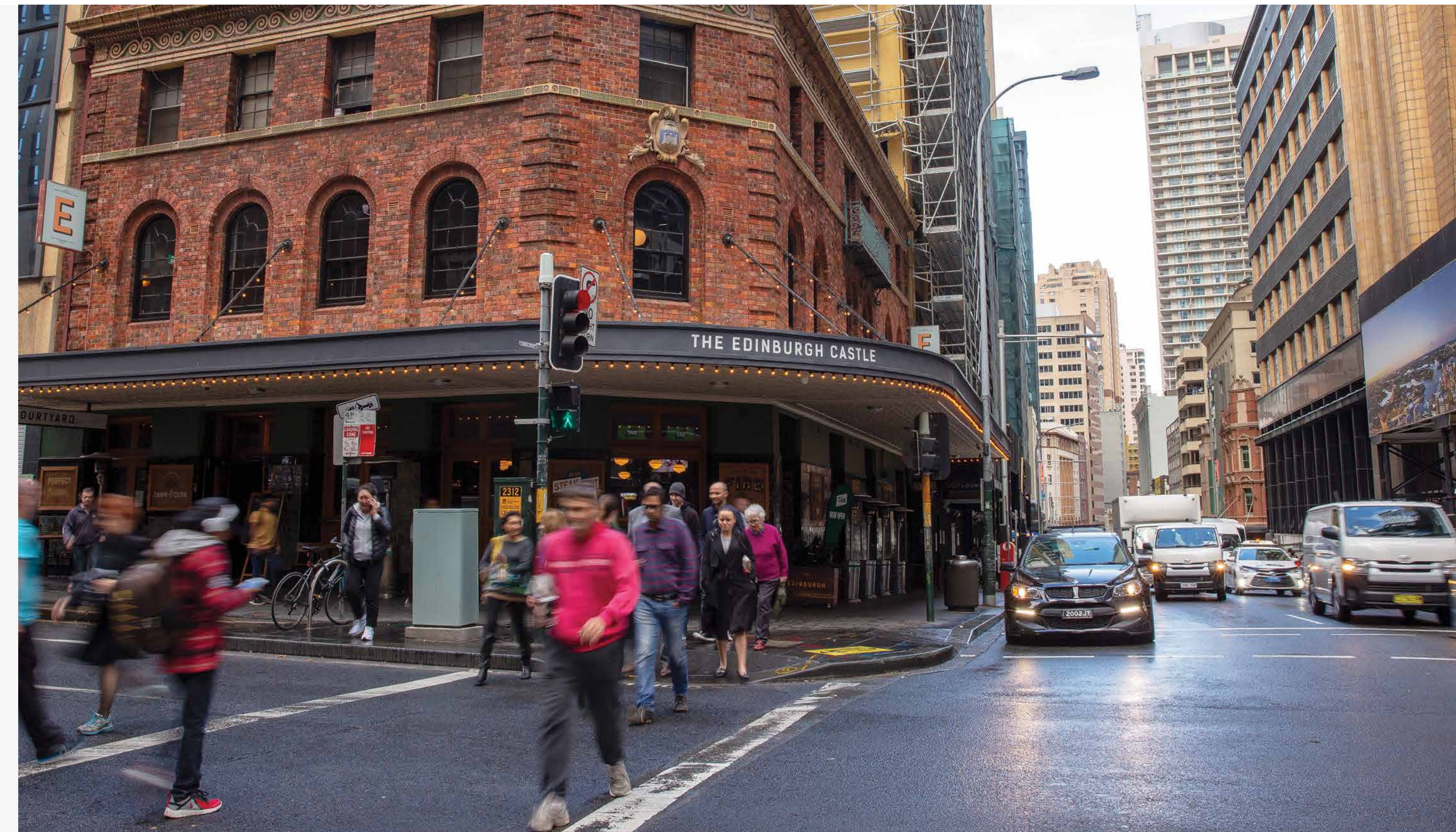


Mitigation measures

Mitigation measures have been outlined to manage the impacts for each stage of future design development and construction for the Pitt Street South and Pitt Street North buildings.

These mitigation measures include:

- adhering to the principles outlined in the design excellence strategy, including a design that demonstrates quality and best practice against international standards
- strategies to protect and enhance local heritage and ensure the development achieves a positive heritage outcome for the buildings



The Edinburgh Castle Hotel heritage building next to the proposed Pitt Street South integrated station development

- implementing best-practice sustainability initiatives during design and construction, which could include installation of acoustic treatments to internal and external elements of the buildings
- measures to minimise noise and vibration once the buildings are in operation, including reducing noise at the loading docks and any acoustic treatment required for plant and equipment
- wind-tunnel testing to model wind speeds and inform mitigation strategies
- an assessment of building façade treatments and their sun reflection potential on motorists and pedestrians
- development of strategies to mitigate potential privacy impacts on nearby residential apartments
- adherence to recommendations in the traffic impact assessments to mitigate any traffic impacts
- waste management strategies
- site accessibility provisions
- a Construction Environmental Management Framework to identify and mitigate construction impacts - this will include mitigation measures to manage noise, vibration, dust and traffic.

The planning process

What is a concept approval?

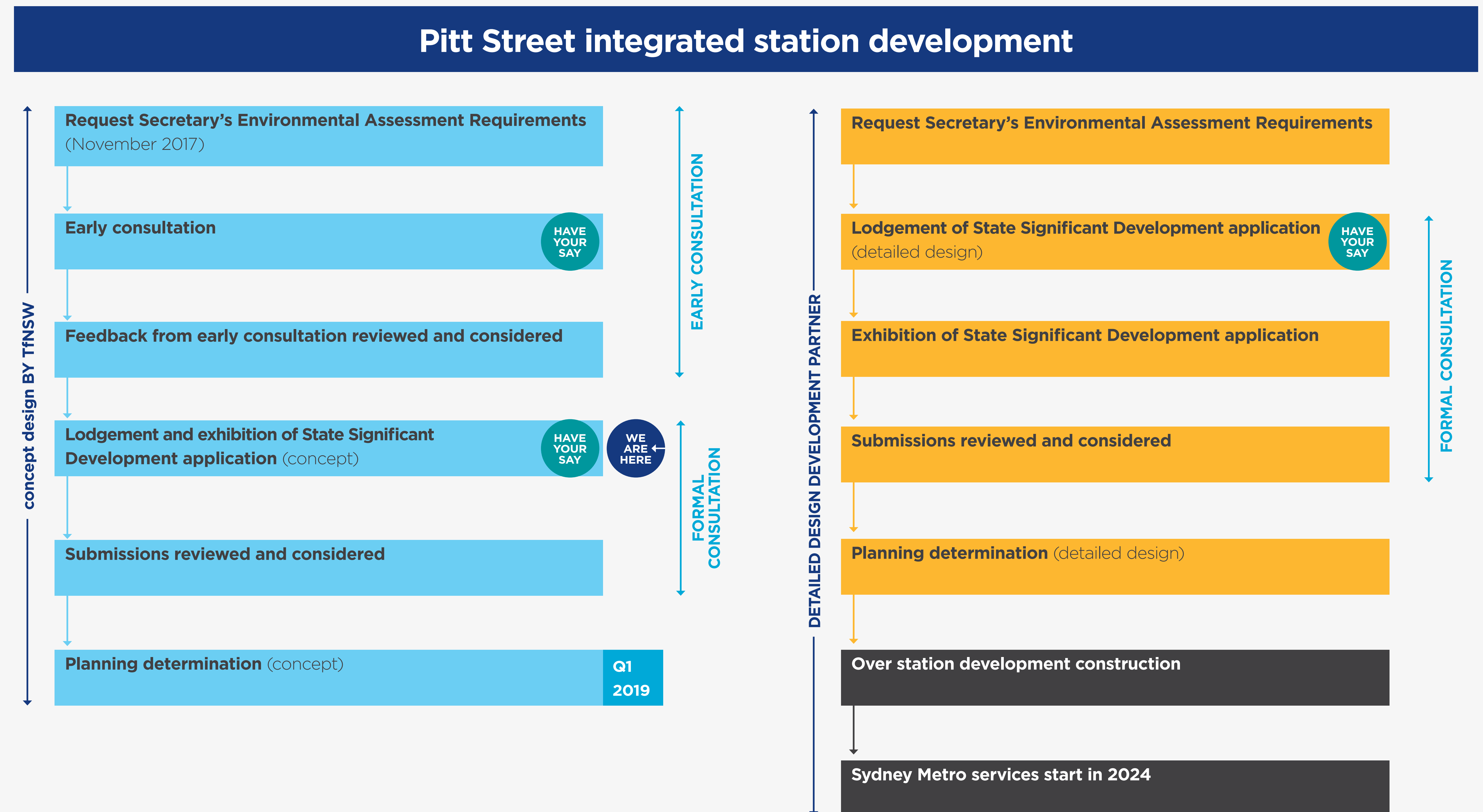
The concept State Significant Development application is the first stage in the planning assessment process. This stage of the approval process includes an assessment of the building envelopes and potential environmental impacts, including height and possible impacts on neighbouring land. The concept application is not sought for any construction or other physical work.

The next stage is to refine and develop the design, including the final architecture for the building. This is called a detailed State Significant Development application.

Following a competitive tender process, Sydney Metro will select a contractor to build the development. This contractor will prepare and submit the detailed State Significant Development applications.

The planning process

Two concept State Significant Development applications, including Environmental Impact Statements, have been submitted to the NSW Department of Planning and Environment, for determination by the NSW Minister for Planning.



Have your say

The community, government agencies and project stakeholders can make a submission on one or both of the concept State Significant Development applications to the NSW Department of Planning and Environment.

At the end of exhibition, the Department will collate submissions and publish them on its website.

If you do not want your name made available to Sydney Metro, or to appear on the Department's website, please clearly state this in your submission(s).

Your submission(s) must reach the NSW Department of Planning and Environment by 12 September 2018 and must include:

1. Your name and address
2. The name of your application
3. The application number(s) that your submission is relevant to:
 - SSD_8876 (Pitt Street South)
 - SSD_8875 (Pitt Street North)**If you wish to comment on both applications you will need to submit two separate submissions.**
4. A brief statement on whether you support or object to the proposal(s)
5. The reasons why you support or object to the proposal(s).

Your submission(s) should be marked **Attention: Director, Key Sites Assessments** and can be sent via:

Website: majorprojects.planning.nsw.gov.au and follow the 'on exhibition' links


Post to: Director, Key Sites Assessments
Department of Planning and Environment
GPO Box 39, SYDNEY, NSW 2001



Contact us

Keeping in touch

For more information visit our website sydneymetro.info or contact us via

 **1800 171 386** 24-hour community information line

 **sydneymetro@transport.nsw.gov.au**

 Sydney Metro, PO Box K659, Haymarket, NSW 1240

Translating and Interpreting Service

If you require the services of an interpreter, please contact the **Translating and Interpreting Service on 131 450** and ask them to call **Sydney Metro on 1800 171 386**. The interpreter will then assist you with translation.

আপনার, একজন দোভাষীর (ইন্টারপ্রেটার) সেবা-সাহায্য আবশ্যিক হলে, অনুগ্রহ করে **১৩১ ৪৫০** নং এ ট্রান্সলেটিং এন্ড ইন্টারপ্রেটিং সার্ভিস এর সাথে যোগাযোগ করুন, এবং **১৮০০ ১৭১ ৩৮৬** নং এ সিডনী মেট্রো কে কল করতে তাদের বলুন। তখন অনুবাদ/ভাষান্তরে, দোভাষী আপনাকে সাহায্য করবে।

如果您需要翻译服务, 请致电131 450 翻译和口译服务, 让他们打 1800 171 386 给悉尼地铁, 翻译员然后将帮助您进行翻译。

Εάν χρειάζεστε τις υπηρεσίες διερμηνέα, παρακαλείσθε να επικοινωνήσετε με την Υπηρεσία Μεταφραστών και Διερμηνέων στο **131 450** και ζητήσετε τους να καλέσουν το **Sydney Metro** στο **1800 171 386**. Ο διερμηνέας θα σας βοηθήσει στη μετάφραση.

통역서비스가 필요하시면, 번역 및 통역 서비스 (Translating and Interpreting Service) 전화 **Translating and Interpreting Service on 131 450** 에 연락하시어 Sydney Metro 전화 **1800 171 386** 에 연결해달라고 요청하십시오. 통역관이 통역을 도와 드릴 것입니다.

إذا كنتم بحاجة إلى خدمات مترجم، يرجى الاتصال بخدمة الترجمة الكتابية والشفهية على الرقم **131 450** واطلبوا منهم الاتصال بمترو سيدني على الرقم **1800 171 386**. وبعد ذلك سيقوم المترجم بمساعدتكم في الترجمة.

Nếu quý vị cần dịch vụ thông dịch viên, xin liên lạc **Dịch vụ Thông Phiên Dịch (Translating and Interpreting)** ở số **131 450** và yêu cầu gọi Sydney Metro ở số **1800 171 386**. Sẽ có thông dịch viên giúp cho quý vị việc thông dịch.

यदि आपको दूमापिए की सेवाओं की जरूरत है, तो कृपया अनुवाद एवं दूमापिया सेवा (**Translating and Interpreting Service**) से **131 450** पर संपर्क करें और उन्हें सिडनी मेट्रो **1800 171 386** पर कॉल करने का निवेदन करें। फिर दूमापिया अनुवाद में आपकी मदद करेगा।

Artist's impression Pitt Street Station

EIS OVERVIEW DOCUMENT

APPENDIX E






Pitt Street Over Station Development

CONCEPT STATE SIGNIFICANT DEVELOPMENT APPLICATION
ENVIRONMENTAL IMPACT STATEMENT OVERVIEW

AUGUST 2018



The Pitt Street integrated station development will better connect our global city, with new places to live, work and play in the heart of Sydney.



Sydney Metro is Australia's biggest public transport project. This new stand-alone railway will deliver 31 metro stations and 66 kilometres of new metro rail, revolutionising the way Australia's biggest city travels.

Joining other great global mass transit development initiatives, the NSW Government has identified stations on the Sydney Metro system which can be better integrated with the areas around them, creating world-class places that will shape our city's future.

Pitt Street Station has been identified as a prime location for an integrated station development.

The Pitt Street integrated station development is made up of Pitt Street Station and two separate building proposals above the new station entries. These buildings will be located on the corner of Pitt and Park streets and on the corner of Pitt and Bathurst streets.

The Pitt Street integrated station development will connect people with employment, transport and some of Sydney's key destinations. It will be a hive of commerce and lifestyle opportunities, with new homes and hotel spaces.

As the new metro station is built underground, the integrated station development can be built above it at the same time. This helps reduce community impacts, and allows for the buildings to be completed close to when Sydney Metro City & Southwest services start in 2024.

Sydney Metro received planning approval for Pitt Street Station in January 2017, and is now seeking two concept State Significant Development approvals for the buildings above the north and the south entrances to the station, these are also known as over station developments.

This overview document highlights key features of Sydney Metro, the Pitt Street integrated station development and concepts for the over station developments above Pitt Street Station. It also summarises important elements of the Environmental Impact Statements for the over station developments. The full concept applications can be downloaded from majorprojects.planning.nsw.gov.au and sydneymetro.info.

Contents

Premier's Message	3
Minister's Message	3
Revolutionising how we travel	5
The biggest urban rail project in Australian history	6
Over station developments	8
Excellence in design	9
Reimagining the heart of the city	10
Pitt Street integrated station development	12
Pitt Street concept proposals	14
Pitt Street South	16
Pitt Street North	18
Environmental Impact Statements	20
Working with the community	24
Planning process	26
Have your say	28
Contact us	31

Premier's Message



Sydney Metro is one of the great global infrastructure projects of our times, not only revolutionising the way we get around our great city but also revitalising its potential for decades to come.

This world-scale investment will be a catalyst for growth well into the 21st century – mirroring the city-shaping projects of the great Dr JJC Bradfield of 100 years ago which have served us so well, like the Sydney Harbour Bridge and the underground city railway line.

Today, as Sydney Metro rapidly takes shape, we look to a strong and vibrant future for Australia's biggest city, underpinned by an unprecedented investment in new infrastructure like the \$20 billion-plus Sydney Metro.

Join us as we build tomorrow's Sydney.

A handwritten signature in black ink, appearing to read 'Gladys Berejiklian'.

Gladys Berejiklian MP

Premier of New South Wales

Minister's Message



Sydney's transformation is well and truly underway.

Australia's biggest public transport project is unlocking the potential of the nation's biggest city right before our eyes – 31 stations, 66 kilometres of new metro rail – on a scale comparable to anything being delivered around the world right now.

Sydney Metro is more than just a new mass transit system for tomorrow's Sydney; it's an investment in city-building and nation-building that will not just make getting around our great city faster and easier, but will shape its future potential for generations to come.

Welcome to tomorrow's Sydney – the journey is only just beginning.

A handwritten signature in black ink, appearing to read 'Andrew Constance'.

Andrew Constance MP

Minister for Transport and Infrastructure



The customer is at the centre of the Sydney Metro rail product.

The success of the Sydney Metro program of works relies not only on the step-change which comes with a new-generation metro rail service, but also on its effective integration into Greater Sydney.

The customer is at the centre of the metro rail product; Sydney Metro is delivering a level of transport service never before seen in Australia, but common in global cities.

Revolutionising how we travel



Sydney Metro will evolve with the city it will serve for generations to come.

Global Sydney's population will pass 6 million by 2036; an extra 1.7 million people will progressively move into Australia's biggest city, which will support an extra 840,000 jobs and 680,000 homes.

Sydney Metro will make it easier and faster to get around, boosting economic productivity by bringing new jobs and new educational opportunities closer to home.

Designed with customers at their centre, stations will be quick and easy to get in and out of; trains will be fast, safe and reliable; and technology will keep customers connected at every step of the journey.

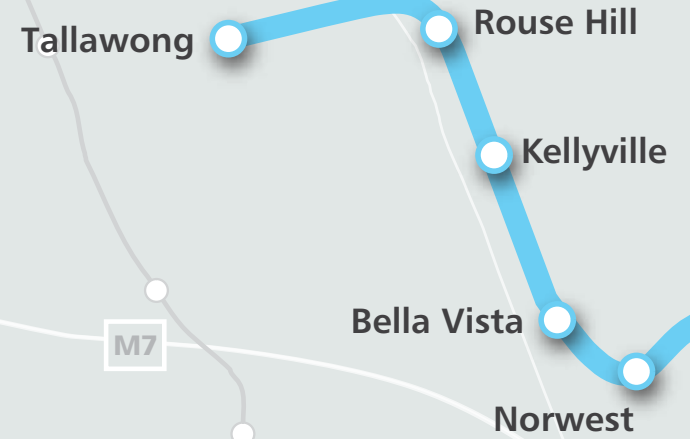
A catalyst for growth, Sydney Metro will integrate with new communities and transform existing suburbs and economic centres. Services will start in 2019. Welcome aboard Australia's biggest public transport project.

Key benefits

Metro delivers improved connectivity across multiple levels:

- moving more people faster than ever before
- fast and easy to change to trains, buses, ferries and light rail
- making it fast and easy to move around a growing city
- making a big city better connected
- aligning and integrating with other city-building transport projects
- opening up options for work, education, health care and much more
- taking the guesswork out of travel – no need to plan, just turn up and go.

The biggest urban rail project in Australian history



Northwest

Sydney Metro Northwest alignment

OPEN
2019
↑

13
M

13 stations

P

4000 commuter car parks

36 kilometres

City & Southwest

Sydney Metro City & Southwest alignment

OPEN
2024
↑

18
M

18 stations

New CBD connections

30 kilometres, including under Sydney Harbour

West

Sydney Metro West study area

SECOND
HALF
2020s
↑

M

Connecting Parramatta and Sydney CBDs

Five key precincts serviced

3.2
MILLION

Western Sydney population, 2036

Sydney Trains suburban network



Over station developments

The Pitt Street integrated station development is made up of Pitt Street Station and two separate building proposals above the new station entries. These buildings are over station developments.

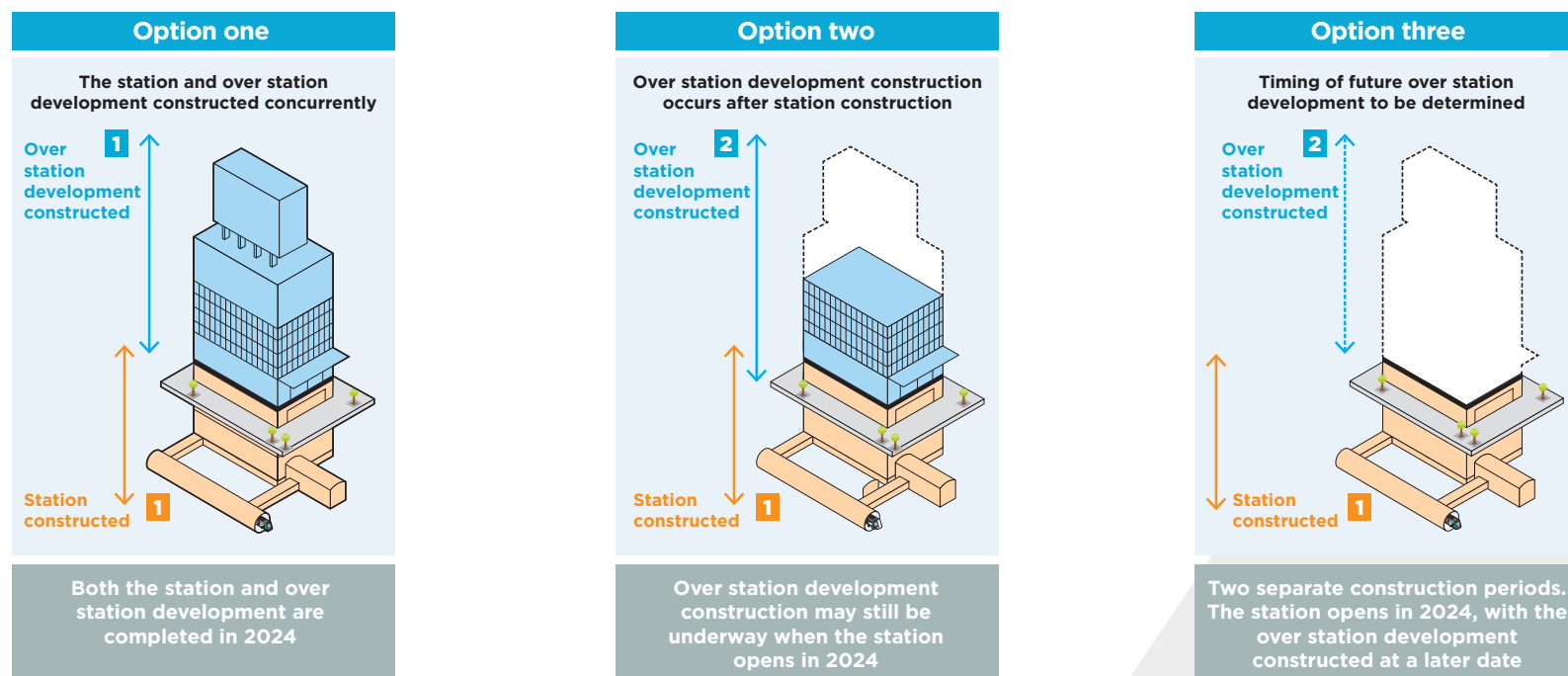
As the new metro stations are built underground, the over station developments can be built above them at the same time. This helps reduce community impacts and the length of time required to deliver the outcomes of integrated station developments. The metro stations have been designed so that work on the over station developments can start while the station construction is underway. This integrated approach means buildings can be completed close to when Sydney Metro City & Southwest services start in 2024.

Staging

Three possible staging options were identified for delivery of the over station development in relation to the metro station below:

- **Option one** – the station and over station development are constructed at the same time by constructing the transfer slab first and then building both the underground station and the building above. Both the station and over station development would be completed in 2024.
- **Option two** – the station is constructed first and ready for operation in 2024. Over station development construction occurs after station construction is completed. This means that over station development construction is likely to still be underway upon opening of the station in 2024.
- **Option three** – the station is constructed first and ready for operation in 2024. The over station development is built at a later stage, with timing yet to be determined. This creates two distinct construction periods for the station and over station development.

Option one is preferred because it delivers the integrated development at the earliest date and minimises construction impacts.



Excellence in design

Design excellence principles will support the placemaking and urban design requirements of integrated station developments.



New city icons

- Development that shapes Sydney's growth and identity
- Contributing to Sydney's reputation for design excellence
- Leaving a lasting legacy



Vibrant public places

- Welcoming and inclusive places for social and cultural interaction
- Opportunities to create places and buildings that celebrate Sydney's culture and values
- Contributing to the vibrant and accessible streets and open spaces around stations



Integrated and inclusive

- Maximising opportunities for land use and transport integration
- Meeting the increasing demand for well-located residential, commercial, retail and community precincts
- Contributing to economic growth through appropriate land use outcomes
- Supporting opportunities for the expansion, improvement and activation of public open spaces



Designed for the future

- Buildings and surrounding spaces will be of the highest quality, exceeding community expectations now and into the future
- New infrastructure and public spaces will be durable, hard wearing and easy to maintain
- Sustainability will be embedded into initiatives



Safety and accessibility

- Day and night-time activation will make precincts vibrant, including safe access
- A sense of security will ensure people can see and be seen

Reimagining the heart of the city

Sydney's new midtown

The Pitt Street integrated station development will provide a community and commercial office hub that is vibrant, attractive and accessible.

It will bring culture, commerce and community to the heart of the city centre and will connect with public spaces designed for socialising, walking or cycling.

Pitt Street integrated station development will be a destination on its own, bringing people together in an easy and connected way, all within minutes of some of Sydney's key destinations.

Attracting visitors, new employment and lifestyle opportunities, it will enhance our city's liveability and boost our local economy.

A new international standard of place

We now have an exciting opportunity to use global best practice and innovative design to create a new sense of place for this prime central business district (CBD) location.

Our specialist team is drawing inspiration from some of the most memorable city shaping global projects, including CIBC Square in Toronto, Hudson Yards in New York and Paddington Station in London.

What is placemaking?

Placemaking is a people-centred approach to planning and designing urban precincts. It strengthens the connection between people and the places they share. Placemaking brings together the physical, cultural and social aspects that define a place.

Pitt Street integrated station development will be a vibrant place that will help to strengthen communities, attract investment and enhance liveability.

It will also be a dynamic new place that will integrate with nearby restaurants, green spaces, plazas, buildings and other public spaces to invite greater community interaction.

A centre for opportunity

The Pitt Street integrated station development will be more than somewhere to catch the train – it will be a thriving and welcoming hub for everyone to enjoy.

Through a range of potential uses – residential, hotel, and office spaces – the buildings above the Pitt Street metro station will attract investment and provide new employment opportunities.

The buildings will provide up to 1800 ongoing jobs, all within easy reach of frequent accessible transport. Residents and visitors will enjoy all of the services and places that the CBD has on offer.

“ The Sydney CBD is set to undergo a major transformation and Sydney Metro is the catalyst for the revitalisation of the heart of the city. A new metro precinct at Pitt Street sets the stage for fresh investment, creating a dynamic new place where people will want to be – unlocking opportunities for future generations. ”

Patricia Forsythe, Executive Director, Sydney Business Chamber



Pitt Street integrated station development



2 Minutes to **CENTRAL**



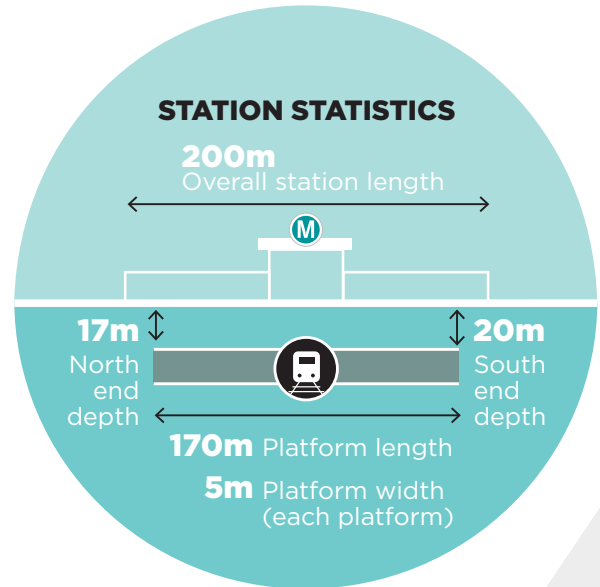
4 Minutes to **BARANGAROO**



7 Minutes to **VICTORIA CROSS**

00:04

A train every four minutes in the peak - with ultimate capacity for a train every two minutes in each direction



9400 PEOPLE
will move through Pitt Street Station in the morning peak

UP TO 1800 PERMANENT JOBS

UP TO 600 NEW HOMES

200

UP TO 200 HOTEL ROOMS

Nearby transport interchanges



Pitt Street North and Pitt Street South

There are two concept proposals for above Pitt Street Station - one on the corner of Park and Pitt streets at the northern entry to Pitt Street Station and one on the corner of Bathurst and Pitt streets at the southern entry to Pitt Street Station.

These sites are referred to as Pitt Street North and Pitt Street South. Each concept proposal is being assessed separately and has its own State Significant Development application and Environmental Impact Statement.

The buildings have been designed to be in keeping with the surrounding city scape and current development proposals for the city. The buildings will blend with the city skyline now and into the future.

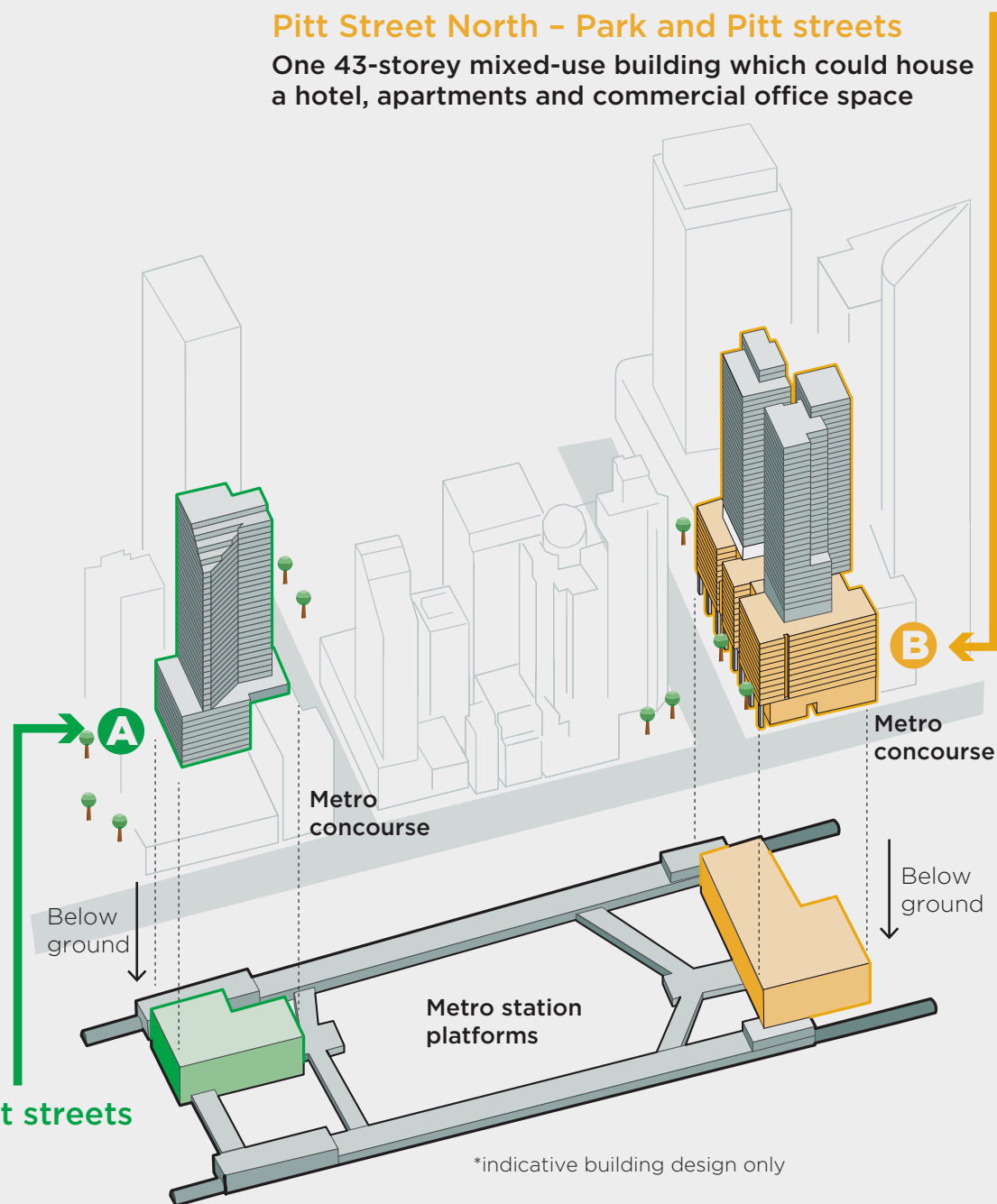
Proposal A is an indicative design for a building to fit within the proposed building envelope, which is the subject of the Pitt Street South concept State Significant Development application.

Proposal B is an indicative design for a building to fit within the proposed building envelope, which is the subject of the Pitt Street North concept State Significant Development application.

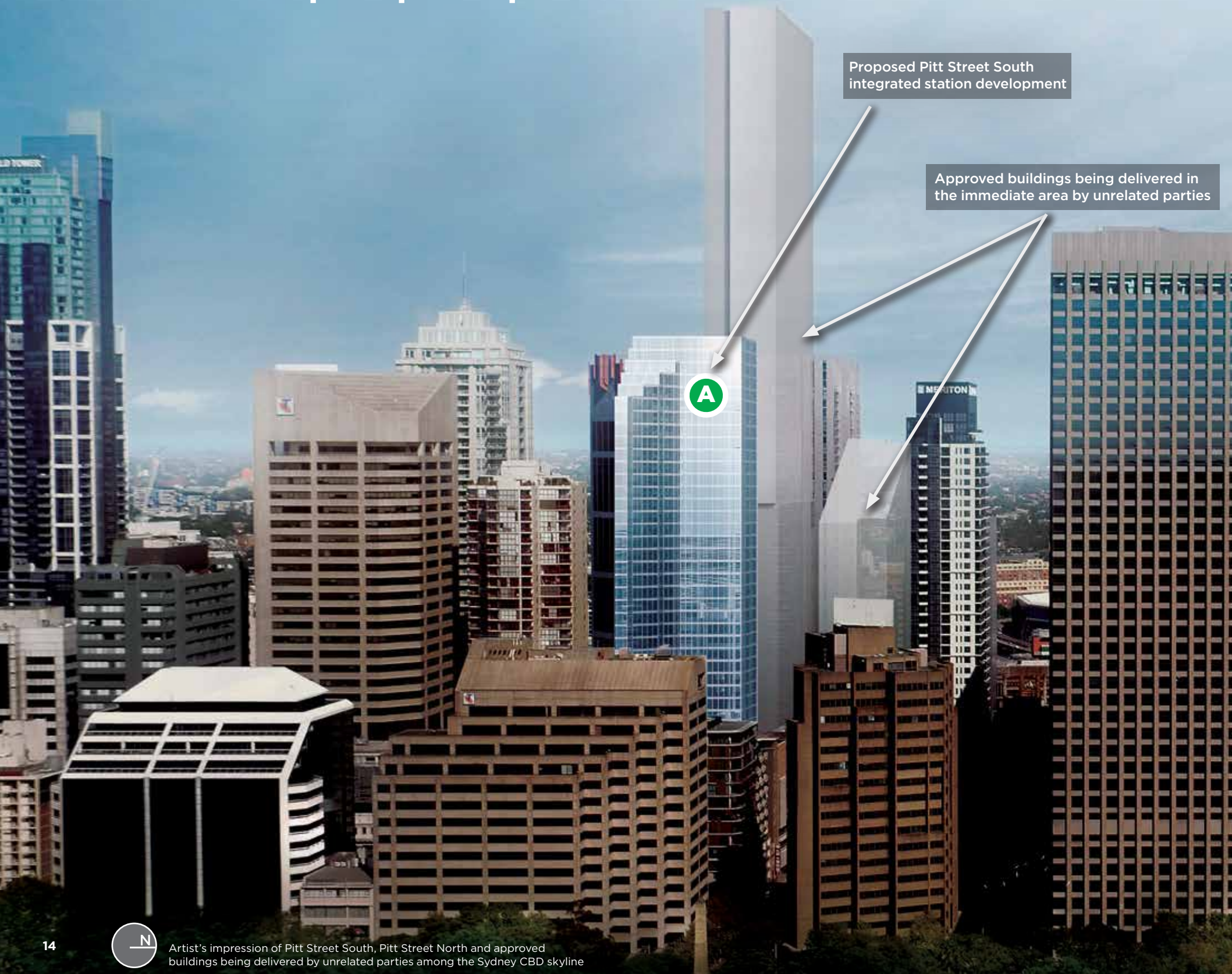
Pitt Street South – Bathurst and Pitt streets One 35-storey building which could house apartments or commercial office space

Pitt Street North – Park and Pitt streets

One 43-storey mixed-use building which could house a hotel, apartments and commercial office space



Pitt Street concept proposals



Proposed Pitt Street South
integrated station development

Approved buildings being delivered in
the immediate area by unrelated parties





Proposed Pitt Street North integrated station development



A

Pitt Street South

One 35-storey building which could house apartments or commercial office space



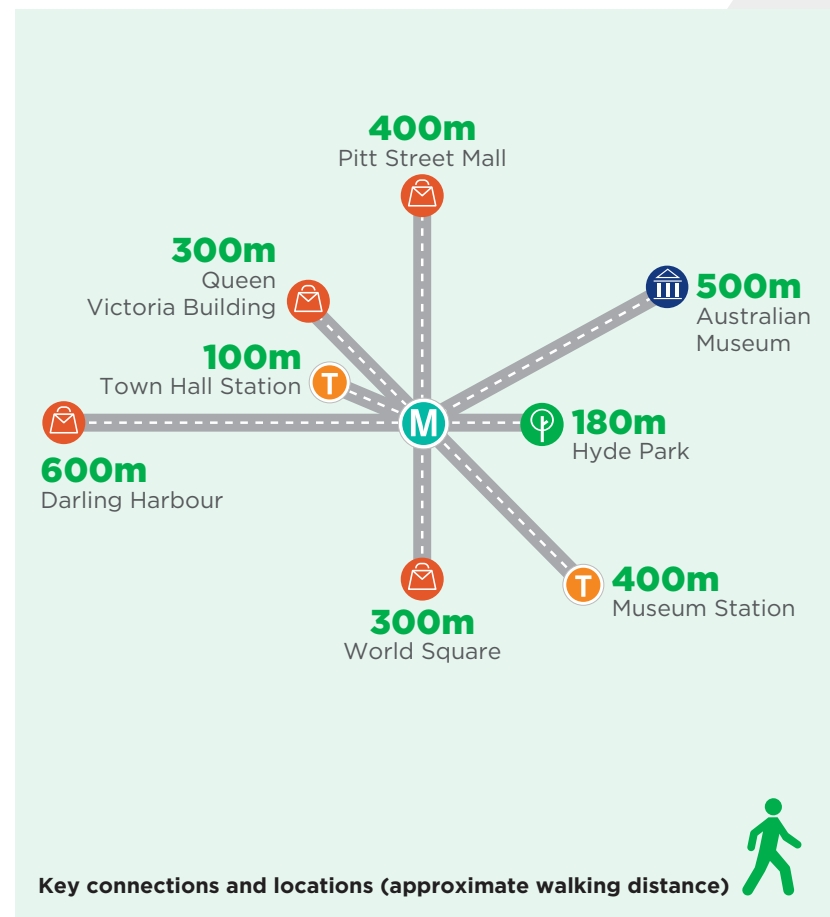
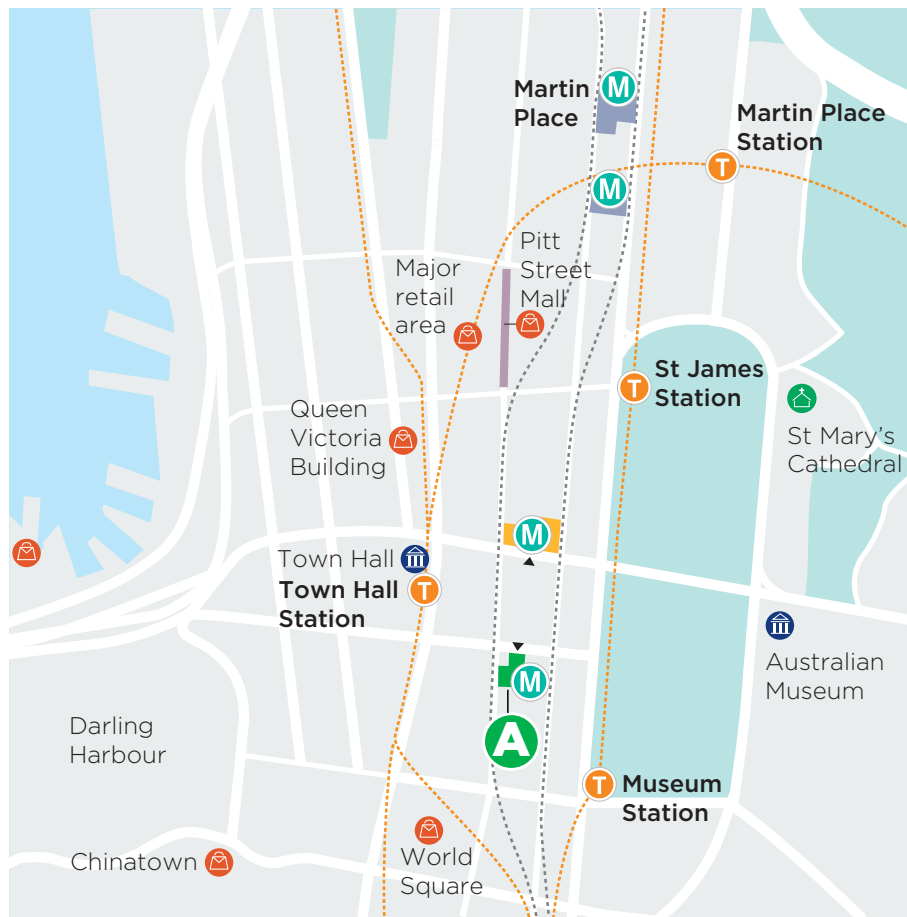
B

Pitt Street North

One 43-storey mixed-use building which could house a hotel, apartments and commercial office space

Pitt Street South

Bathurst and Pitt streets





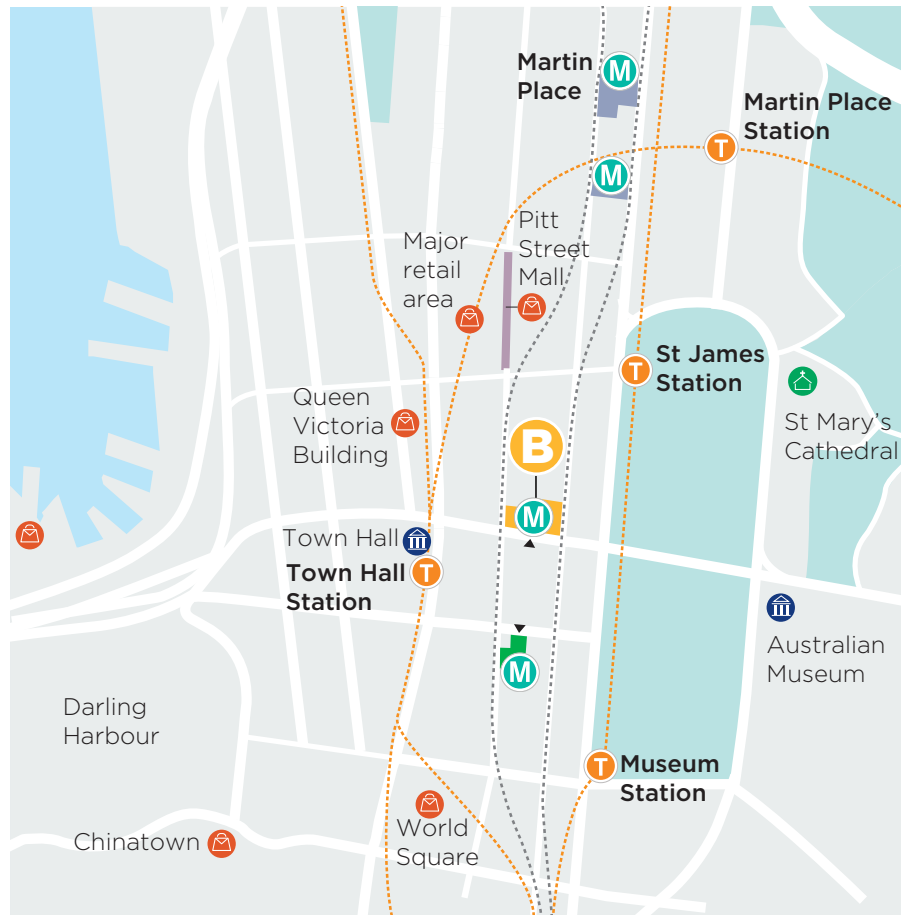
Proposed height and uses - Pitt Street South

One 35-storey building which could house apartments or commercial office space



Pitt Street North

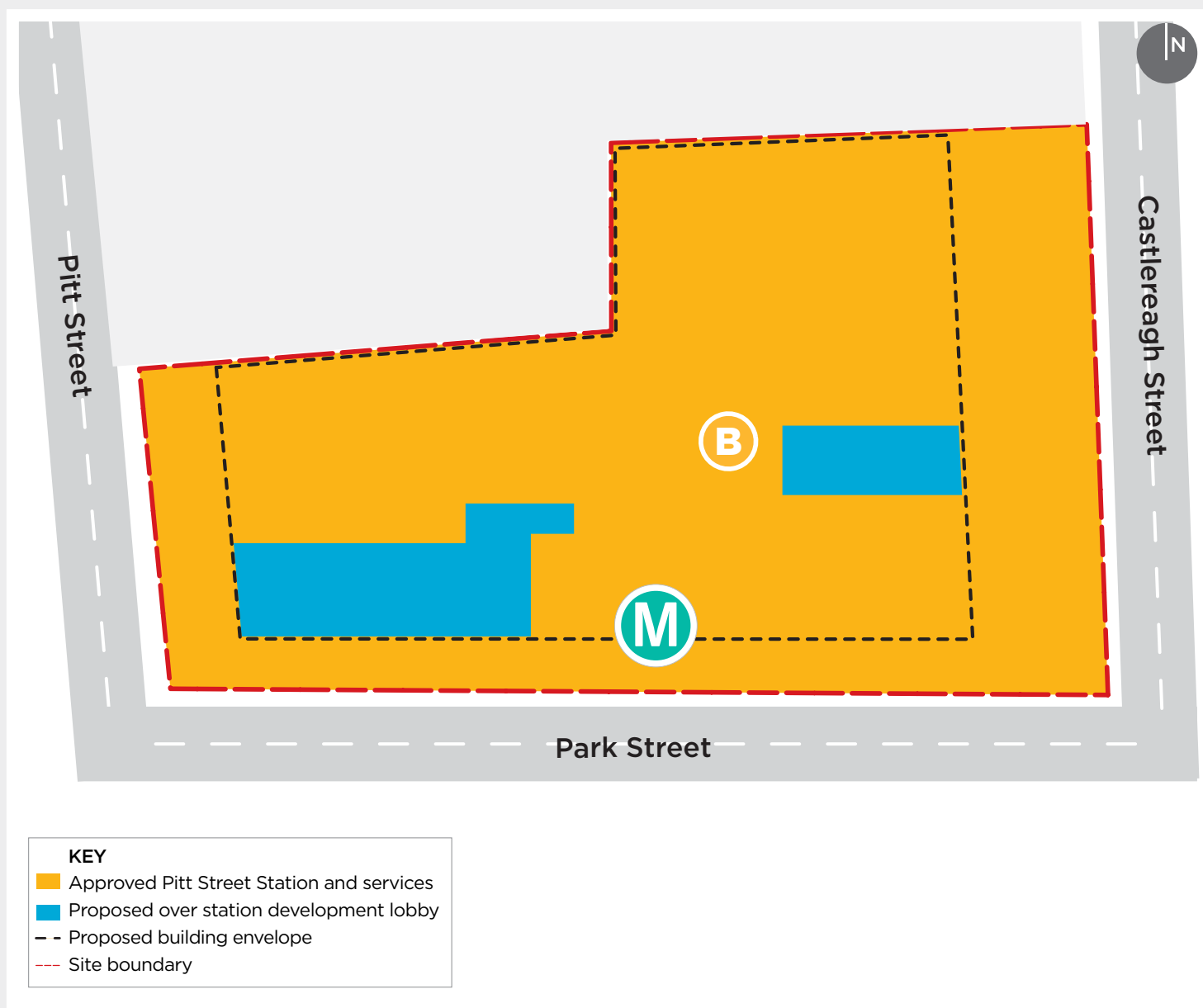
Park and Pitt streets





Proposed height and uses - Pitt Street North

One 43-storey mixed-use building which could house a hotel, apartments and commercial office space



What are the Environmental Impact Statements about?

The Environmental Impact Statements for Pitt Street South and Pitt Street North contain information about the proposed buildings above the new metro station at Pitt Street, as well as an assessment of possible environmental impacts. This document outlines the key aspects of the Environmental Impact Statements.

The buildings and location

The building envelopes have been designed to minimise overcrowding of the city skyline and contribute to the creation of a quality precinct in the Sydney CBD.

Building materials and architectural elements for both locations will be considered and developed as part of the detailed design.

Pitt Street South

The concept includes a 35-storey building that will integrate with the metro station below. The building will include up to 16,250 square metres of office space and 11 car spaces, or up to 159 residential apartments and 34 car spaces.

The building will be located on the southern portion of the block at the intersection of Pitt and Bathurst streets in an L-shape, to exclude the area above the heritage-listed Edinburgh Hotel. Frontages will be on Pitt Street and Bathurst Street. Refer to page 17 to see the layout.

Pitt Street North

The concept includes a 43-storey mixed use building that will integrate with the metro station below. Building uses will include up to 1,500 square metres of office space, up to 300 apartments and up to 200 hotel rooms.

The building will be located at the southern portion of the block between Pitt, Park and Castlereagh streets, occupying the full block width at the south. Refer to page 19 to see the layout.

Views

View impacts comply with the applicable planning controls including the Sydney Local Environmental Plan. View sharing principles have been considered when designing the buildings, and both buildings will be appropriately set back from adjoining properties. It should be noted that there are no provisions for new buildings to fully maintain private residential views and view changes outlined below are in keeping with the context of the Sydney CBD.

Pitt Street South

The most prominent long-distance views of the proposed envelope will be from the east, where the building will not be obscured by other buildings. The building is well suited to the surrounding context and will not interrupt any key public view corridors across the Sydney CBD.

Some of the apartments in the Princeton Apartments, Century Towers and the future Greenland Centre residential apartment buildings will have changed views, mostly at the mid-rise levels of the buildings.

Pitt Street North

The most prominent view of the Pitt Street North building will be from the east, where it will not be obscured by other buildings. This includes views of the building from Hyde Park. From most vantage points, the building is consistent with the city skyline and the high rise character of the Sydney CBD.

Two residential buildings to the south, 27 Park Street and 197 Castlereagh Street, will have changed views to the north and north-west as a result of the Pitt Street North building.

Some apartments in 27 Park Street will see the top portion of the Pitt Street North building where they now see sky.

Views towards Hyde Park, Sydney Harbour or North Head from 27 Park Street will not be impacted. At 197 Castlereagh Street there will be reduced views of Piccadilly Tower, Sydney Tower and the MLC Centre.

Overshadowing of public open spaces

A key focus in the concept design has been to minimise overshadowing on public spaces, including Hyde Park.

Overshadowing from both buildings will comply with the sun access plane provisions under the Sydney Local Environmental Plan. This means overshadowing impacts on Hyde Park will be minimised and sun in the park will be maintained during mid-winter and other times of the year.

Overshadowing of nearby buildings

The buildings comply with conditions under the Sydney Local Environmental Plan, and both buildings will be appropriately set back from adjoining properties. New buildings are not required to fully maintain sun to adjoining properties, and changes outlined below are in keeping with the context of the Sydney CBD.

Pitt Street South

The building will not have any significant overshadowing impacts on the majority of surrounding residential apartments; however it will cause increased overshadowing of some units on the northern side of the Princeton Apartments.

These impacts were addressed at the time of the Princeton Apartments development. The original building approval for the Princeton Apartments required that potential occupants with north facing windows be notified that views and sun access from those windows will be affected by any development on the proposed site.

Pitt Street North

The building will not have any significant overshadowing impacts on the majority of surrounding residential apartments; however it will cause increased overshadowing to lower level residential apartments on the western side of the future building at 201 Elizabeth Street.

The building will also cause increased overshadowing to the northern side of the residential building at 329 Pitt Street and the north and western facing facades of 27 Park Street.



Heritage

The buildings are located in close proximity to a number of heritage items. Significant views to, from or between heritage items will not be reduced or obstructed when compared to the buildings previously on the sites.

Pitt Street South

The Pitt Street South building will have heritage buildings nearby including the Edinburgh Castle Hotel, the Metropolitan Fire Brigade building, the former Speedwell House, the former YMCA building and the former Sydney Water building, now the Primus Hotel.

Pitt Street North

The Pitt Street North building will have heritage buildings nearby including the Criterion Hotel, the National Building and the Masonic Club.

Traffic

There are not expected to be any adverse impacts on the performance of the surrounding road network from traffic generation at either site.

Pitt Street South

Traffic impacts for the Pitt Street South building will be relatively minor, with provision for about 34 car spaces and loading and servicing facilities.

Pitt Street North

Traffic impacts for the Pitt Street North building will also be relatively minor, with provision for about 50 car spaces and loading and servicing facilities.

Environmental sustainability

Both concept proposals include a sustainability strategy outlining energy efficient lighting and devices, energy and water monitoring systems, the use of recycled building materials, and the use of responsible construction practices.

Wind

There are no expected impacts on existing wind speeds and conditions at ground level as a result of the new buildings. The buildings are therefore considered appropriate for pedestrian comfort and safety from wind.

Strategies for wind mitigation will form part of the detailed designs, including a process for wind tunnel testing.

Noise and vibration

An acoustic assessment has been undertaken for both buildings and this demonstrates that impacts both to and from the buildings will be able to be sufficiently mitigated.

The acoustic performance of the buildings will be considered further as part of the detailed design phases.

Public art

A public art strategy will be developed during detailed design to improve the travel experience for customers. Public art will be commissioned based on standards of excellence and innovation, integrity of work, relevance to the site contexts and consistency with planning policies and Sydney Metro and City of Sydney strategies.

The Environmental Impact Statements for each proposal also consider a number of other environmental aspects including:

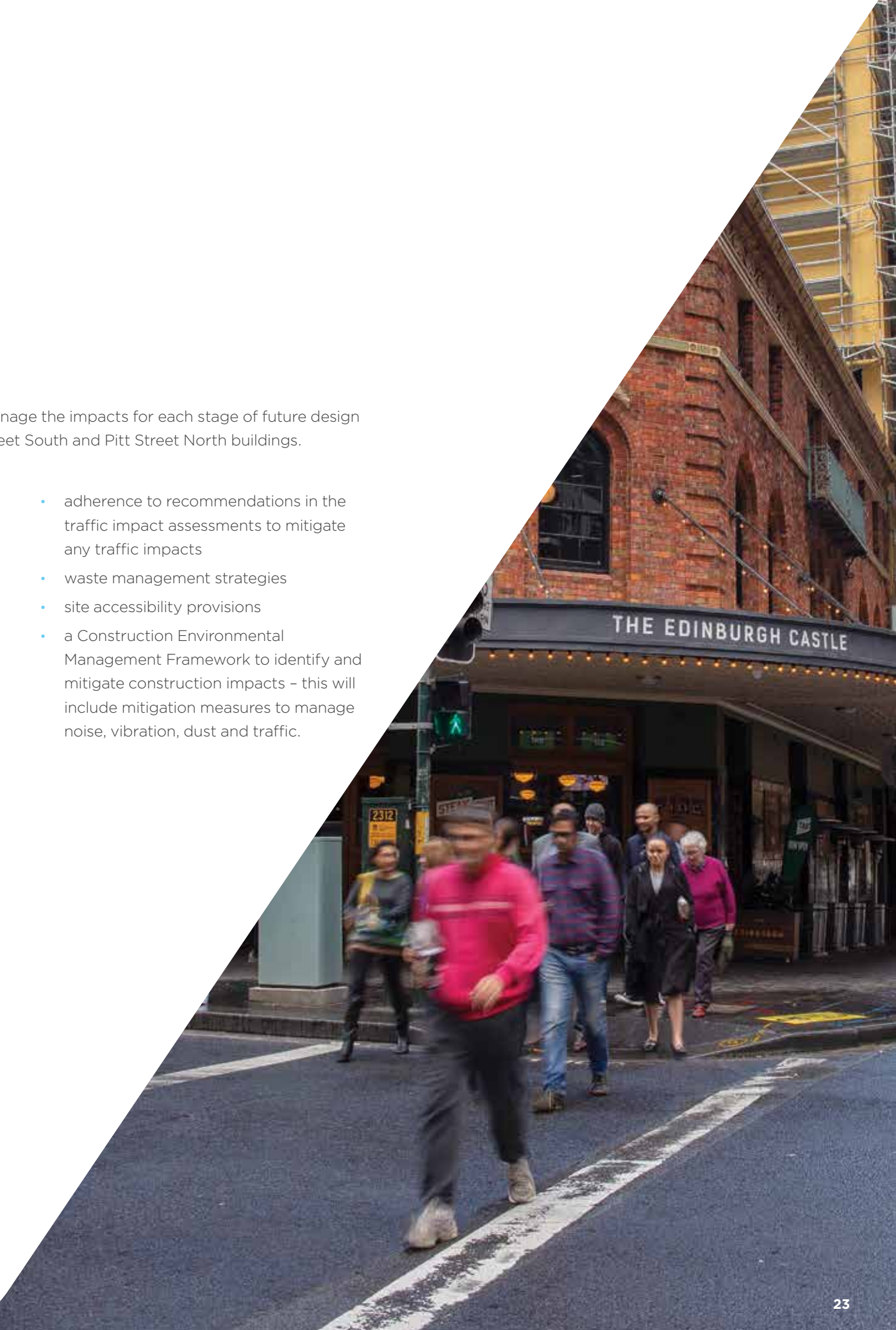
- building form and setbacks
- streetscape and public domain
- integration with Sydney Metro infrastructure
- impact on rail infrastructure
- transport and accessibility
- parking and pedestrian impacts
- design excellence
- airspace for Sydney Airport
- utilities, infrastructure and services
- stormwater and flooding
- construction impacts
- crime prevention through environmental design.

Mitigation measures

Mitigation measures have been outlined to manage the impacts for each stage of future design development and construction for the Pitt Street South and Pitt Street North buildings.

These mitigation measures include:

- adhering to the principles outlined in the design excellence strategy, including a design that demonstrates quality and best practice against international standards
- strategies to protect and enhance local heritage and ensure the development achieves a positive heritage outcome for the buildings
- implementing best-practice sustainability initiatives during design and construction, which could include installation of acoustic treatments to internal and external elements of the buildings
- measures to minimise noise and vibration once the buildings are in operation, including reducing noise at the loading docks and any acoustic treatment required for plant and equipment
- wind-tunnel testing to model wind speeds and inform mitigation strategies
- an assessment of building façade treatments and their sun reflection potential on motorists and pedestrians
- development of strategies to mitigate potential privacy impacts on nearby residential apartments
- adherence to recommendations in the traffic impact assessments to mitigate any traffic impacts
- waste management strategies
- site accessibility provisions
- a Construction Environmental Management Framework to identify and mitigate construction impacts – this will include mitigation measures to manage noise, vibration, dust and traffic.



Working with the community

Community consultation

In 2017, the community was invited to participate in early engagement for the Pitt Street integrated station development via the following communication methods:

- 10,500 invitations were sent to the local community, inviting people to a community information session in November 2017
- five advertisements were placed in newspapers - *The Australian Chinese Daily*, *The Sydney Morning Herald*, *The Mosman Daily*, *The North Shore Times* and *The Central Courier*
- a media release, website forums and Facebook were also used to communicate the concept proposal and invite the community to give feedback.

A total of 36 people attended the information session.

Sydney Metro will continue to work with stakeholders and the community to ensure they are informed about the project and have opportunities to provide feedback to the project team.

Information on how to provide feedback on each of the concept State Significant Development applications is provided on pages 28-29 of this booklet.

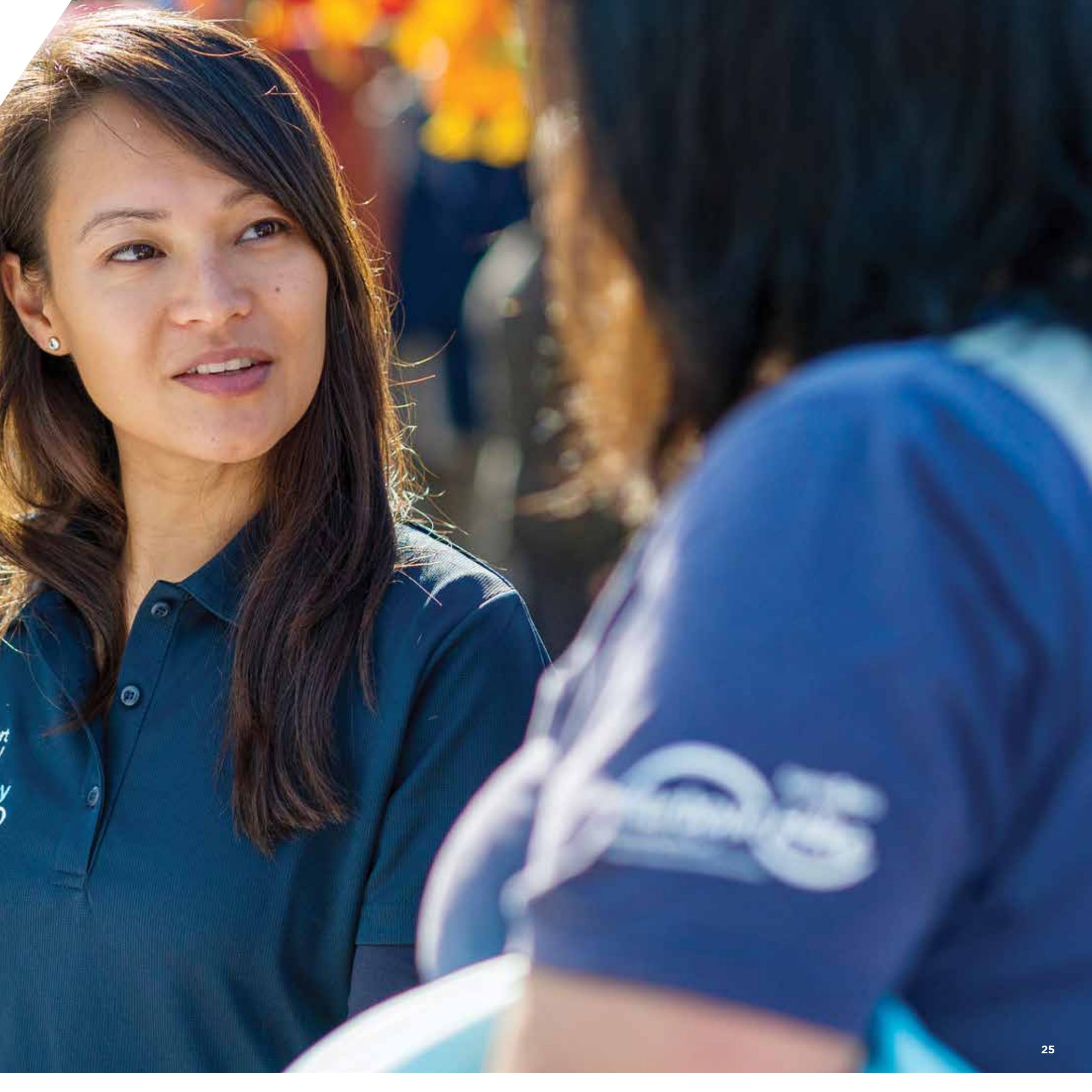
Place managers working with the community

Sydney Metro has dedicated community relations specialists called place managers. Their role is to act as the single contact person between directly affected members of the community and the project team.

Our place managers will continue to play a vital role in maintaining close and ongoing contact with local communities and stakeholders during the design and delivery of Sydney Metro.

They can be contacted on the community information line **1800 171 386** or via the project email **sydneymetro@transport.nsw.gov.au**.





Planning process

What is a concept approval?

The concept State Significant Development application is the first stage in the planning assessment process. This stage of the approval process includes an assessment of the building envelopes and potential environmental impacts, including height and possible impacts on neighbouring land. The concept application is not sought for any construction or other physical work.

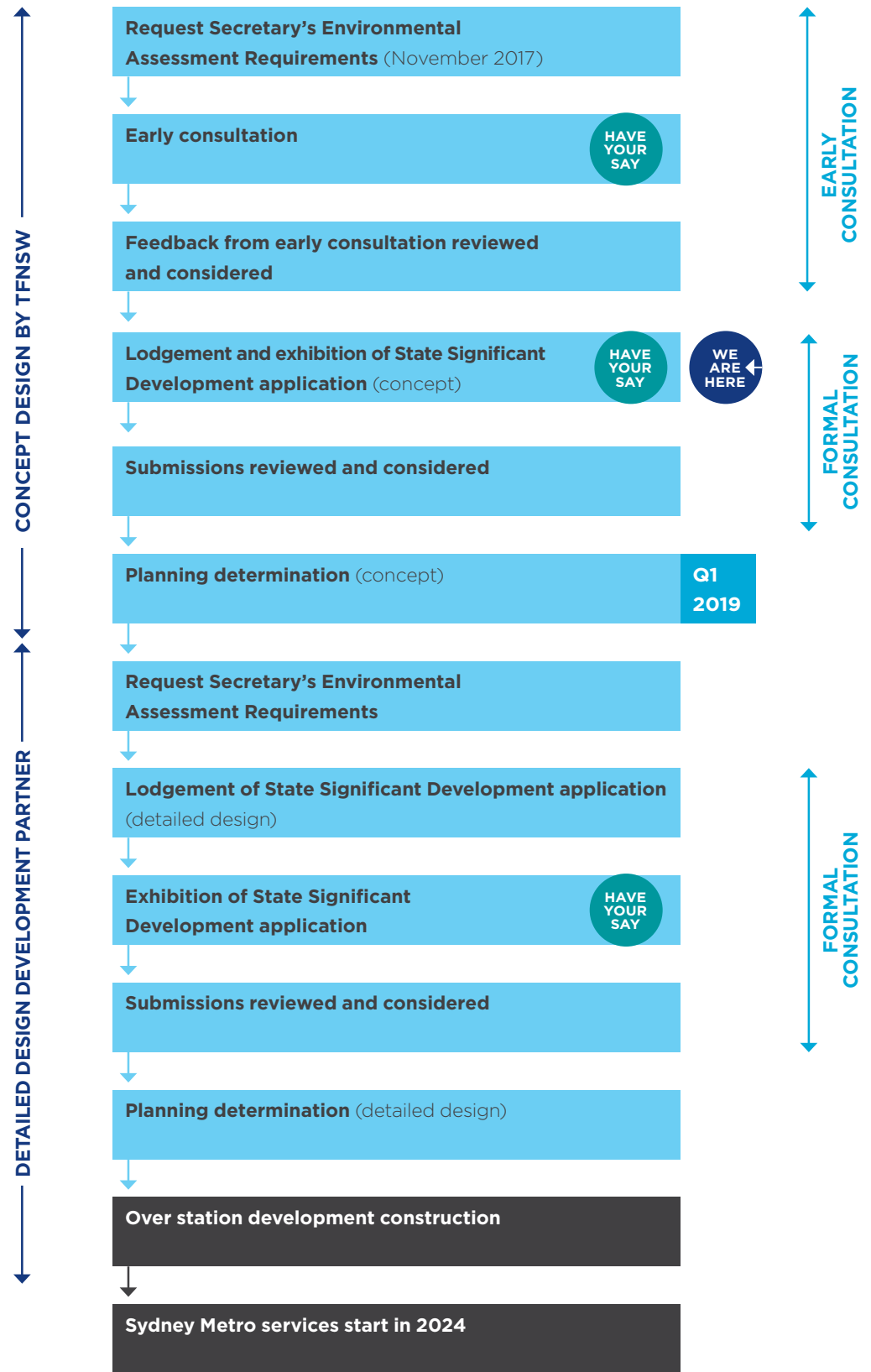
The next stage is to refine and develop the design, including the final architecture for the building. This is called a detailed State Significant Development application.

Following a competitive tender process, Sydney Metro will select a contractor to build the development. This contractor will prepare and submit the detailed State Significant Development applications.

The planning process

Two concept State Significant Development applications, including Environmental Impact Statements, have been submitted to the NSW Department of Planning and Environment, for determination by the NSW Minister for Planning. The planning process is outlined on page 27.

Pitt Street integrated station development



Have your say

The concept State Significant Development applications for both Pitt Street South and Pitt Street North are on public exhibition until 12 September 2018.

The community, government agencies and project stakeholders can make a submission on one or both of the concept State Significant Development applications to the NSW Department of Planning and Environment. At the end of exhibition, the Department will collate submissions and publish them on its website.

It is the NSW Department of Planning and Environment's policy to also place a copy of your submission(s) on their website. If you do not want your name made available to Sydney Metro, or to appear on the Department's website, please clearly state this in your submission(s).

Your submission(s) must reach the NSW Department of Planning and Environment by 12 September 2018 and must include:

1. Your name and address
2. The name of your application
3. **The application number(s) that your submission is relevant to:**
 - **SSD_8876 (Pitt Street South)**
 - **SSD_8875 (Pitt Street North)**

If you wish to comment on both applications you will need to submit two separate submissions.

4. A brief statement on whether you support or object to the proposal(s)
5. The reasons why you support or object to the proposal(s).

Your submission(s) should be marked Attention: Director, Key Sites Assessments and can be sent via:

Website:
majorprojects.planning.nsw.gov.au
and follow the 'on exhibition' links

Post to:
Key Sites Assessments
Department of Planning and Environment
GPO Box 39, SYDNEY, NSW 2001

Anyone lodging submissions must declare reportable political donations (including donations of \$1000 or more) made in the previous two years.

For more details, and a disclosure form, go to **planning.nsw.gov.au/donations**.

All submissions and information obtained during the public exhibition period will be used in accordance with the *Privacy Act 1988*. All submissions received are regarded as public documents and any information contained in them can be published in subsequent assessment documents.

Copies of the submissions received may be issued to interested parties. If the author of a submission does not wish for the information to be distributed, this needs to be clearly stated in the submission.

For enquiries, please contact the NSW Department of Planning and Environment:

Phone: **1300 305 695**
Email: **information@planning.nsw.gov.au**

Following exhibition, issues raised in these submissions will be summarised in submissions reports. Sydney Metro will consider the issues raised, and may make changes to one or both concepts as a result of submissions. The Minister for Planning will then make a decision about whether to approve each concept State Significant Development application.

If the over station developments proceed, consultation with key stakeholders and the community will continue during the preparation and assessment of the detailed State Significant Development applications, and the construction and operation phases. This ongoing engagement process will play an important role in reducing the potential impacts and enhancing the benefits for all stakeholders.

Where to view the concept State Significant Development applications

Each Environmental Impact Statement and its accompanying documents may be viewed on the NSW Department of Planning and Environment website:

majorprojects.planning.nsw.gov.au and **sydneymetro.info**

You can also view the documents at:

City of Sydney Council

Town Hall House
Level 2, 456 Kent Street
Sydney NSW

Customs House

31 Alfred Street
Sydney NSW

Community information sessions

The project team has organised community drop-in sessions where displays and information about each Environmental Impact Statement will be available.

You are invited to attend these sessions and meet expert members of the project team who will be there to answer any questions you may have.

There is no need to make a booking – and you can call in at any time.

Date and time	Location
Tuesday, 28 August 2018 11am–2pm	Wesley Conference Centre 220 Pitt Street, Sydney
Thursday, 30 August 2018 4–7pm	Primus Hotel 339 Pitt Street, Sydney
Tuesday, 4 September 2018 11am–2pm	Primus Hotel 339 Pitt Street, Sydney
Thursday, 6 September 2018 4–7pm	Wesley Conference Centre 220 Pitt Street, Sydney





Contact us

Keeping in touch

For more information visit our website sydneymetro.info or contact us via:

Sydney Metro



1800 171 386 24-hour community information line



sydneymetro@transport.nsw.gov.au



Sydney Metro, PO Box K659,
Haymarket, NSW 1240



If you need an interpreter, call
Translating and Interpreting Services
National on **131 450** and ask them
to call **1800 171 386**



Translating and Interpreting Service

If you require the services of an interpreter, please contact the **Translating and Interpreting Service on 131 450** and ask them to call **Sydney Metro on 1800 171 386**. The interpreter will then assist you with translation.

আপনার, একজন দোভাষার (হস্তারপ্রচার) সেবা-সাহায্য আবশ্যিক হলে, অনুগ্রহ করে **১৩১ ৪৫০** নং এ ট্রান্সলেটিং এন্ড ইন্টারপ্রেটিং সার্ভিস এর সাথে যোগাযোগ করুন, এবং **১৮০০ ১৭১ ৩৮৬** নং এ সিডনী মেট্রো কে কল করতে তাদের বলুন। তখন অনুবাদ/ ভাষান্তরে, দোভাষী আপনাকে সাহায্য করবে।

如果您需要翻译服务, 请致电131 450 翻译和口译服务, 让他们打 1800 171 386 给悉尼地铁, 翻译员然后将帮助您进行翻译。

Εάν χρειάζεστε τις υπηρεσίες διερμηνείας, παρακαλείστε να επικοινωνήσετε με την **Υπηρεσία Μεταφραστών και Διερμηνέων** στο **131 450** και ζητήστε τους να καλέσουν το **Sydney Metro** στο **1800 171 386**. Ο διερμηνέας θα σας βοηθήσει στη μετάφραση.

통역서비스가 필요하시면, 번역 및 통역 서비스 (Translating and Interpreting Service) 전화 **Translating and Interpreting Service on 131 450** 에 연락하시어 **Sydney Metro 전화 1800 171 386** 에 연결해달라고 요청하십시오. 통역관이 통역을 도와 드릴 것입니다.

إذا كنتم بحاجة إلى خدمات مترجم، يرجى الاتصال بخدمة الترجمة الكتابية والشفهية على الرقم **131 450** واطلبوا منهم الاتصال بمركز سيدني على الرقم **1800 171 386**. وبعد ذلك سيقوم المترجم بمساعدتكم في الترجمة.

Nếu quý vị cần dịch vụ thông dịch viên, xin liên lạc **Dịch vụ Thông Phiên Dịch** (Translating and Interpreting) ở số **131 450** và yêu cầu gọi Sydney Metro ở số **1800 171 386**. Sẽ có thông dịch viên giúp cho quý vị việc thông dịch.

यदि आपको दुभाषिए की सेवाओं की ज़रूरत है, तो कृपया अनुवाद एवं दुभाषिया सेवा (**Translating and Interpreting Service**) से **131 450** पर संपर्क करें और उन्हें सिडनी मेट्रो **1800 171 386** पर को फोन करने का निवेदन करें। फिर दुभाषिया अनुवाद में आपकी मदद करेगा।

Pitt Street



Pitt Street Station entrance





Printed on environmentally responsible paper made Carbon Neutral. The greenhouse gas emission of the manufacturing process has been independently certified by the FSC® and is manufactured from Recycled paper in a process Chlorine free (PCF) environment under the ISO 14001 environmental management system.

Information in this document has been prepared in good faith and is correct at the time of printing. August 2018.

© Sydney Metro 2018. 18124-CSW 08.18

sydneymetro.info
facebook.com/sydneymetro



**SUMMARY OF ISSUES
RAISED AT COMMUNITY
INFORMATION
SESSIONS**

APPENDIX F



Appendix F – summary of issues raised at Community Information Sessions

During the community information sessions held by Sydney Metro (i.e. during the exhibition of the EIS), the following key issues were raised by the attendees:

- General project information
- Connections existing stations & Metro station
- OSD & design
- Station link
- Sydney Metro West

The sentiments of the attendees in relation to these key issues have been captured in the table below.

General project information	<ul style="list-style-type: none"> • Alignment: It was suggested that the alignment would be better placed in areas without functioning train to divert commuters away from the CBD which is already congested • Completion dates: Questions were asked about the completion date for the overall metro and integrated station development.
Connections existing stations & Metro station	<ul style="list-style-type: none"> • Pedestrian connections: Concerns were raised about the need to upgrade street level pedestrian connections to accommodate more people. • Underground pedestrian concourse: General question where raised about underground connections to Town Hall and exiting nearby stations, and between Pitt Street North and South Metro stations • Station entry at Pitt St South: Questions were asked seeking to clarify the location of the entry to Pitt St south • Public open space: Feedback was given about the need for public places to sit near the station and around the precinct
OSD & design	<ul style="list-style-type: none"> • Built form: Comments were made about the need for interesting and inspiring buildings in the CBD • Scale: Question were asked about the overall size and scale of the developments in comparison with other buildings in the area • Timing/concept of OSD: Questions were asked about the timing of the OSD relative to the rest of the station • Land uses: Questions were asked about proposed land use and if these had been confirmed • Ground floor retail: Suggestions were made to incorporate ground floor retail like in Tokyo to activate street frontages • Princeton Apartments: Concerns were raised about

	<p>the potential impacts of the proposal on the amenity of residents in Princeton Apartments, particularly apartments at the lower levels with north facing windows.</p> <ul style="list-style-type: none"> • Value capture: Questions were asked about building development funding, and if the purpose of the over station development was to fund the metro • Connectivity: Questions were asked if there was direct access to platforms from buildings above the station • Operational impacts: Questions were asked about operational impacts such as waste removal, building servicing. • Construction Questions were asked about operational impacts such as waste removal, building servicing. • Heights: Questions were asked how the heights of the building were determined, and if sun access plane was considered
Station link	<ul style="list-style-type: none"> • ECRL closure: Questions were asked about the length of time of the closure, what alternative mode of transportation were provided, the frequency / timetable, and the route of the buses.
Sydney Metro West	<ul style="list-style-type: none"> • Link with Metro West: Questions were asked about the link between Metro West and Metro C&SW at its location. • Station locations: General questions were asked about the locations of Metro West stations, and when they will be finalised.

**ISSUES CATEGORIES
AND WHERE TO
FIND RESPONSES TO
ISSUES RAISED IN
SUBMISSIONS**

APPENDIX G



Where issues addressed – submissions in objection

Issue code	Issue	Where addressed in report
Overdevelopment of the site		
OD1	Floorplate/footprint/bulk and scale	Section 6.4.4.2
OD2	Height of proposal	Section 6.4.4.2
OD3	Treating the site as undeveloped sets a dangerous precedent that all air space is 'undeveloped' and therefore up for grabs	Section 6.4.12.2
OD4	The new development should be in keeping with the scale, size, bulk and height of the existing buildings	Section 6.4.4.1
OD5	Airspace should afford an opportunity to motivate the design of individual tower buildings	Section 6.4.4.1
OD6	Ambiguous to grant approval for a building that has the option of being commercial or residential	Section 6.4.12.2
OD7	Cumulative impact of higher buildings on disaster management, infrastructure, wind tunnels, solar access, skyline views and resident health	Section 6.4.4.1
OD8	Restrictive covenant imposed on owners and occupants of 125-129 and 123-135 Bathurst Street, and 296-302 Pitt Street	Section 6.4.1.2
Overshadowing and loss of solar access		
OS1	Loss of amenity to the Princeton apartments in mid-winter	Section 6.4.1.1
OS2	Loss of solar access to Princeton apartments inconsistent with ADG requirements	Section 6.4.1.1
OS3	Title protecting solar access in the applicant's title in 2012	Section 6.4.1.2
OS4	Failure to undertake solar analysis for Greenland Centre	Section 6.4.1.3
OS5	Impact of reduced solar access on potential to install solar panels	Section 6.4.1.4
OS6	The applicant has not dealt with clause 4.2.3 and clause 4.2.3.1 of the City of Sydney DCP 2012 in relation to solar access enhanced and solar access loss legislation	Section 6.4.2.2
OS7	Sydney Metro has not considered the impact of light scatter	Section 6.4.1.1
Loss of privacy to adjacent properties		
P1	Requirement for owners of Princeton apartments to install screens to maintain privacy	Section 6.4.2.1
P2	EIS inconsistent with provision 4.3.2 of the SDCP 2012	Section 6.4.2.2
P3	The proposal will overlook into bedroom and living room windows of the Princeton apartments	Section 6.4.2.3
P4	Incorrect categorisation of Northern facing windows in Prinetown as typically secondary windows	Section 6.4.2
Overshadowing of Hyde Park		
OSH1	Non-compliance with the Sydney LEP and Sun Access Plane limits (clause 6.17)	Section 6.4.3.1
OSH2	Unacceptable breach of the sun access plane between April to September	Section 6.4.3.1
OSH3	Proposal will set a precedent for high buildings that increase the shadow creep at Hyde Park during the winter months	Section 6.4.3.1
OSH4	Where there is an existing approved building on an adjoining site, it is incumbent on the new development to provide the separation requirements	Section 6.4.3.2

Building Separation		
BS1	Minimum building separation should be 18-25 meters above 9 storeys	Section 6.4.3.2
BS2	Proximity of proposal to existing building is a fire risk	Section 6.4.3.3
BS3	ADG states that this is a separation requirement, not a setback provision. Therefore, whether there is an existing building, any new development should comply with separation requirements	Section 6.4.3.2
BS4	The Applicant has proposed a separation distance between its project and Princeton to be 12metres. This is not acceptable and not compliant with 2F ADG	Section 6.4.3.2
BS5	Non-compliance with 2F ADG (3m-12m building separation)	Section 6.4.3.2
Loss of private views		
PV1	Absence of a covenant of Princeton's title regarding potential for northern views to be affected in future	Section 6.4.5.4
PV2	View loss from future Greenland building	Section 6.4.5.2
PV3	View loss from Century Tower	Section 6.4.5.3
PV4	View loss from the Princeton Apartments	Section 6.4.5.1
Wind Impacts		
W1	Reduced access to sea breezes will increase reliance on artificial cooling within adjacent homes	Section 6.4.6.1
W2	Height and width of the development will create a wind canyon along Pitt Street	Section 6.4.6.1
Heritage Impacts		
HI1	Heritage façade should be retained for the Pitt Street South Metro Station under SSD_8876	Section 6.4.7.3
HI2	Impact upon nearby heritage building including the Edinburgh Castle Hotel, Castlereagh Fire Station, The Queen Victoria Building, St Andrews Cathedral	Section 6.4.7.1
HI3	Overshadowing of nearby heritage buildings including the former Sydney Water Building, Edinburgh Castle Hotel, City of Sydney Fire Station	Section 6.4.7.2
HI4	Proposed envelop is unusual, ugly shape wrapping around two sides of the Edinburgh Castle Hotel	Section 6.4.7.1
Building Setbacks		
SB1	Inadequate setbacks along Bathurst Street	Section 8.1.1
SB2	Western setbacks should be enforced	Section 8.1.2
Vehicular and pedestrian movements		
M1	Inacceptable increase in vehicular and pedestrian congestion	Section 6.4.9.1
M2	Potential increase in vehicular and pedestrian accidents	Section 6.4.9.1
M3	Additional car spaces will contribute to traffic congestion	Section 6.4.9.2
Other Amenity Impacts		
A1	Open-air spaces will have a significant noise impact upon neighbouring apartments	Section 6.4.10.1

A2	Enclosure of new buildings and increasing levels of traffic will contribute to higher noise levels in the local area	Section 6.4.10.2
A3	Lack of detail about the plant location and specifications on the lower levels of the proposal	Section 6.4.10.3
A4	Disturbance, dust and noise from the station ventilation plant	Section 6.4.11
Communication		
C1	Failure to provide additional information including further diagrams, and rationale	Section 6.4.12.1
C2	Communications and engagement activities should have been undertaken earlier in the planning process	Section 6.4.12.2
Open and recreational spaces		
OR1	Create more greenery, vegetation and parkland spaces	Section 6.4.4.1
Financial Impacts		
F1	Adjacent properties will decrease in value	Section 6.4.4.2

Where issues addressed – submissions in support

Issue Code	Issue	Where addressed in report
Rail Station Design		
D1	Location of Pitt Street South station entrance	6.3.2.1
Building Height		
HS1	Building height should be increased	6.3.1.1
Building Use		
U1	Addition of retail spaces on the street and within the podium	3.3.3.1

Issues raised by submissions

Code Number	Address	Nature of submission	Issues raised
279803	Sydney, NSW	Object	OD1, OS1, OS2, OSH1, OSH2, BS1, HI2, HI3, M1
279797	Sydney, NSW	Object	OD1, OD2, OS4, OSH1, OSH2, PV2, MI, M2, M3
279793	Sydney, NSW	Object	OD1, OD2, OS2, OS4, PV2, PV3, SB1, M1, M2, M3
279581	Sydney, NSW	Object	OD1M, HI1, HI2, HI3,
279809	Sydney, NSW	Object	OS1, OS2, P2, P3, OSH1, BS1, BS5, PV4
279807	Sydney, NSW	Object	OS1, OS2, OS7, BS1, BS5, PV4, M1
279805	Sydney, NSW	Object	OD2, OS2, OS6, P1, P2, P3, OSH1, OSH2, BS3, BS5, PV4, WI, HI2, A2, A3, A4
279392	Sydney, NSW	Object	OD1, OD2, OS2, HI1, M1, OR1
279975	Sydney, NSW	Object	OD1, OD4, OD7, OS4, OS6, PV2, W1, W2
279971	Sydney, NSW	Object	OD1, OD2, OS4, OS6, SB1, M1, M2, M3
279922	Sydney, NSW	Object	OD1, P2, P3, BS4, BS5, HI2, M1, M3
279916	Sydney, NSW	Object	OD1, OS1, OS2, P2, P3, OSH2, BS4, PV4, SB1, SB2
279913	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS6, OSH2, BS1, BS4, HI1, HI2
279896	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS4, OS6, P2, P3, BS1, BS5
279886	Sydney, NSW	Object	OD1, OS1, OS2, OSH2, PV1, PV4, F1
279872	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS6, OS7, P2, P3, BS1, BS2, BS3, BS4, BS5, W1, A2
279851	Sydney, NSW	Object	OD1, OD3, OD7, BS1, M1, M3
279823	Sydney, NSW	Object	OD1, OD2, OS2, P1,2, OSH1, OSH2, BS1, BS5
279821	Darlington, NSW	Object	OD1, OD7, OS1, OS4, OS5, OS6, OSH2, BS5
279828	Sydney, NSW	Object	OD1, OD7, OS1, OS2, OSH1, OSH2, BS1, BS5, HI2, HI3
279825	Sydney, NSW	Object	OD1, P2, OSH1, OSH2, BS1, BS5, F1
280132	Sydney, NSW	Object	OD1, OS1, P2, P3, OSH1, OSH2, PV4
280146	Sydney, NSW	Object	OD1, OD2, C2
280143	Sydney, NSW	Object	OD1, OD8, OS2, P2, BS1, BS5, HI2, C2
280025	Sydney, NSW	Object	OD1, OD4, OS1, P3, OSH2, PV4
280017	Sydney, NSW	Object	OD1, OD2, OD3, OS1, OS2, OS6, OSH1, BS1, BS5, SB1, SB2
280004	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS5, P2, OSH1, OSH2, BS1, BS3, BS4, BS5, C2, HI2
280002	Sydney, NSW	Object	OD1, OD2, OS1, OS2, P2, BS1, BS5, PV4, W2, M1, M2, M3, A2, A4
280572	Sydney, NSW	Object	OD1, OD2, OD7, OS6, P2, PV2, PV3, PV4, W1
280568	Sydney, NSW	Object	OD1, OD2, OD7, OS1, OS4, A2
280889	Sydney, NSW	Object	OD1, OD2, OD4, OD7, OS1, B2, PV4, W2, M1, C1
280885	Sydney, NSW	Object	OD1, OD2, OS1, OS2, BS1, PV4
280903	Sydney, NSW	Object	OD1, OD2, OD7, OS1, P2, P3, PV1, A1, A2, M1, M2, M3
280897	Sydney, NSW	Object	OD1, OS1, OS2, OSH1, OSH2, BS1, BS5

280895	Sydney, NSW	Object	OD1, OD7, OS1, OS2, OS5, OS6, OS7, P2, P3, BS3, BS4, BS5, PV2, PV4, HI2, HI3, M1, M2, M3, C1, C2
280893	Sydney, NSW	Object	OD1, OD2, P3, OSH1, OSH2, BS1, PV4
280564	Sydney, NSW	Object	OD1, OD2, OD4, OD5, OS1, OS2, OS4, P2, P3, PV4, W2, HI1
280185	Sydney, NSW	Object	OD1, OD2, OS1, OS2, P2, P3, BS5, PV4, HI2, SB1, SB2
280151	Sydney, NSW	Object	OS1, OS2, OS6, P2, OSH1, OSH2, BS1, BS4, BS5
280149	Sydney, NSW	Object	OD1, OS1, OS2, P3, OSH1, B3, PV4, HI2
280191	Sydney, NSW	Object	OS1, OS2, P3, OSH1, B3, PV4, HI2, HI3, C1, C2
280187	Sydney, NSW	Object	OD1, OD3, OSH3, BS1, SB1, SB2,
281184	Sydney NSW	Object	OD1, OS1, P3, OSH1, PV4, HI2, HI3, F1
279384	Darlinghurst NSW	Object	OD1, OD3, OS1, OS2, OS6, P1, P2, OSH1, OSH2, PV2, PV4
280021	Sydney, NSW	Object	OD1, OD2, OS1, OSH1, OSH2, B35, PV4, HI2, HI3
280031	Sydney, NSW	Object	OD6, OS1, OS2, OS3, OS6, P4, OSH1, BS1, BS2, BS5, PV1, SB2, C1, C2
290035	Sydney, NSW	Object	OD1, OS1, OS2, OS7, P2, OSH1, BS1, BS2, BS5, PV2, PV4, W1, HI3, A2, A3, A4, OR1
279801	Sydney, NSW	Object	OD1, OD4, OD7, W2, M2, C1
279815	Matraville, NSW	Object	OD2, OS1, OS2, OS6, P2, BS5
280009	Waterloo, NSW	Object	OSH1, OSH2, OSH3, OSH4
279968	Sydney, NSW	Object	OD1, OD2, P2, P3, OSH1, OSH2, PV4
279952	Sydney, NSW	Object	OS1, P3, PV4
279966	Sydney, NSW	Object	OS1, OS2, OSH1, OSH2, BS1
280180	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS6, OSH1, OSH2, BS5
280199	Sydney, NSW	Object	OS2, P2, P3, OSH1, BS2, BS5, WI, HI2, HI3, A2
280023	Sydney, NSW	Object	OD1, OD2, OS2, P2, P3, BS1, BS5, HI1, HI2, HI3
280012	Sydney, NSW	Object	OD1, OD2, OD7, OS1, OS7, P3, BS1, PV4, WI, A1, A3
280029	Sydney, NSW	Object	OS2, P2, BS5, HI3, C1
280033	Sydney, NSW	Object	OD1, OD2, OS1, OS2, P1, P2, P3, OSH1, OSH2, BS1, BS5, PV4, A2, A4
279813	Sydney, NSW	Object	OS1, P3, OSH1, OSH2, BS5, PV4, M1
280213	Sydney, NSW	Object	OD1, OD2, OS1, OS2, P3, OSH1, BS5
279799	Sydney, NSW	Object	OD1, OS1, BS1, PV4
279341	Sydney, NSW	Object	OD1, OS1, OS2, P2, P3, OSH1, BS5
280233	Alexandria NSW	Object	OD1, OD2, OD4, W2, HI2, M1, OR1
280014	Sydney, NSW	Object	BS5, HI2, HI3, HI4, SB1, SB2
280237	Sydney, NSW	Object	OD1, OD7, OSH1, PV3, WI, W2, HI2, M1, A1, A2, CI, C2
283284	Sydney, NSW (PV)	Object	OD1, OD2, W2, HI1, HI2, OR1
279817	Sydney, NSW	Object	P2, P3, BS1, BS5, PV4, W1, C2
279995	Pennant Hills, NSW	Object	OD1, OD2, OS1, OS2, OS5, OS6, P1, P2, P3, OSH1, OHS2, BS1, BS3, BS4, BS5, PV4, HI2, HI3, HI4, A2, A3, A4
279920	Sydney, NSW	Object	OD1, OD2, OS1, P2, OSH1, BS5, A3, A4
279579	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS6, P3, P4, BS1, BS5, PV4
280019	Sydney, NSW	Object	OD1, OD2, OSH1, BS1, BS5, HI1
280211	Sydney, NSW	Object	OD1, OD2, P2, OSH1, BS5, PV4, HI2
279878	Sydney, NSW	Object	OD1, OD2, OS1, OS2, OS6, P1, P3, OSH1, OSH2, BS1, BS5, PV4, HI2, HI3
280027	Sydney, NSW	Object	OD1, OS2, P2, OSH1, BS5, C1
279562	Sydney, NSW	Object	OD1, OD7, OS2, PV3, W2
279554	Sydney, NSW	Object	OD2, OS2, P2, OSH1, OSH2, BS1, HI3
280037	Sydney, NSW	Object	OD1, OD2, OS1, OS2, P3, BS1, BS5, PV4, A3, A4
279577	Sydney, NSW	Object	OD1, OD2, OS2, OSH1, BS1, BS5
277203	Surry Hills, NSW	Support	HS1
276725	Sydney, NSW	Support	D1, U1
276705	Sydney, NSW	Support	D1

**GOVERNMENT
ARCHITECT NSW
ENDORSEMENT LETTER**

APPENDIX H





17 July 2018

Tim Parker
Project Director
Sydney Metro City and
Southwest
Level 43, 680 George Street
Sydney 2000

Sydney Metro City and Southwest - Design Excellence and Design Competitions

Dear Tim,

Thank you for your letter of July 9th and the accompanying fact sheet. GANSW support the proposed strategy as an alternative to a design competition run in accordance with the Sydney LEP, for integrated station development sites where development is located above the station 'box'.

GANSW acknowledge the commitment to design excellence demonstrated by Sydney Metro and the complexity of delivering integrated station development. We note and support the inclusion of local government representation in the Design Excellence Evaluation Panel (DEEP) and the line of sight provided for the DEEP and its advice to the Metro Tender Review Panel through the DEEP report and its presentation to the Tender Review Panel by the DEEP Chair.

The limitations that Authorised Engineering Organisation (AEO) requirements place on the range of design firms able to participate in tenders for ISD projects restricts the diversity of firms available to Sydney Metro. Therefore, the move to encourage partnering of non-AEO firms with AEO teams as a means to overcome this limitation is strongly supported. A world class city such as Sydney deserves the highest quality of design and innovation for its public domain, infrastructure and tall buildings. The delivery of design excellence is best served by the participation of a wide range of design firms with diverse design approaches, representing design excellence in fields outside rail development. The EOI process for shortlisting teams should foreground diversity and innovation for design teams to ensure the best value for government and the public is achieved through this process.

We support the continuing involvement of the Metro DRP in a Design Integrity role for each station development, and would strongly support and recommend engagement with the State Design Review Panel (SDRP) through the Stage 2 design development process for each station. The SDRP provides the most independent, robust and accountable mechanism for design quality advice for project proponents and DPE assessments.

Sincerely,

Peter Poulet
Government Architect NSW
cc. Anthea Sargeant, Ben Lusher

Government Architect
New South Wales
L24, 320 Pitt Street
Sydney NSW 2000
GPO Box 39
Sydney NSW 2001

government.architect
@planning.nsw.gov.au
T +61 (02)9860 1464

governmentarchitect.nsw.gov.au

CITY OF SYDNEY CORRESPONDENCE

APPENDIX I



[REDACTED]

From: [REDACTED]
Sent: Friday, 28 September 2018 3:09 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: PSN & PSS Concept SSD Applications - Res
Attachments: PSS OSD 20180808.jpg; 2018-08-08 Appendix C_Architectural Drawings of Proposed Building Envelo....PDF; PSN OSD 20180808.jpg; 2018-08-08 Appendix C_Architectural Drawings_Building Envelope.PDF

Follow Up Flag: Follow up
Flag Status: Completed

Dear [REDACTED]

PSN OSD tower modelled from information in submitted drawings within attached PDF (with minimum setbacks 6m). Portion of tower over LEP2015 SAP HP West 3 is within Cat B land and is not higher than Cat A building.

PSS OSD tower modelled from information in submitted drawings within attached PDF and height of volume cut by SAP HP West 3.

Heights are annotated in attached images:

PSN OSD 20180808.jpg

PSS OSD 20180808.jpg

Regards,

[REDACTED]



Telephone: +612 9265 9960
cityofsydney.nsw.gov.au

ESD REPORT ADDENDUM

APPENDIX J



2 November 2018

To: OSD Planning Team

Our ref: 2125561

Your ref:

**Sydney Metro City & South West
Pitt Street South Over Station Development
Ecologically Sustainable Development Report Letter Addendum – Performance
Targets**

GHD has prepared an Ecologically Sustainable Development (ESD) report for the Pitt Street South Over Station Development which included a number of ESD minimum performance targets.

Sydney Metro has revised and updated the proposed minimum performance targets for Pitt Street South, as follows:

If the entire site is a residential building:

- BASIX 35 Energy
- Exceed minimum compliance with BASIX Water. While a higher target will be sought through the Tender process and during design development, there are limited opportunities / space for the additional water saving / reuse features which would enable better performance in this area.

If the entire site is a commercial / office building:

- 5 Star NABERS Energy
- 3.5 Star NABERS Water

In terms of Green Star ratings:

- If the building is predominantly residential, then 5 Star Green Star
- If the building is predominantly office / commercial, then 5 Star Green Star

Sincerely
GHD Pty Ltd

A handwritten signature in black ink, appearing to read 'Simon Spratling', with a stylized flourish at the end.

Simon Spratling
Senior ESD Consultant
03 8687 8603

**SUPPLEMENTARY
OVERSHADOWING
IMPACT SENSITIVITY
ANALYSIS REPORT**

APPENDIX K





Sydney Metro City & Southwest Pitt Street South Over Station Development:

Supplementary Overshadowing Impact Sensitivity Analysis Report

Applicable to:	Sydney Metro City & Southwest
Author:	Steve King
Owner	Transport for NSW
Status:	Final Draft
Version:	1
Date of issue:	25 10 2018
Review date:	
© Sydney Metro 2018	

Table of Contents

1.	Preliminaries: Overshadowing impacts	3
2.	Documents	3
3.	Overshadowing Analysis	3
	3.1 Methodology	3
4.	Overshadowing Impact	4
	4.1 Potentially affected properties.....	4
	4.2 Predicted overshadowing impact	5
	4.2.1 <i>Options examined</i>	5
	4.2.2 <i>Analysis</i>	5
	4.2.3 <i>Summary outcomes</i>	6
5.	Conclusions	6
	ATTACHMENT A: VIEWS FROM THE SUN	7
	ATTACHMENT B: DETAILED TABLES	16

1. Preliminaries: Overshadowing impacts

I provide this report as an expert opinion, relating to potential overshadowing impacts on neighboring residential dwellings by the proposed development at 125 - 129 and 131 - 135 Bathurst Street and 296 - 300 and 302 Pitt Street Sydney.

Specifically, I have been tasked to test sensitivity of overshadowing of the existing Century Tower and Princeton Apartment buildings, to variations in setbacks of the subject proposal.

2. Documents

I base my report on digital 3D models of the alternative scenarios, supplied to me by GHDWoodhead, architects.

3. Overshadowing Analysis

3.1 Methodology

My analysis of overshadowing has again been carried out by use of 3D digital models in the *Trimble SketchUp* software package. This model analysis is effectively identical to that normally undertaken for solar access compliance of a proposed multi-residential building under the Apartment Design Guide.

The models were prepared by GHDWoodhead, architects. The existing and proposed building models are inserted into a context of surrounding buildings derived from the so-called 'city model', approved plans obtained from the Council, and survey data supplemented by plans from marketing material.

I particularly note that the resulting composite model includes the Greenland Centre at 115 Bathurst Street, presently under construction.

I have undertaken a summary check of the topographical and building dimensions of the 3D digital model by reference to figured dimensions from the plans and sections. I cannot independently warrant other model dimensions, but I feel confident to rely on the general accuracy of the modelling.

I have independently geolocated the models, and verified the direction of True North by online reference to the cadastral grid north.

My detailed analysis relies primarily on projections known as '*View from the Sun*'. A view from the sun is an aerial perspective from a very large distance, that shows all sunlit surfaces at a given time and date. It therefore allows a very precise count of sunlight hours on any glazing or horizontal surface, with little or no requirement for secondary calculations or interpolation.

To facilitate the comparison of existing and prospective conditions, the proposed building envelope is rendered in the models as semi transparent.

A full table of views from the sun on a 30 minute interval is provided as Attachment A.

4. Overshadowing Impact

4.1 Potentially affected properties

My understanding is that commercial occupancies do not enjoy the protection of any control for overshadowing. The relevant overshadowing impacts are those to residential properties on surrounding sites. The views from the sun readily identify the residential buildings which are potentially impacted by June 21 shadows attributable to the subject proposal.

- *Century Tower*
- *Princeton Apartments*

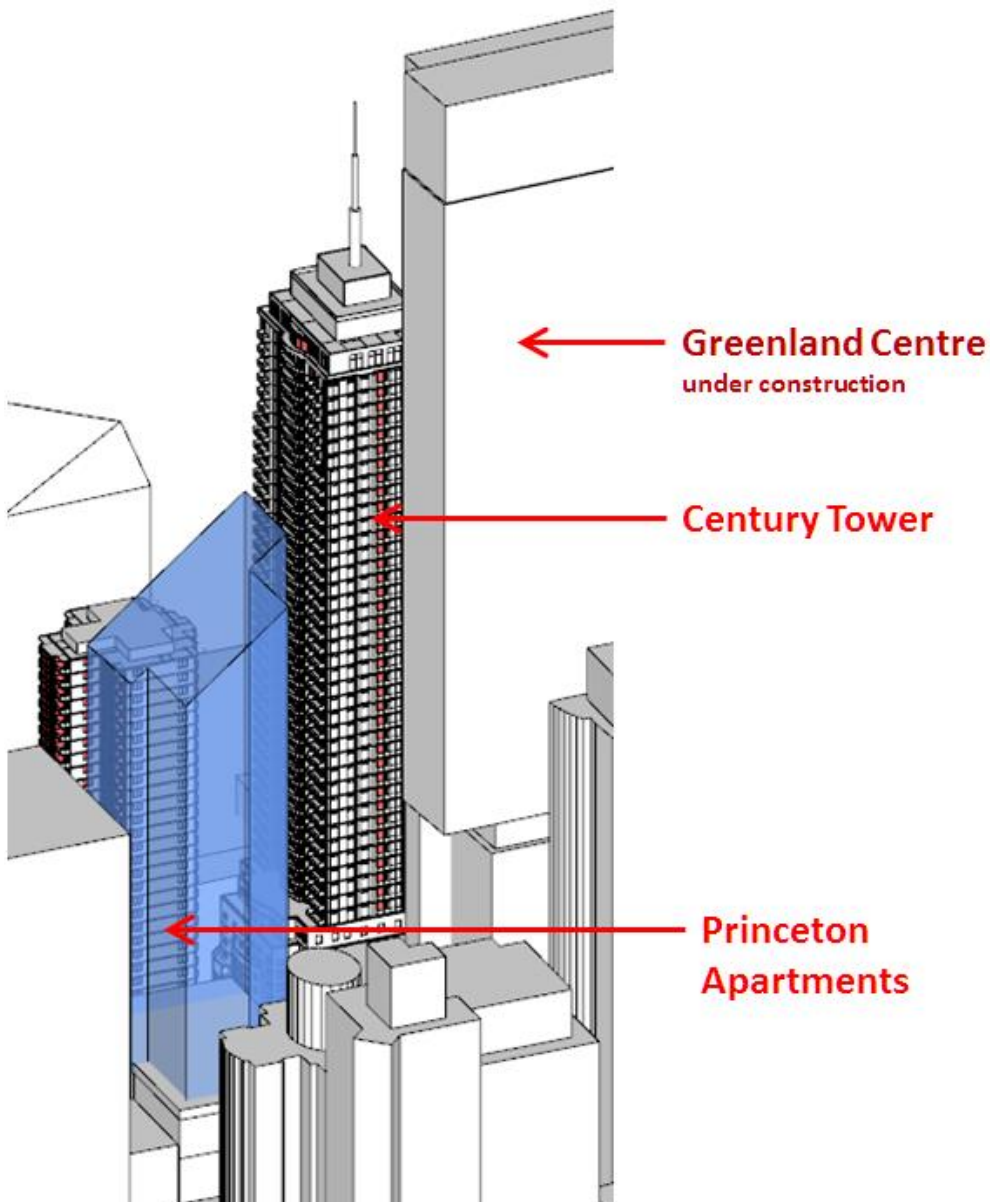


Figure 1: Adjacent properties in context of the proposal
View from the sun at 10 AM

4.2 Predicted overshadowing impact

4.2.1 Options examined

I have been provided with three discrete digital models which allow me to examine the 'retained solar access compliance' – as a measure of overshadowing impact – on both affected buildings, under the following scenarios:

- Existing
- Option 1 is the current proposal;
- Option 2 relates to setbacks which are consistent with the SDCP 2012, and maintains a 12 metre separation to the south.

Figures 2&3 record the modelled setbacks for Options 1 and 2.

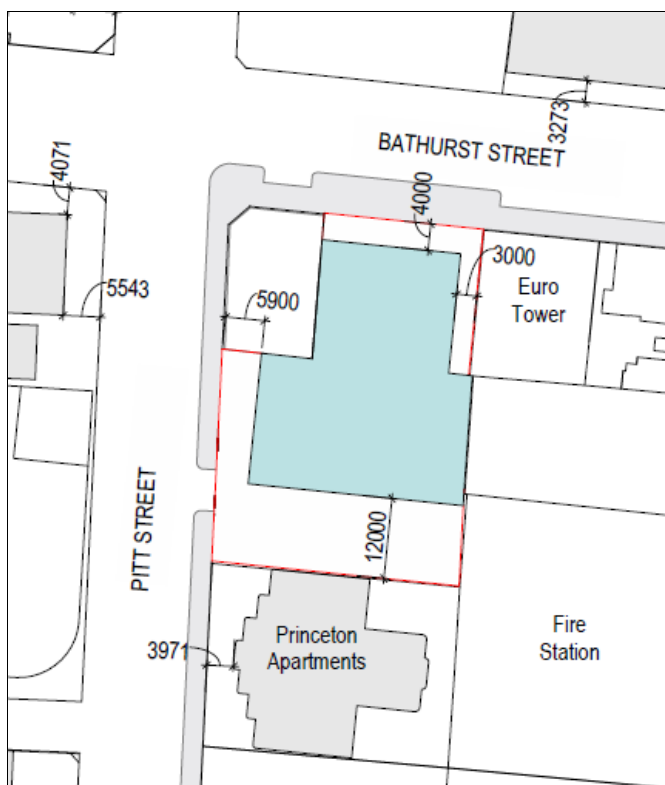


Figure 2: Model setbacks Option 1

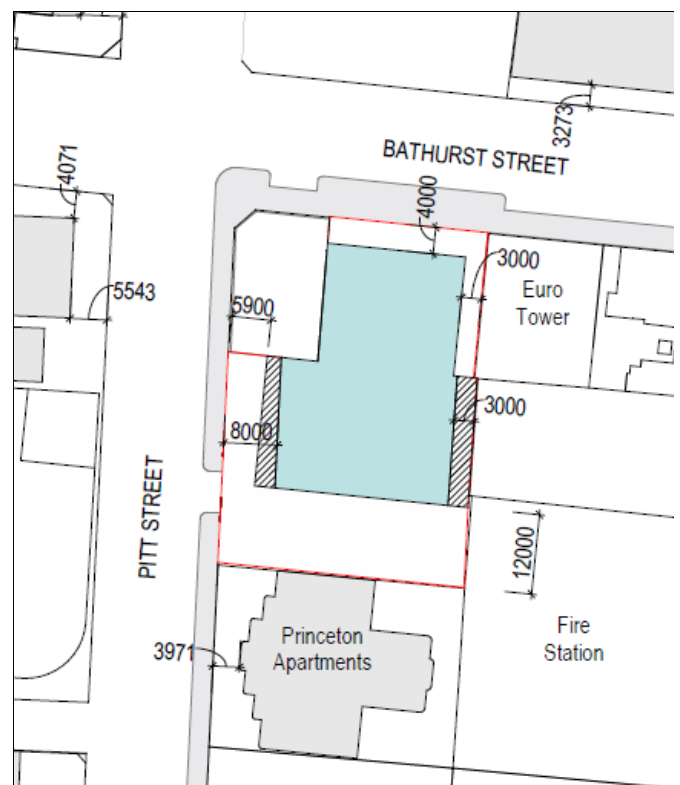


Figure 3: Model setbacks Option 2

4.2.2 Analysis

In order to make an accurate assessment of the overshadowing impact for each of the two affected buildings identified, a full analysis was carried out to record the present nominal solar access compliance of the whole building, and to identify those apartments which were likely to lose their complying status under each scenario.

The change in percentage of dwellings complying for solar access was then computed for each affected building.

The reduction in solar access compliance due to the overshadowing impact of the proposal is calculated as a proportion of the total number of apartments.

4.2.3 Summary outcomes

Table 1 summarises the solar access compliance status for both affected buildings, under all three scenarios.

Century Tower 296 units					
	>3 hrs 9-3	>2 hrs 9-3 (>3hrs 8-4)	>2 hrs 9-3	>2hrs 8-4	No sun
Existing	5 1.7%	15 5.1% 6.8%	2 0.7% 7.4%	50 16.9% 24.3%	89 30.1%
Option 1	5 1.7%	11 3.7% 5.4%	6 2.0% 7.4%	30 10.1% 17.6%	89 30.1%
Option 2	5 1.7%	12 4.1% 5.7%	5 1.7%	30 10.1%	89 30.1%
Princeton Apartments 116 units					
	>3 hrs 9-3	>2 hrs 9-3 (>3hrs 8-4)	>2 hrs 9-3	>2hrs 8-4	No sun
Existing	41 35.3%	2 1.7% 37.1%	19 16.4% 53.4%	1 0.9% 54.3%	17 14.7%
Option 1	0 0.0%	5 4.3% 4.3%	0 0.0% 4.3%	17 14.7% 19.0%	17 14.7%
Option 2	0 0.0%	11 9.5% 9.5%	0 0.0% 9.5%	15 12.9% 22.4%	17 14.7%

Table 1: Summary of solar access compliance for Century Tower and Princeton Apartments
The full takeoff tables can be provided as Excel spreadsheets.

Comparing overshadowing impact by 'retained compliance', there are very small differences between the scenarios.

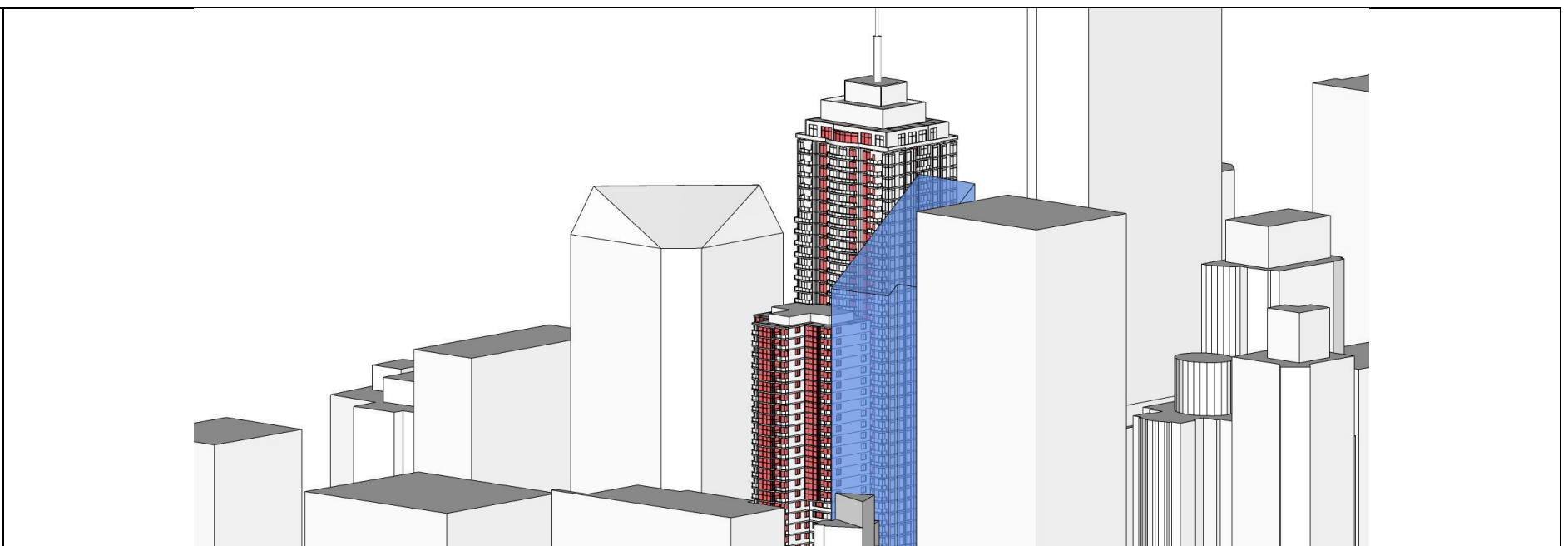
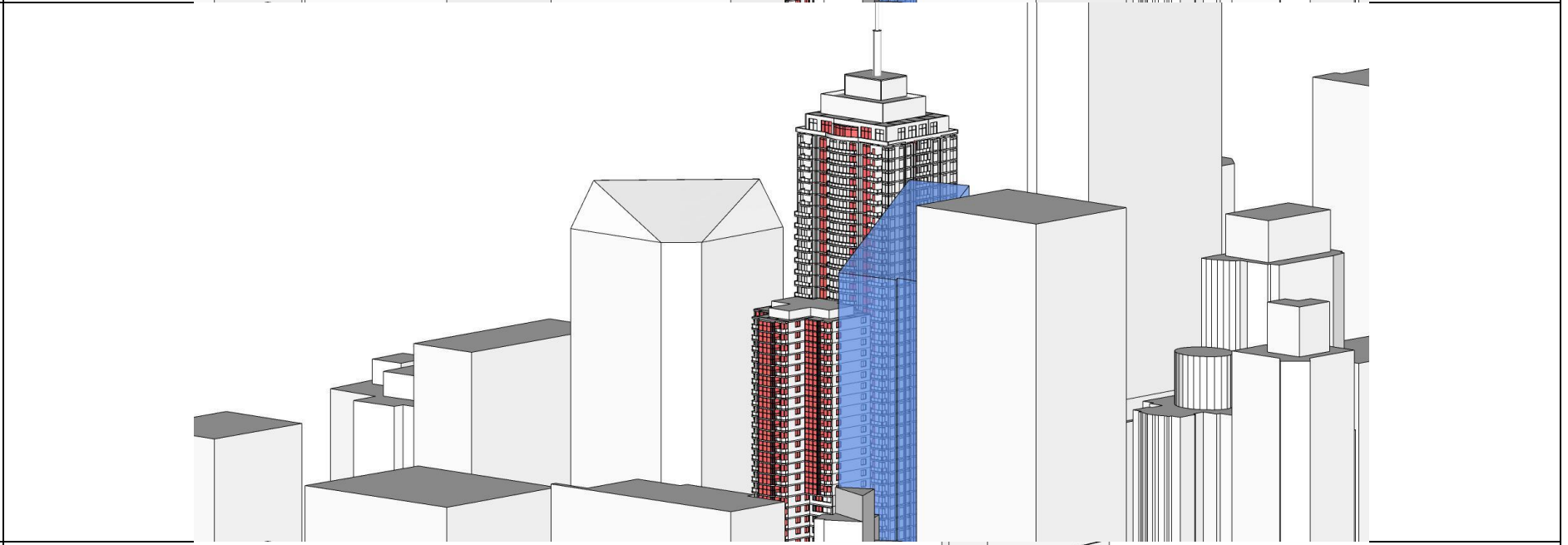
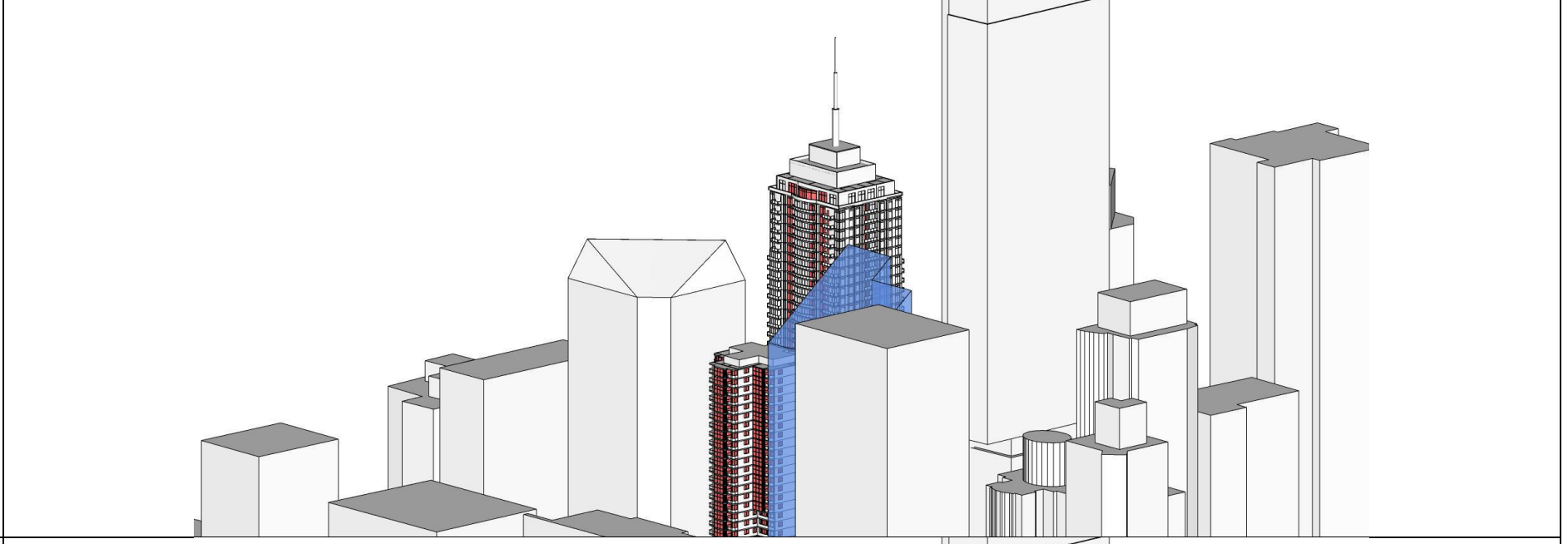
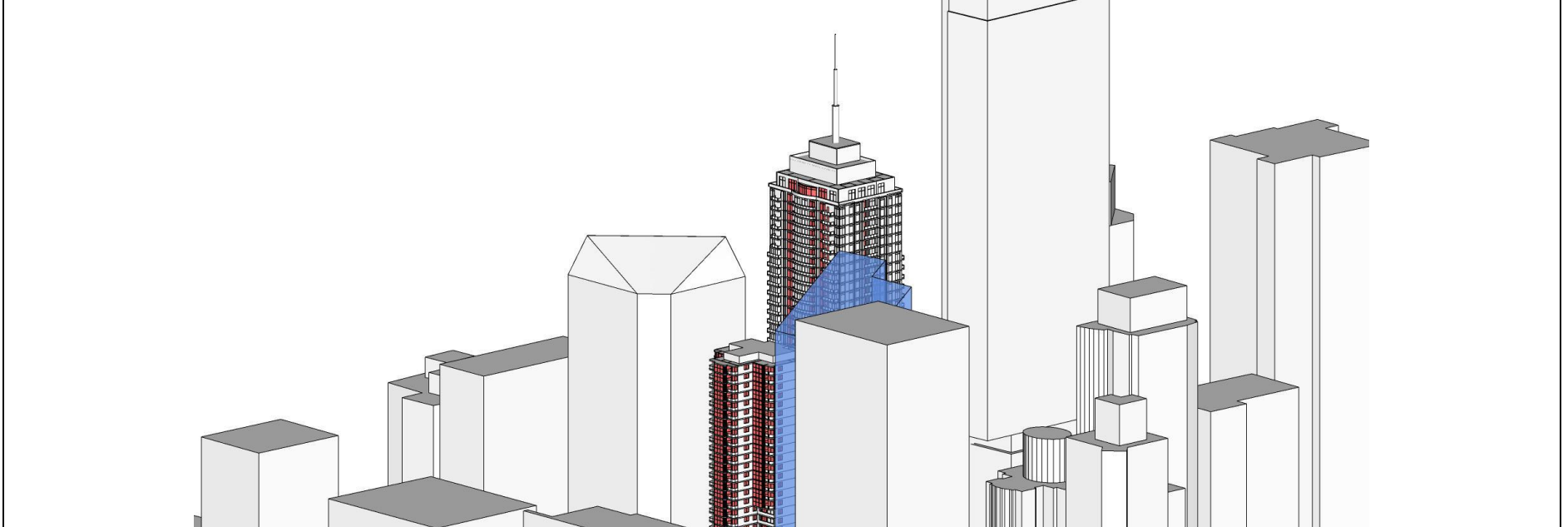
Arguably, the only change of any significance is that Option 2 (compliant east and west setback envelope) shows six (6) more apartments in Princeton Apartments retain solar access compliance of minimum two hours between 9 AM to 3 PM, than in the current proposal, Option 1 (highlighted in the table above). This is partially offset by two apartments with two hours of direct sun between 8 AM and 3 PM.

5. Conclusions

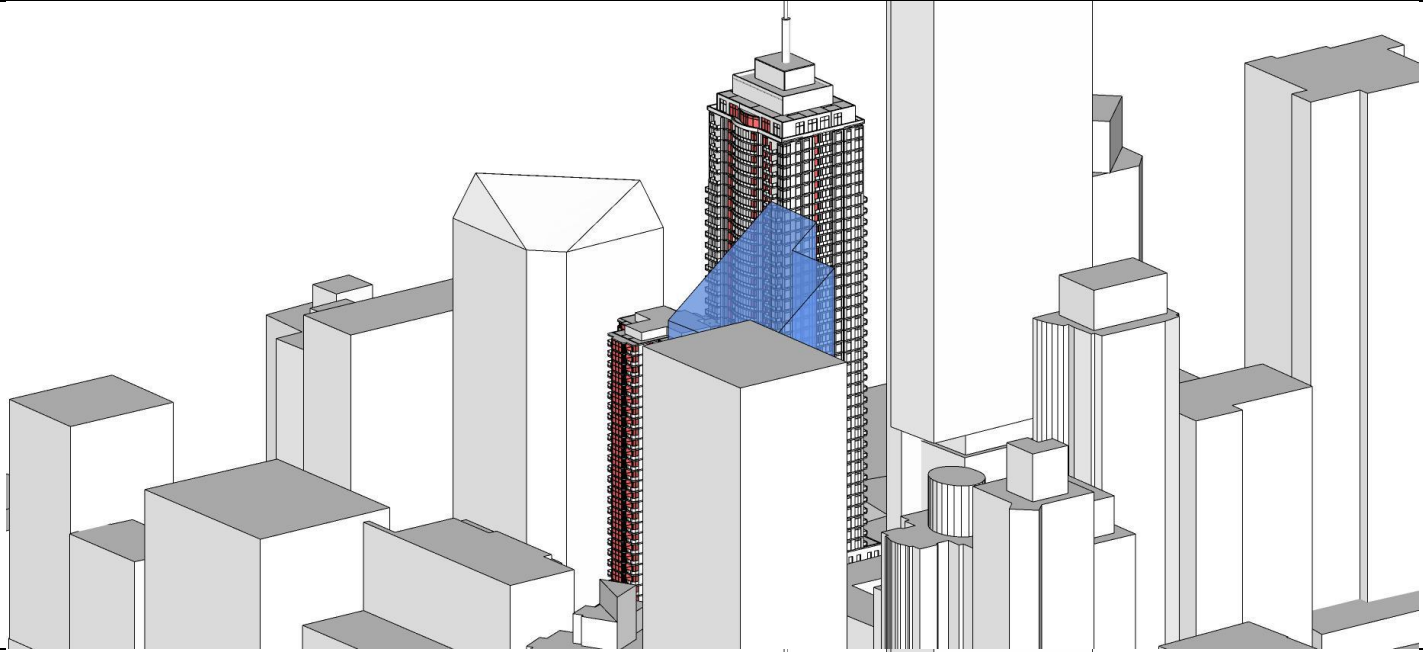
I have examined the differences in overshadowing impact of the current proposal and a scenario with compliant east and west setbacks. I find that differences in retained solar access compliance for the two scenarios are very small, and suggest that any benefit is not substantial.

ATTACHMENT A: VIEWS FROM THE SUN

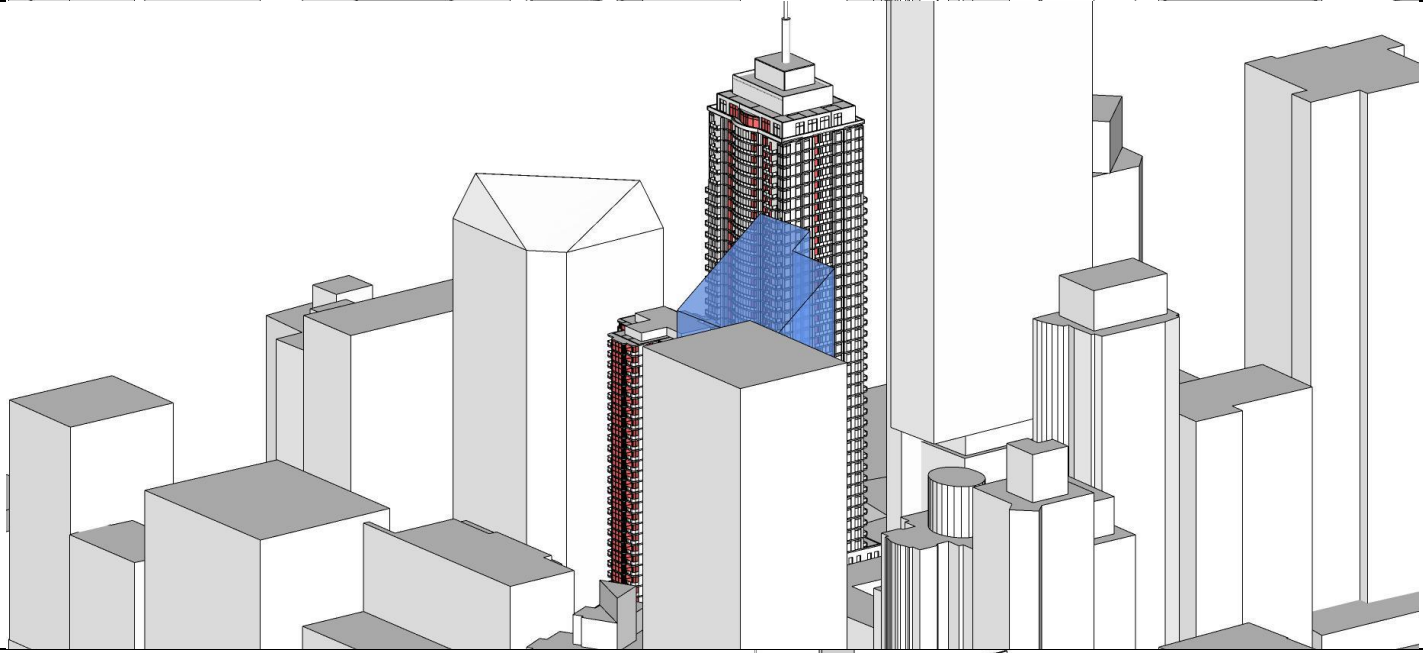
The table below reproduces for reference the detailed 'views from the sun' on a half hourly basis, comparing the building envelope as proposed, and with increased setbacks.

<p>0800 Option 1</p>	
<p>0800 Option 2</p>	
<p>0830 Option 1</p>	
<p>0830 Option 2</p>	

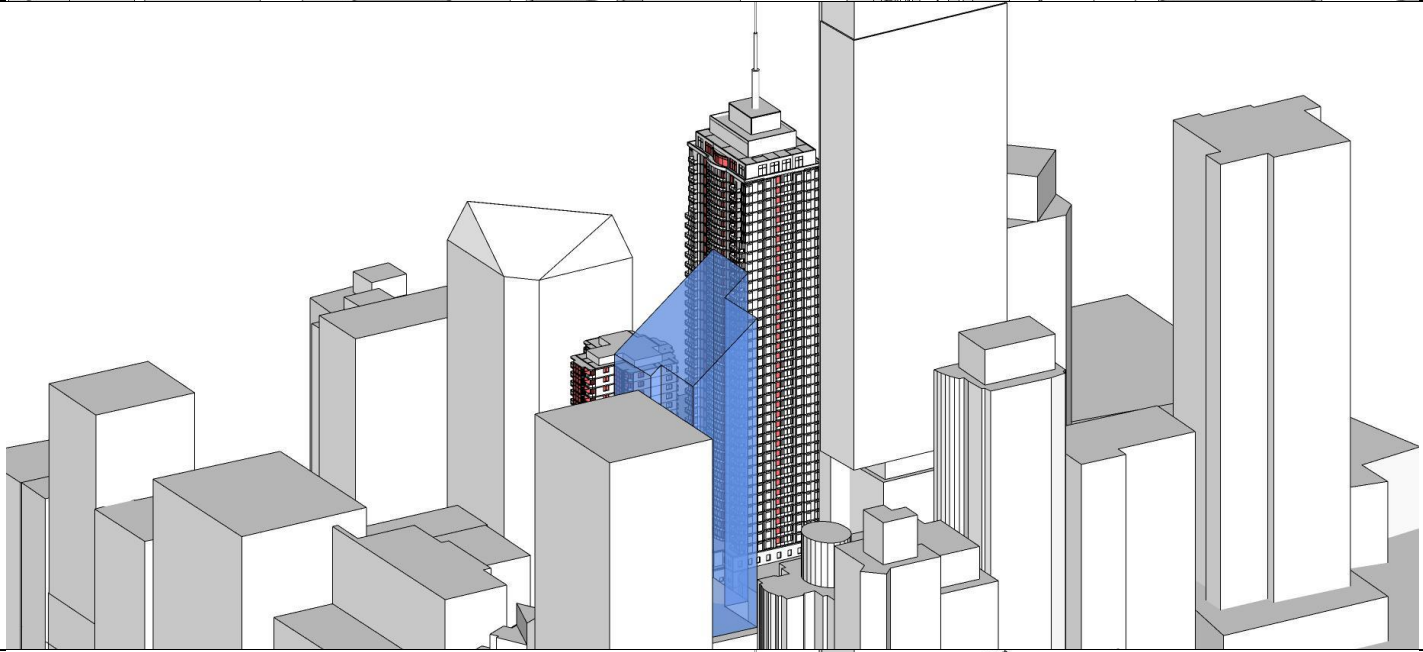
0900
Option
1



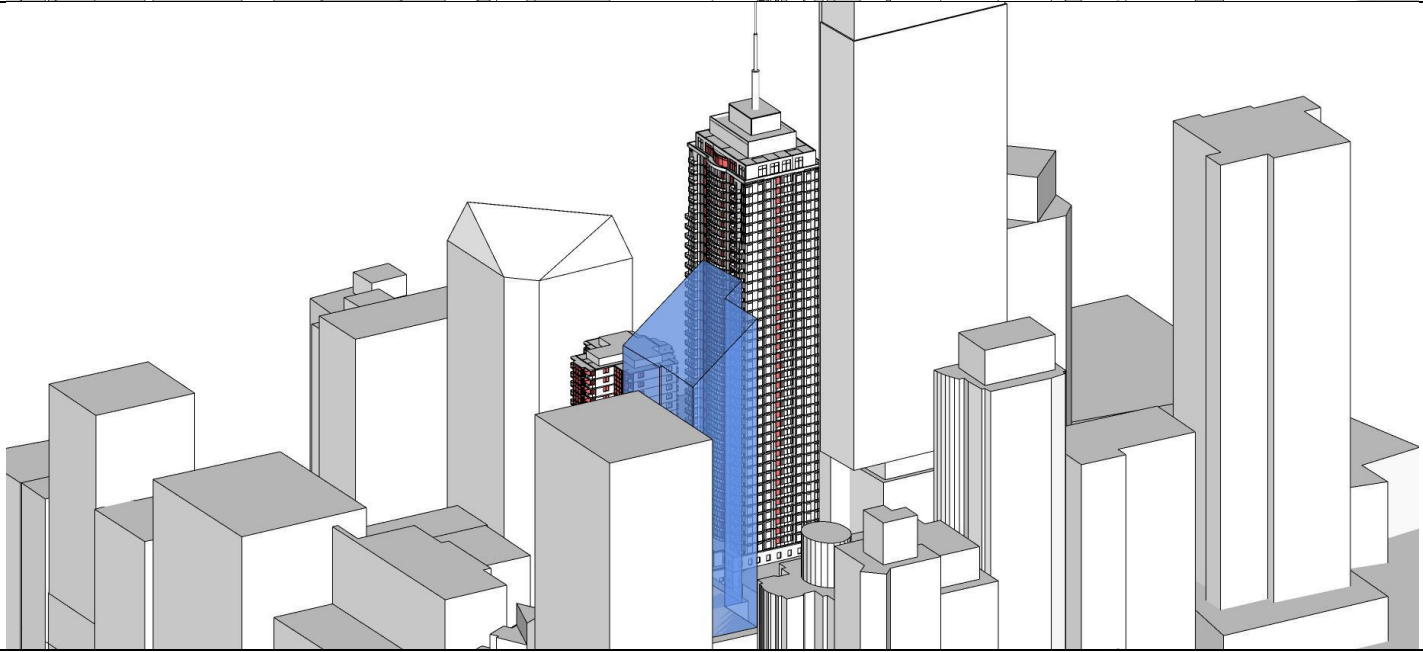
0900
Option
2



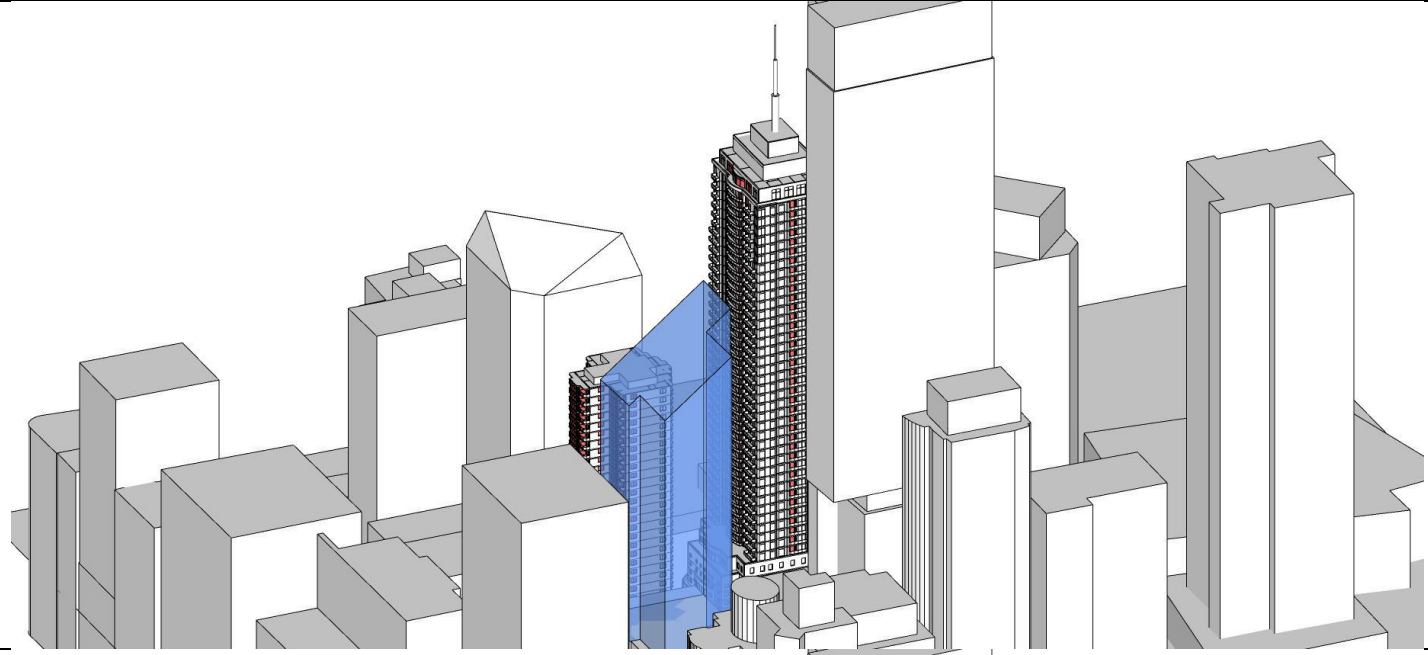
0930
Option
1



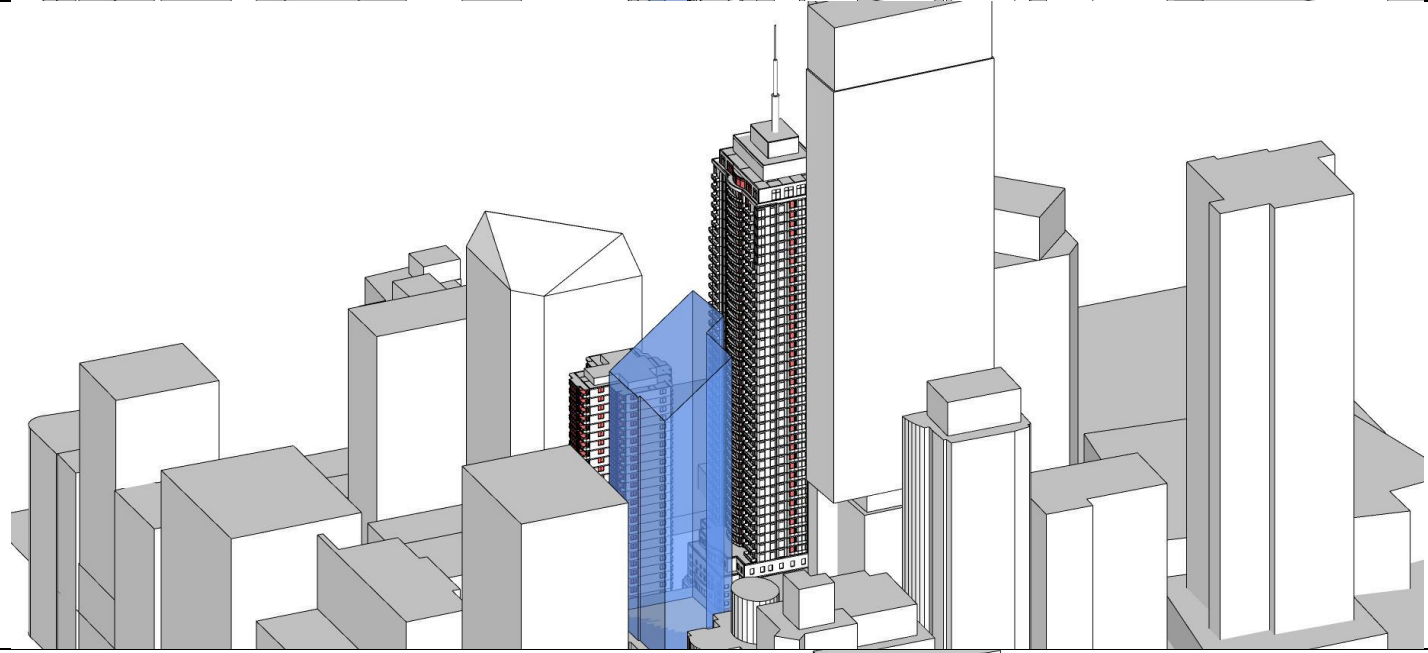
0930
Option
2



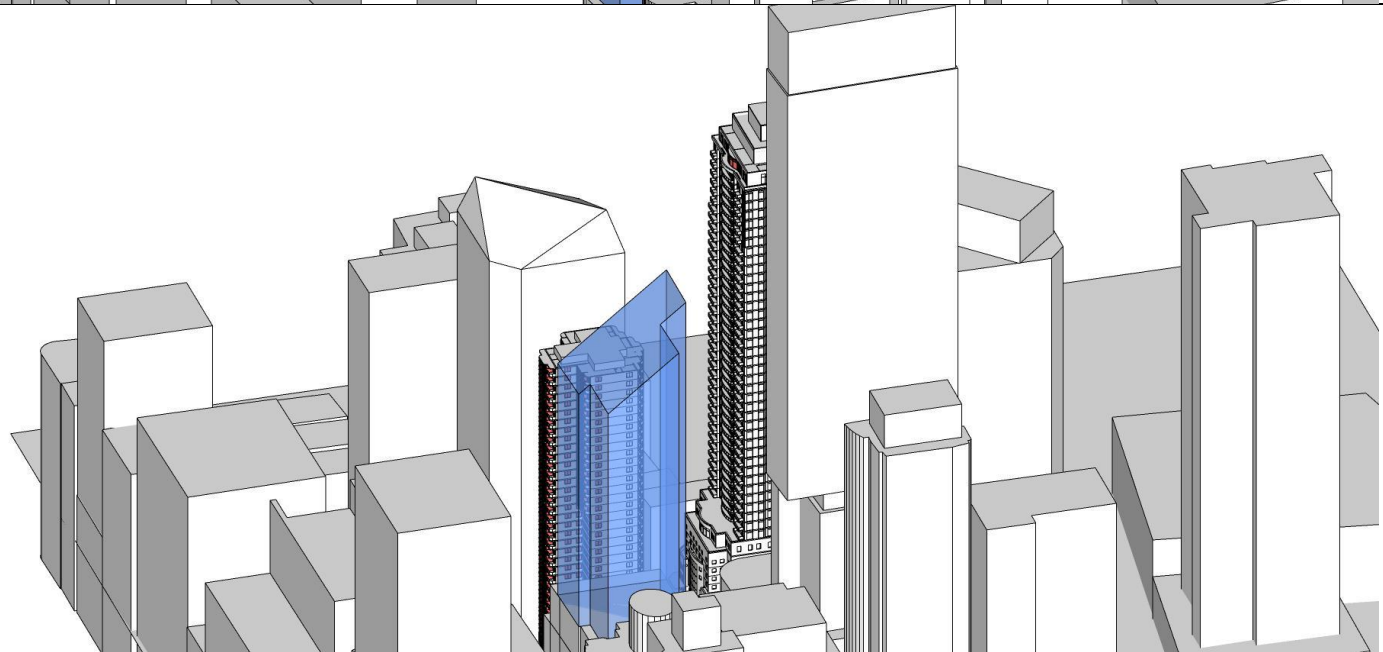
1000
Option
1



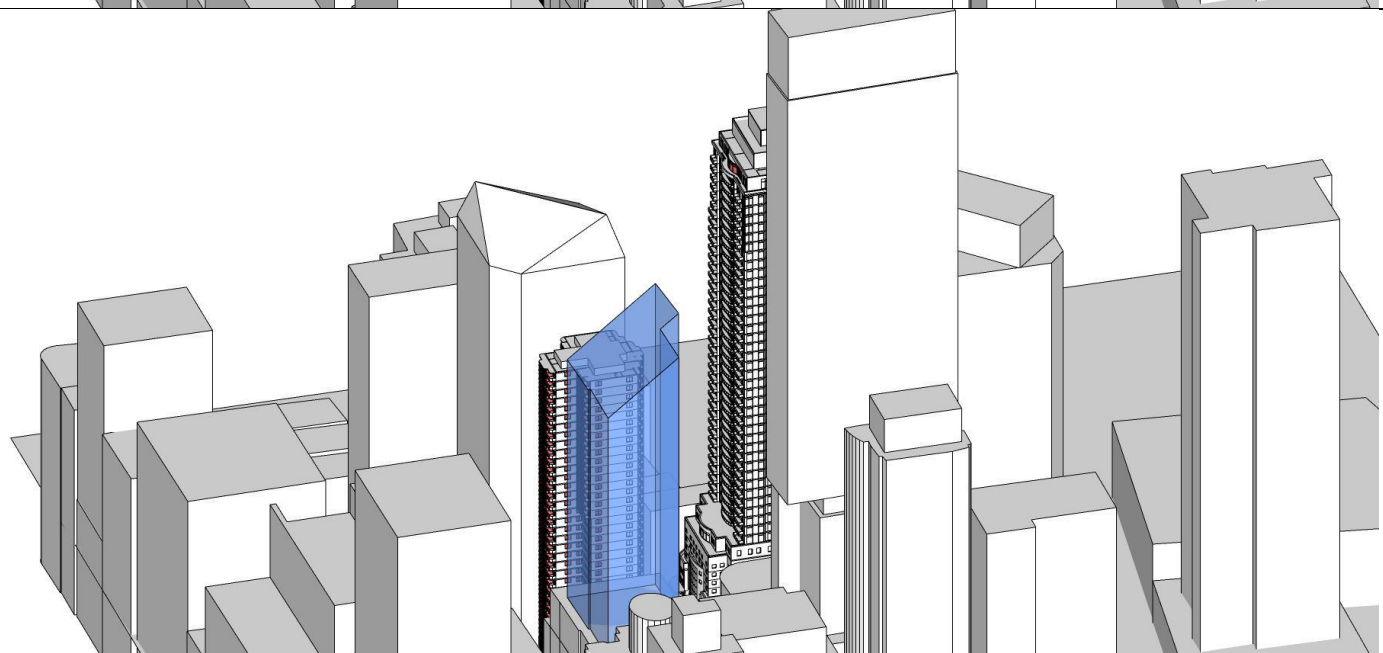
1000
Option
2



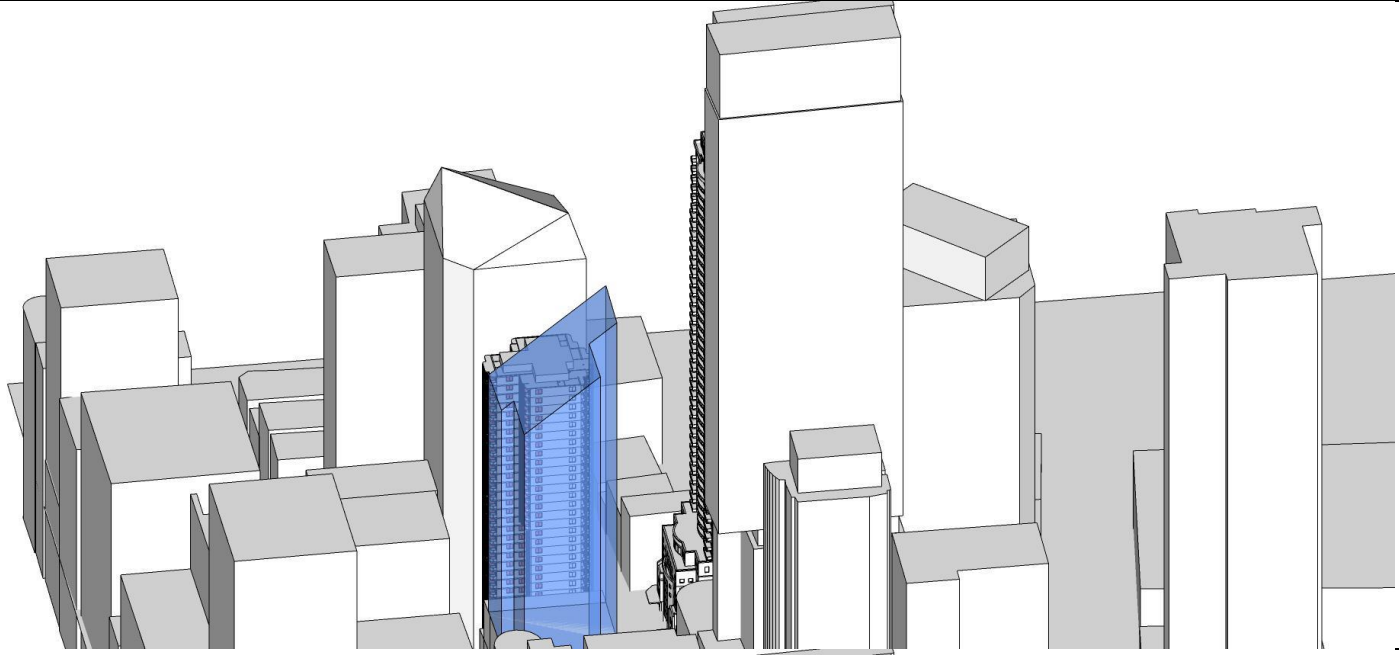
1030
Option
1



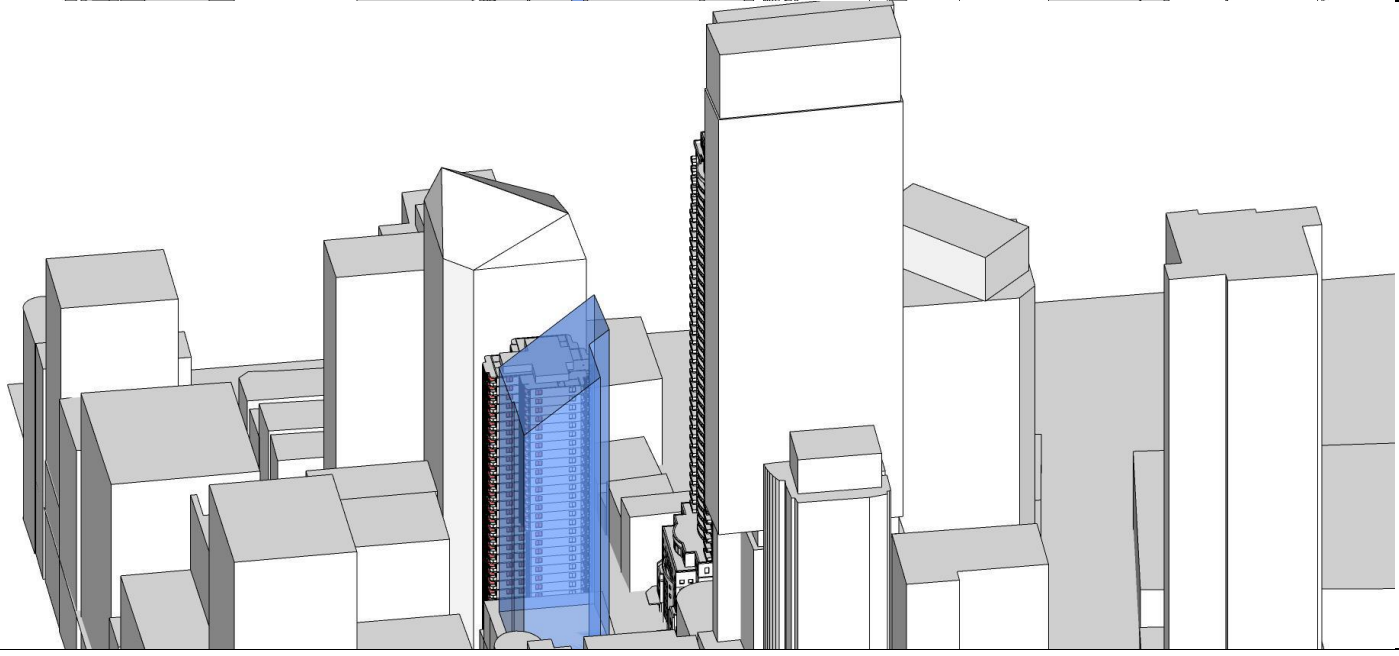
1030
Option
2



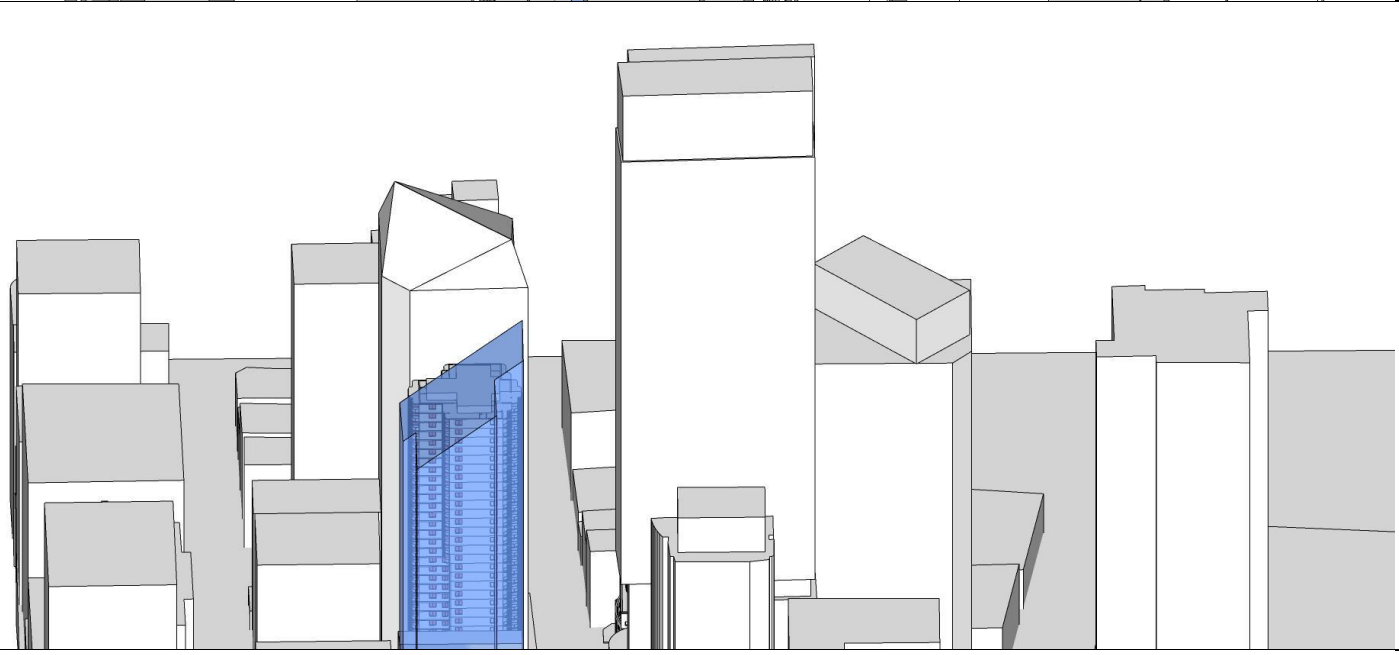
1100
Option
1



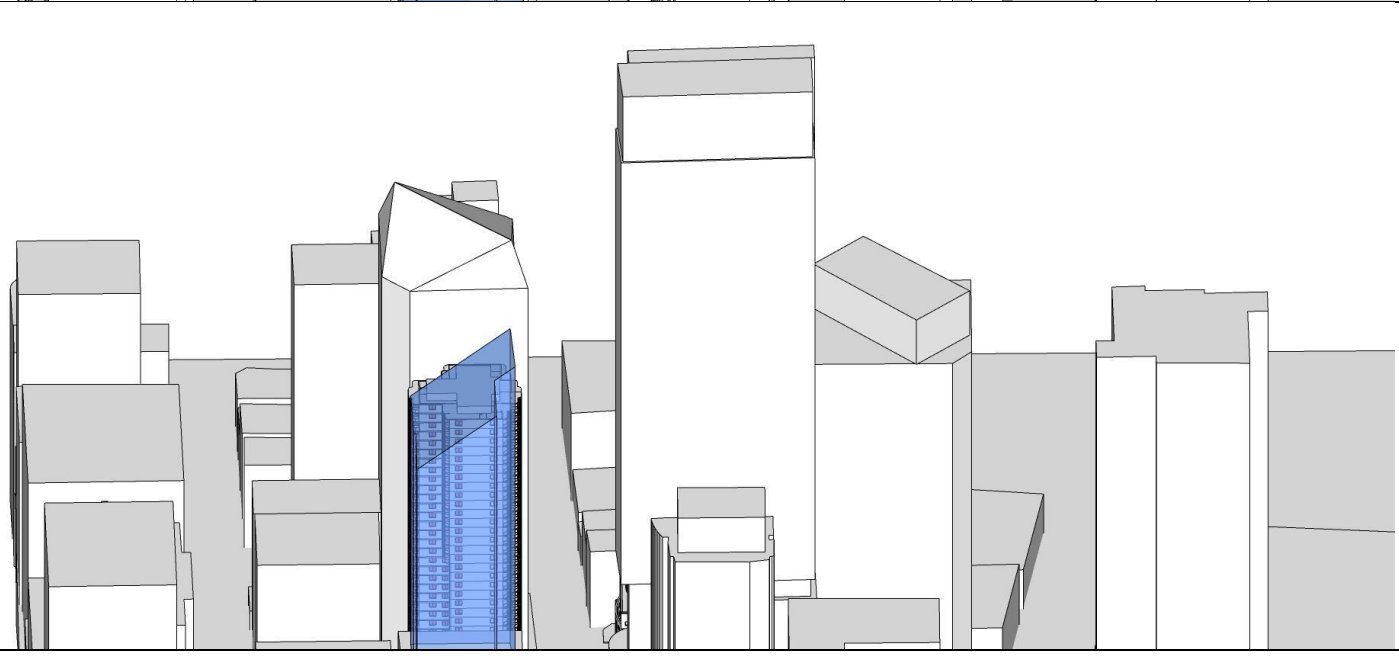
1100
Option
2



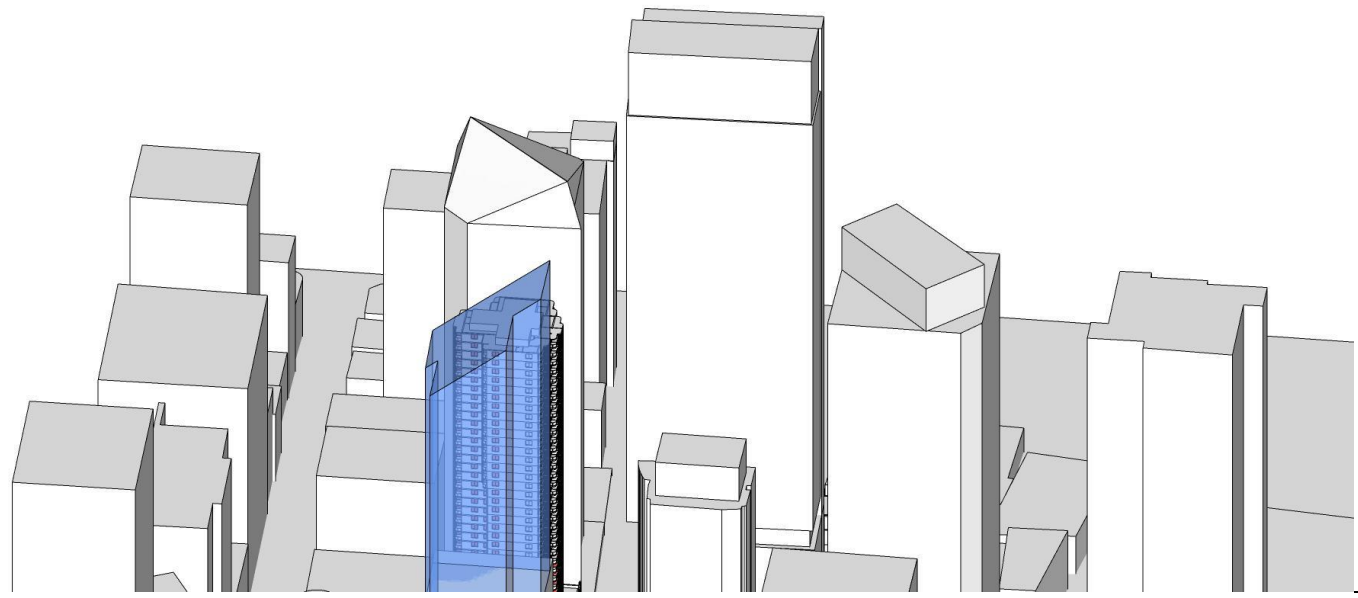
1130
Option
1



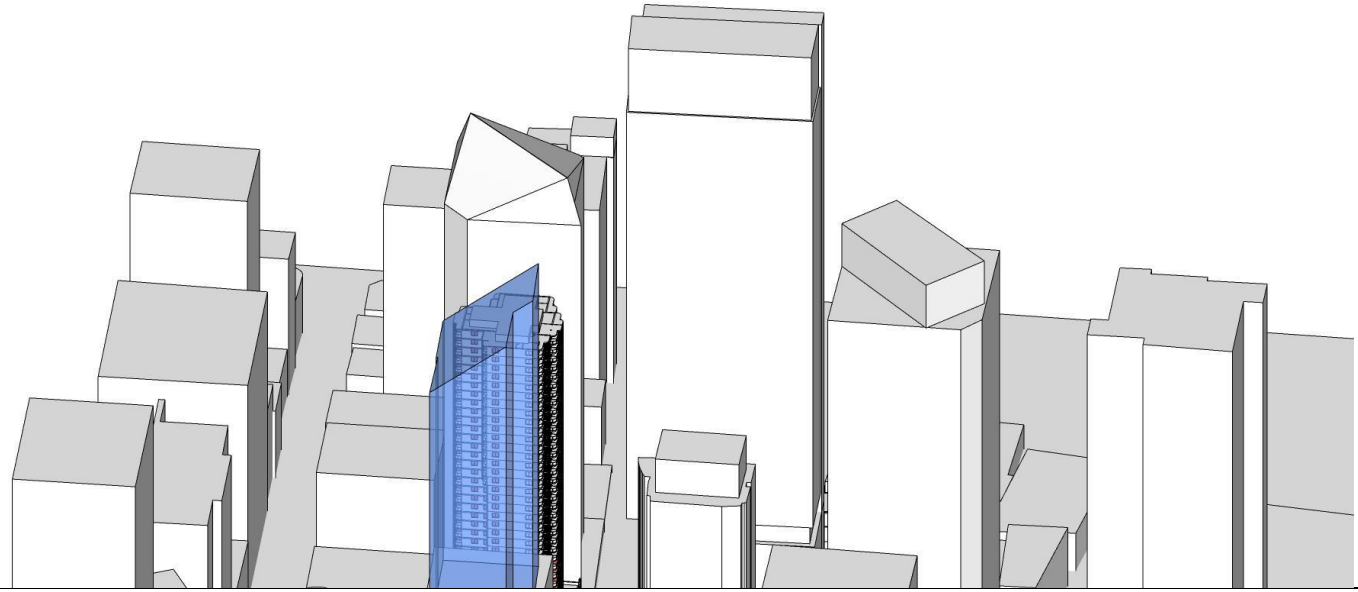
1130
Option
2



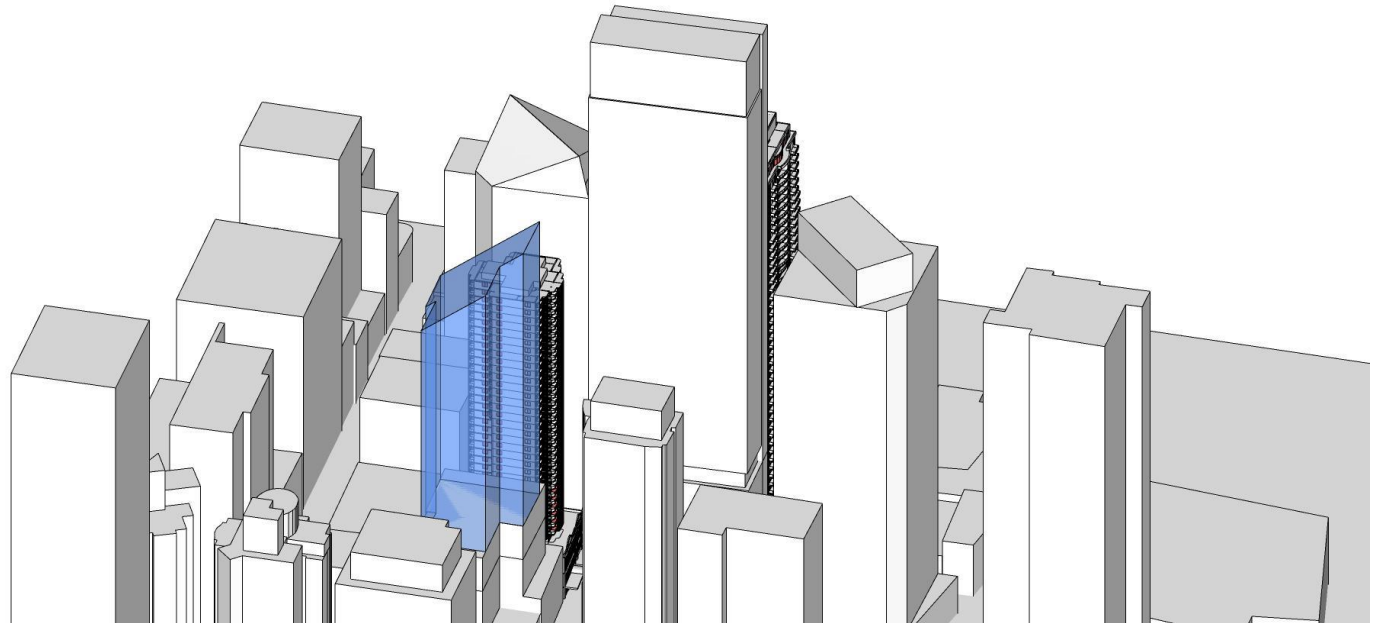
1200
Option
1



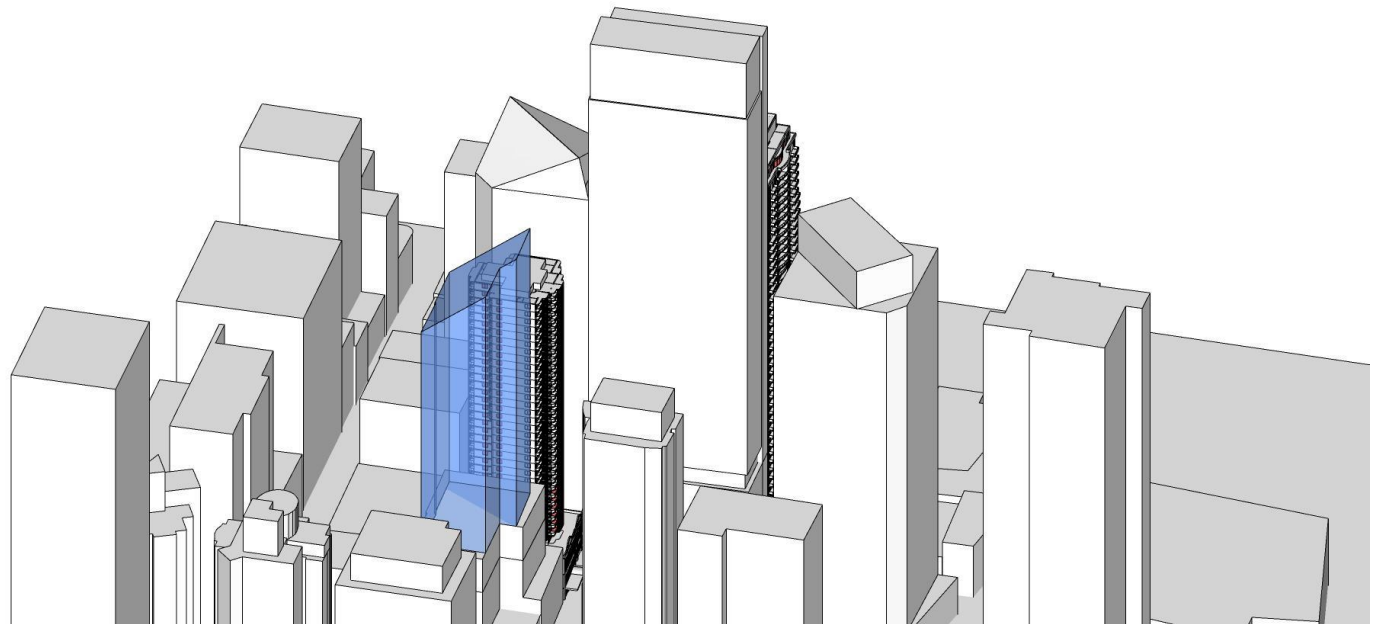
1200
Option
2



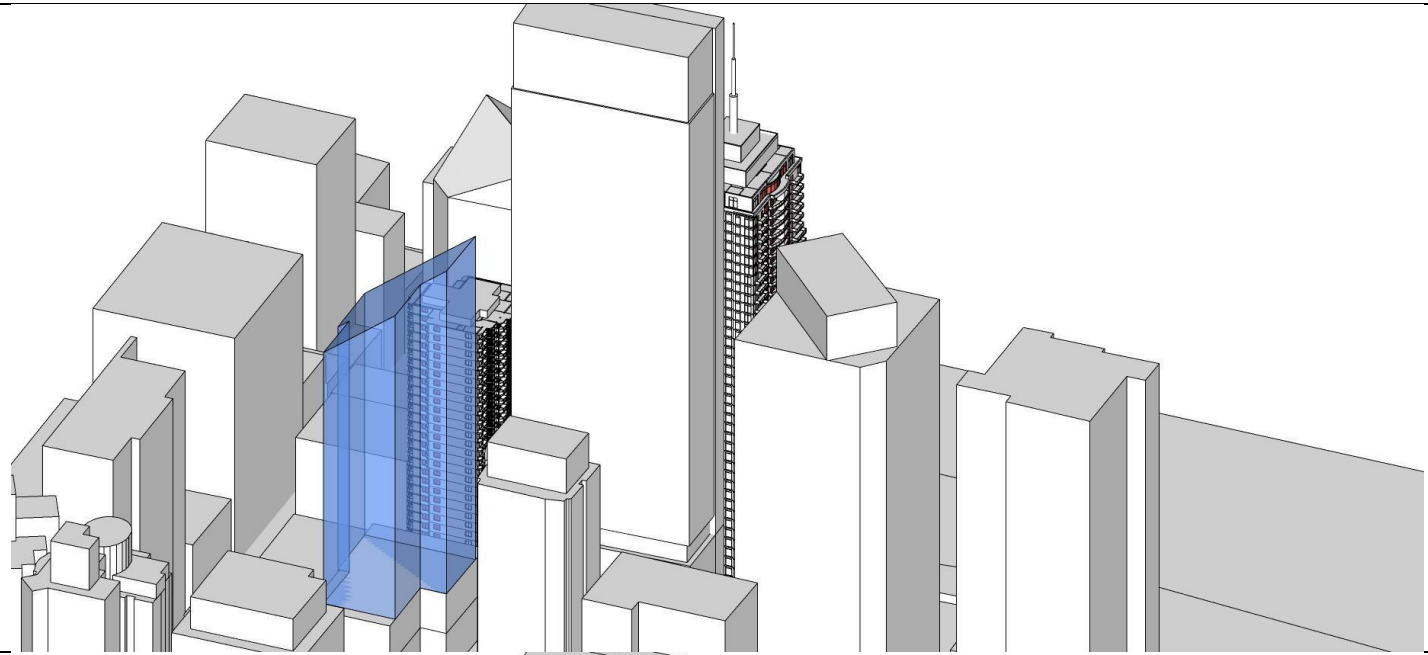
1230
Option
1



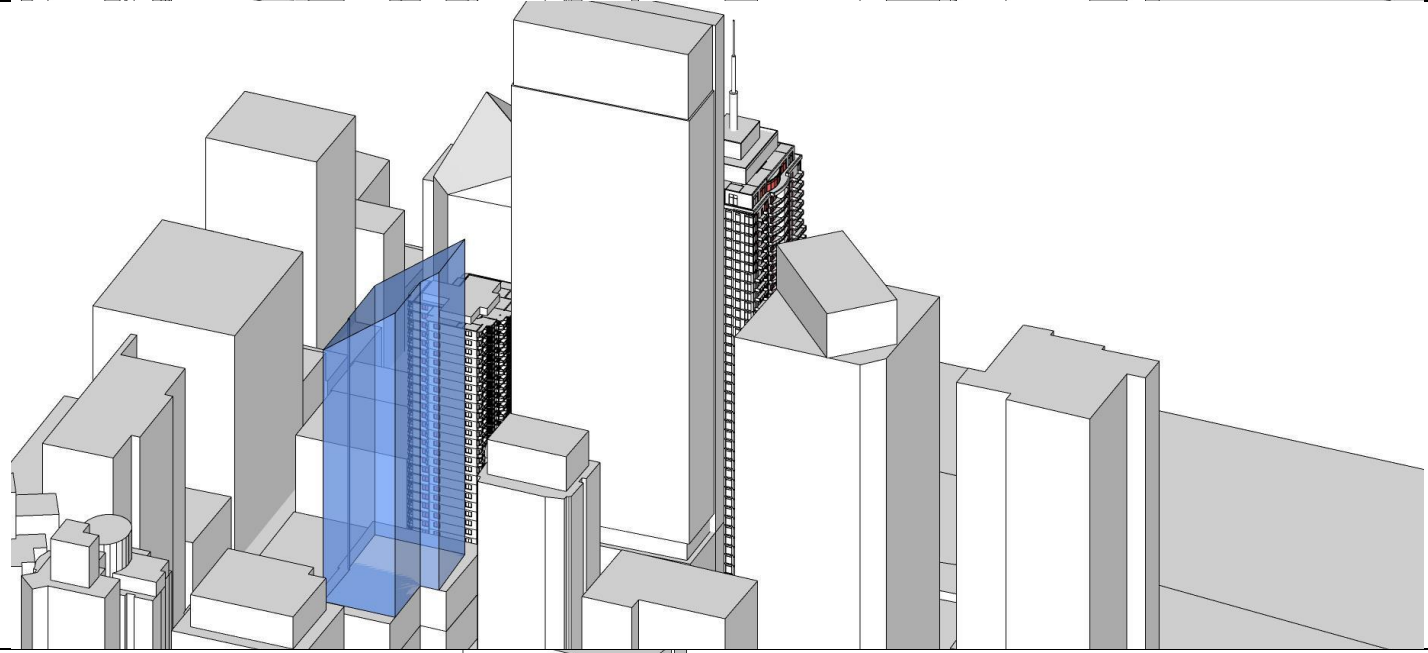
1230
Option
2



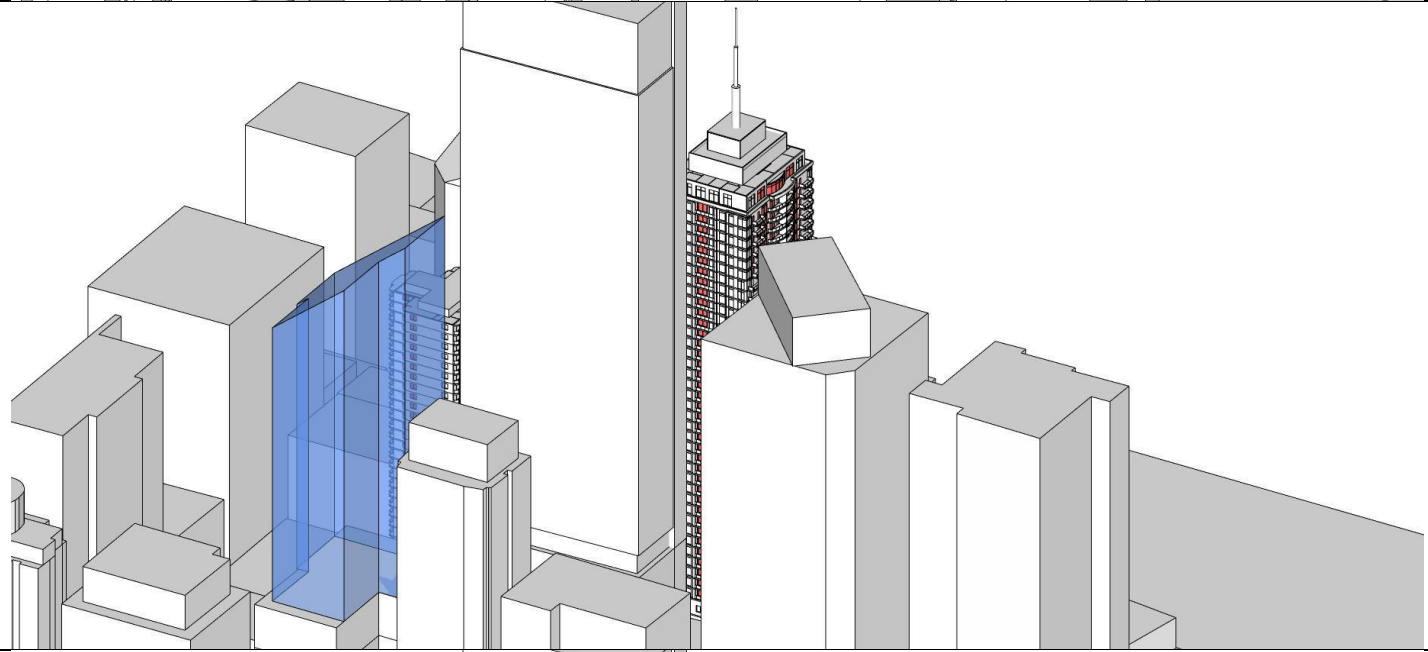
1300
Option
1



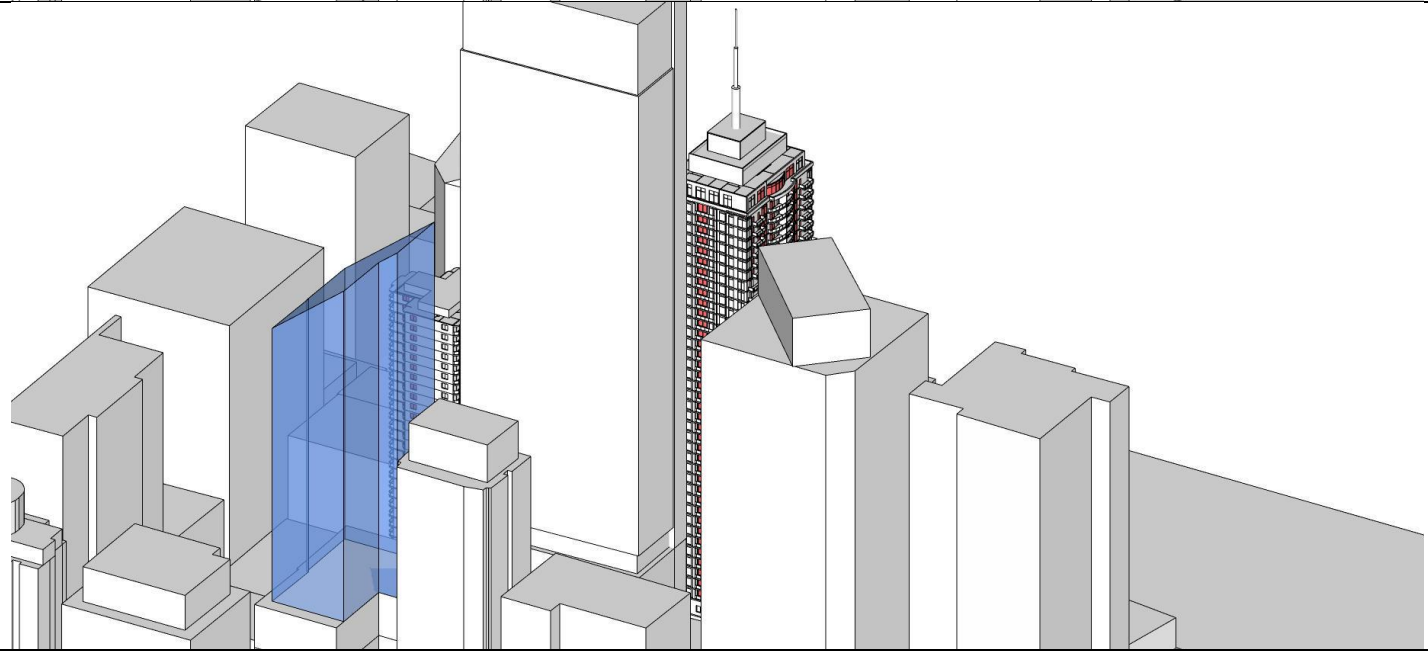
1300
Option
2



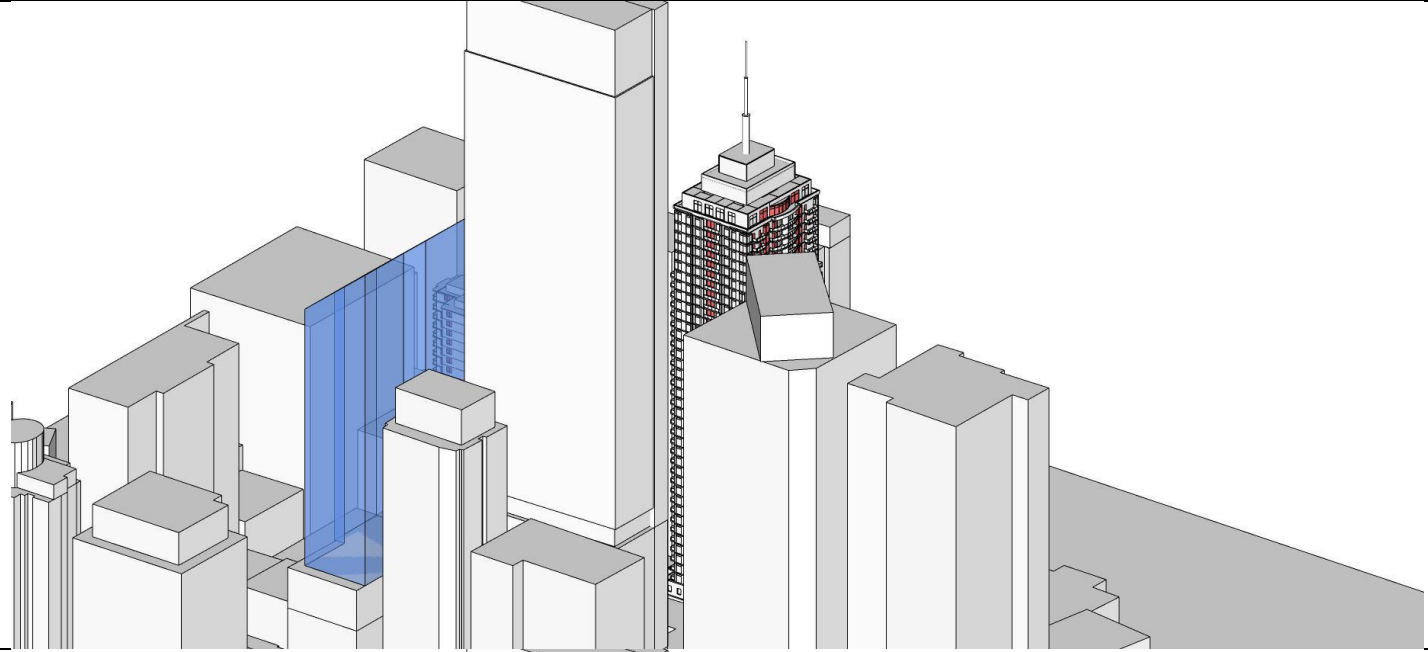
1330
Option
1



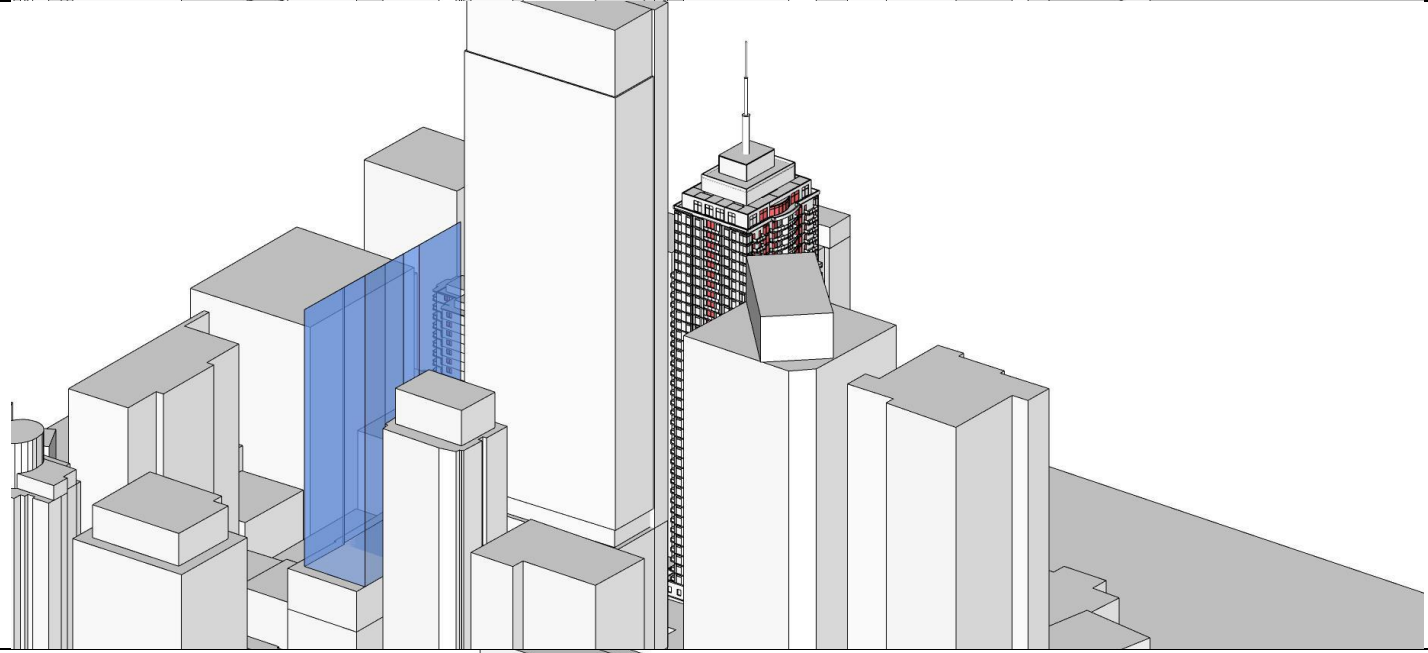
1330
Option
2



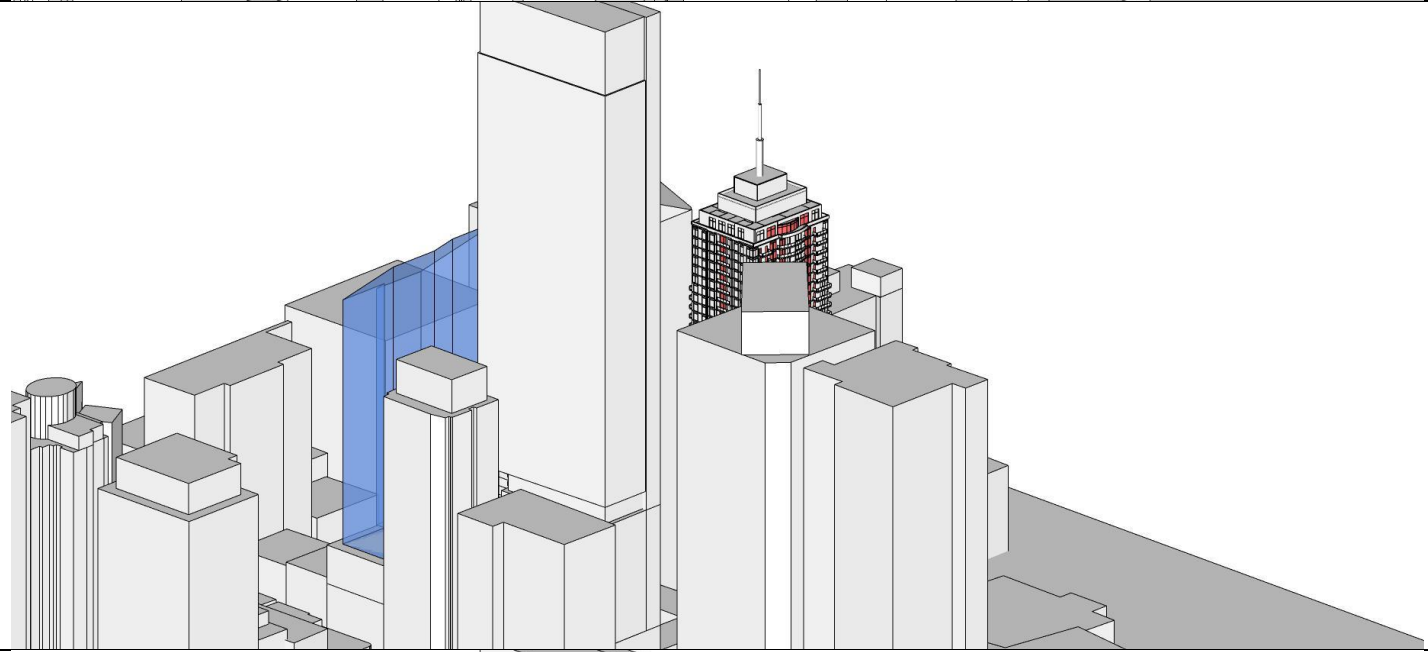
1400
Option
1



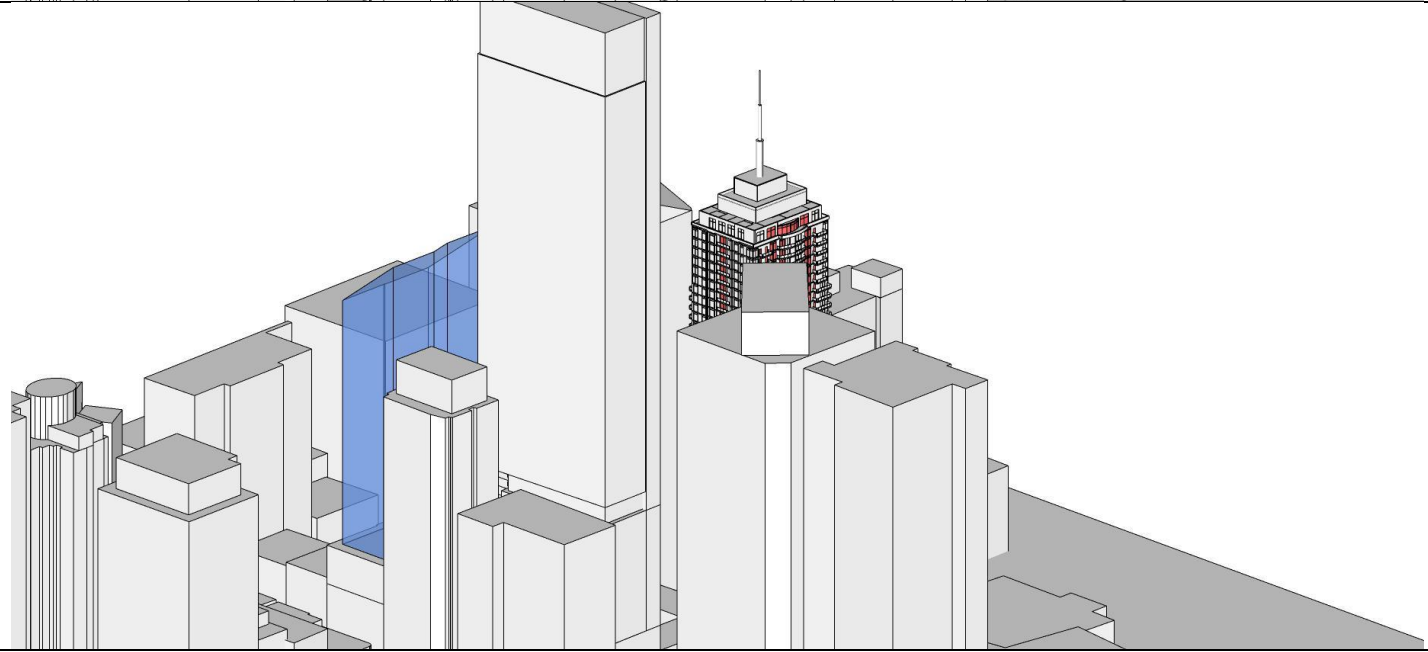
1400
Option
2



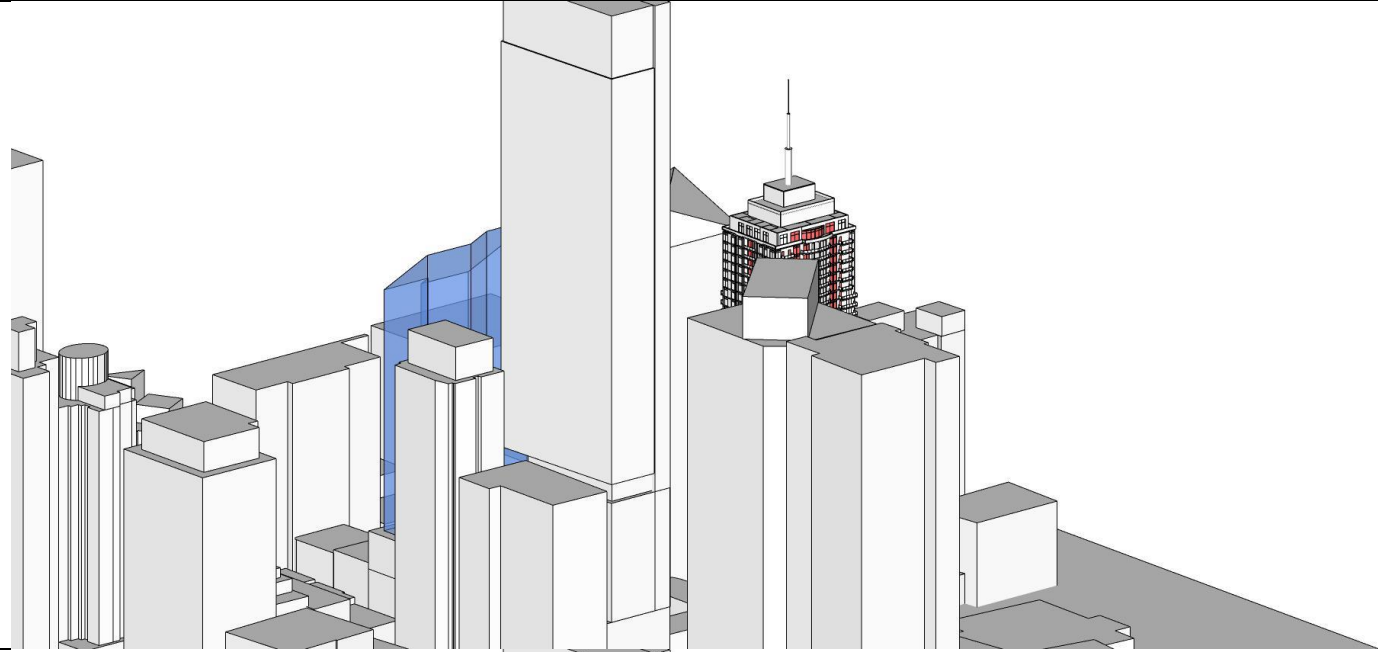
1430
Option
1



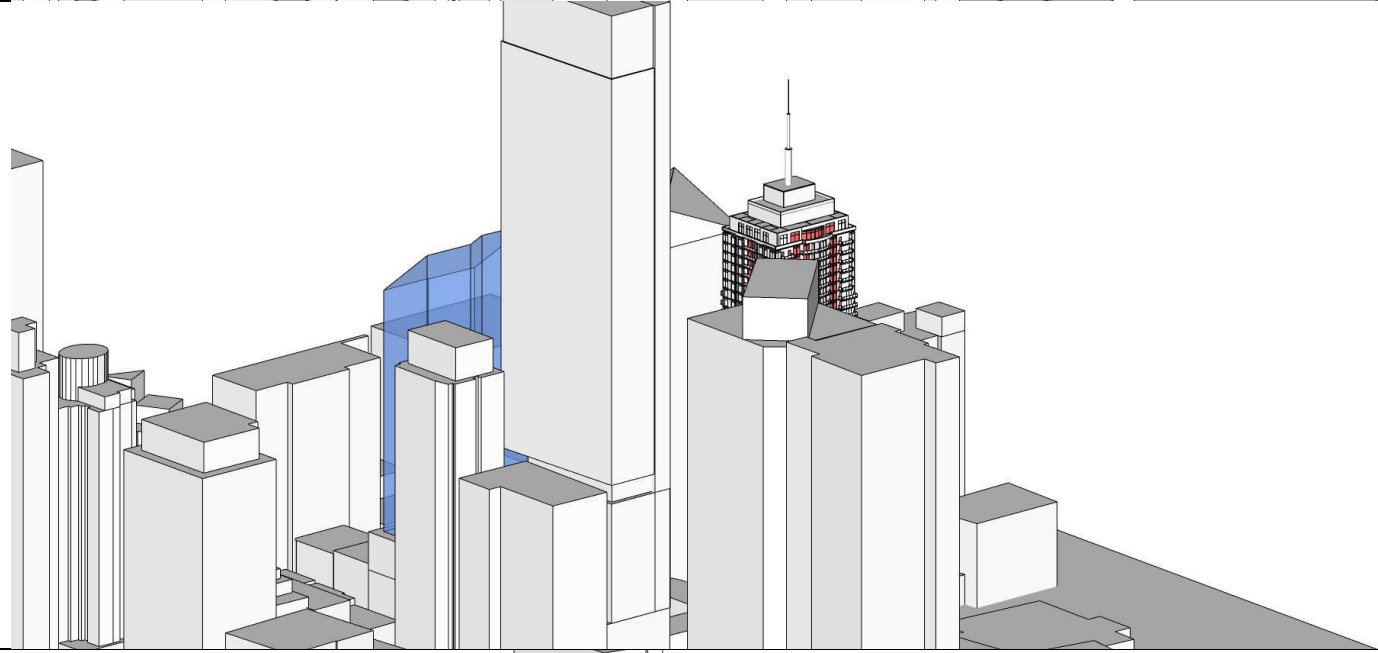
1430
Option
2



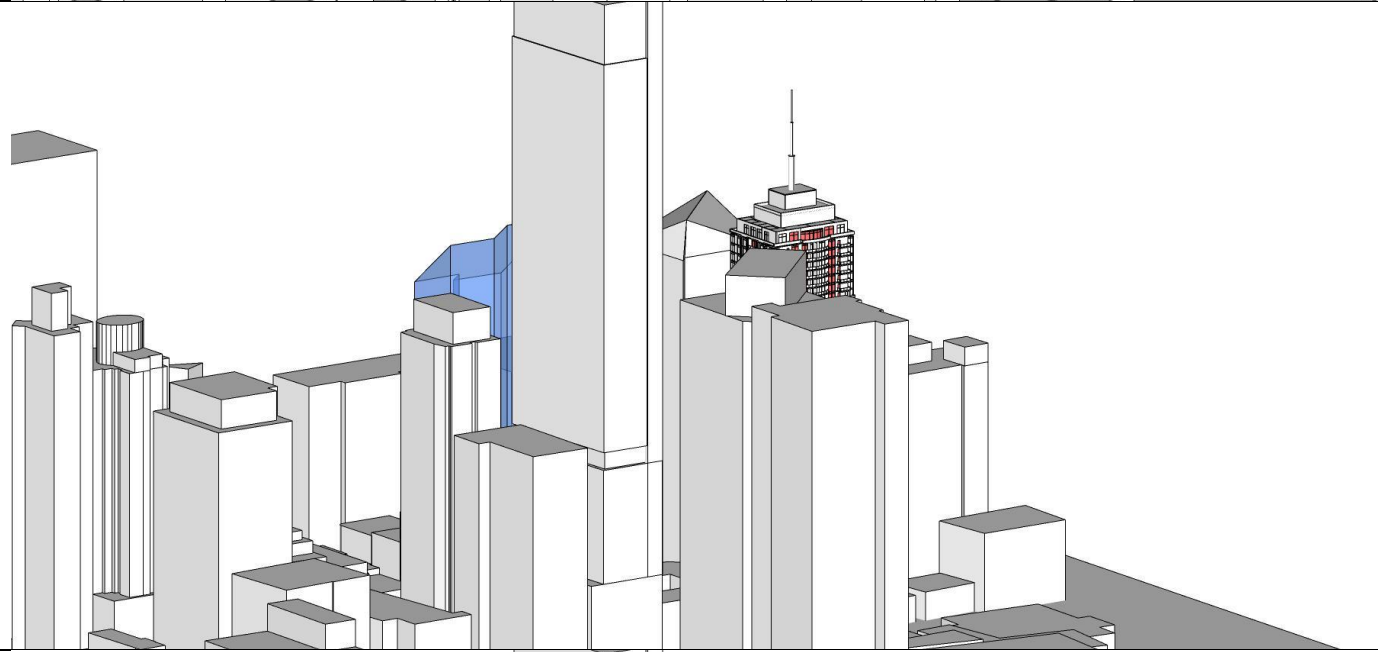
1500
Option
1



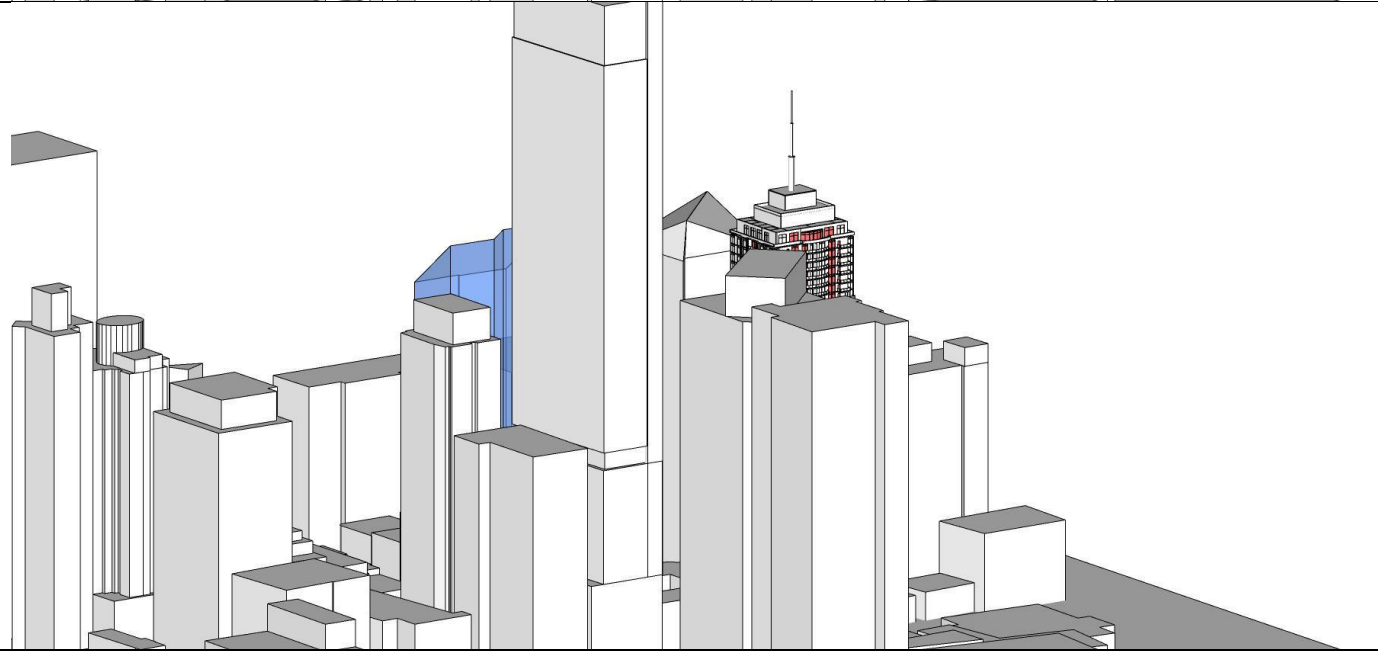
1500
Option
2



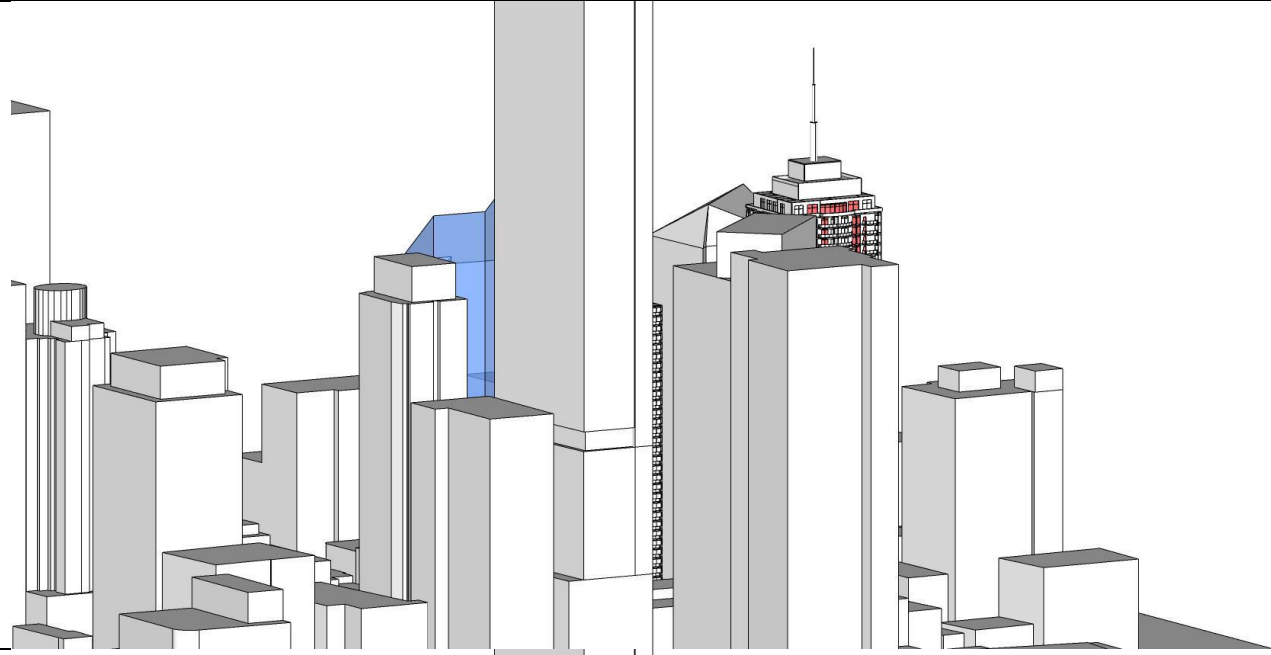
1530
Option
1



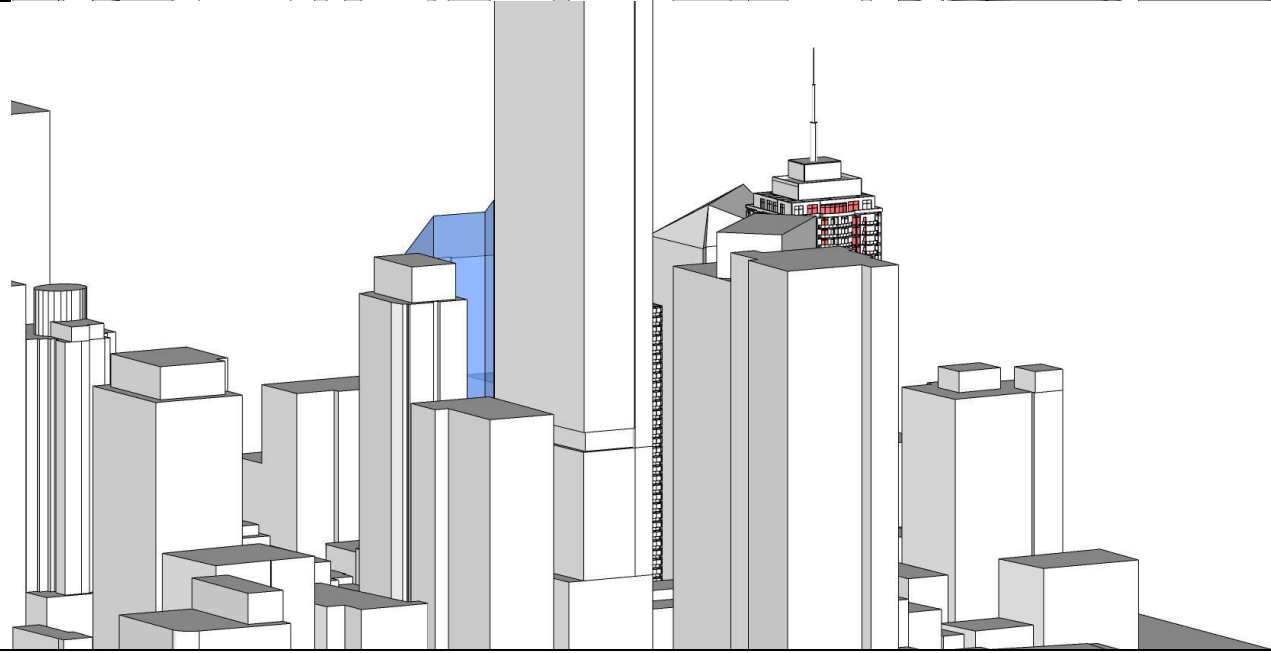
1530
Option
2



1600
Option
1



1600
Option
2



**MEDIA RELEASE ISSUED
PRIOR TO EXHIBITION
OF THE CONCEPT SSD
APPLICATION**

APPENDIX L



NEW OPPORTUNITIES FOR THE SYDNEY CBD AS SYDNEY METRO'S PITT STREET STATION TAKES SHAPE

Planning is progressing for the integrated station development at the new Pitt Street metro station.

The community can now have its say on the proposed buildings above Pitt Street Station with the Environmental Impact Statements for both Pitt Street North and Pitt Street South now lodged with the Department of Planning and Environment.

There are two over station development (OSD) proposals for above Pitt Street Station.

One proposal is for a mixed-use development above the northern entry to Pitt Street station, on Park Street between Pitt and Castlereagh streets.

The other proposal is for a commercial or residential development above the southern entry to Pitt Street station, near the corner of Bathurst and Pitt streets.

The proposed building at the northern entry could include a residential component, a hotel and commercial office space.

At the southern entry, the proposed building will house either residential apartments or commercial office space.

Stations on the new Sydney Metro system will be vibrant places and landmarks in their own right.

This is a chance to build more than just railway stations – through excellence in design and delivery, Sydney Metro will deliver fully-integrated places which are intuitive and safe, architecturally unique and which promote people's health and wellbeing.

The NSW Government has identified stations which can be better integrated with the places around them and will deliver new initiatives similar to other great mass transit-oriented developments around the world, including Hudson Yards in New York and the new Paddington Station on London's Crossrail.

As the new metro stations are built underground, buildings will be able to be constructed above them at the same time, reducing community impacts and the length of time required to deliver developments.

Following a competitive tender process, three groups have been shortlisted to deliver Pitt Street Station and the new buildings above it.

The three groups shortlisted to build Pitt Street station and the over station development are:

- Grocon (Pitt Street) Developments Pty Ltd;
- Lendlease Development Pty Limited;
- Brookfield Properties (Devco) Australia Pty Ltd.

The Environmental Impact Statement is on exhibition from 16 August 2018 to 12 September 2018. Displays and information will be available at the following drop-in community information sessions:

- 28 August, 11am – 2pm, Wesley Conference Centre, 220 Pitt Street, Sydney
- 30 August, 4 – 7 pm, Primus Hotel, 339 Pitt Street, Sydney
- 4 September, 11am – 2pm, Primus Hotel, 339 Pitt Street, Sydney
- 6 September, 4 – 7 pm, Wesley Conference Centre, 220 Pitt Street, Sydney

Details on how to make a submission can be found at:
www.majorprojects.planning.nsw.gov.au (link is external).

**SECTION DEMONSTRATING
PROPOSED LEVELS
IN RELATION TO THE
PRINCETON APARTMENTS**

APPENDIX M



