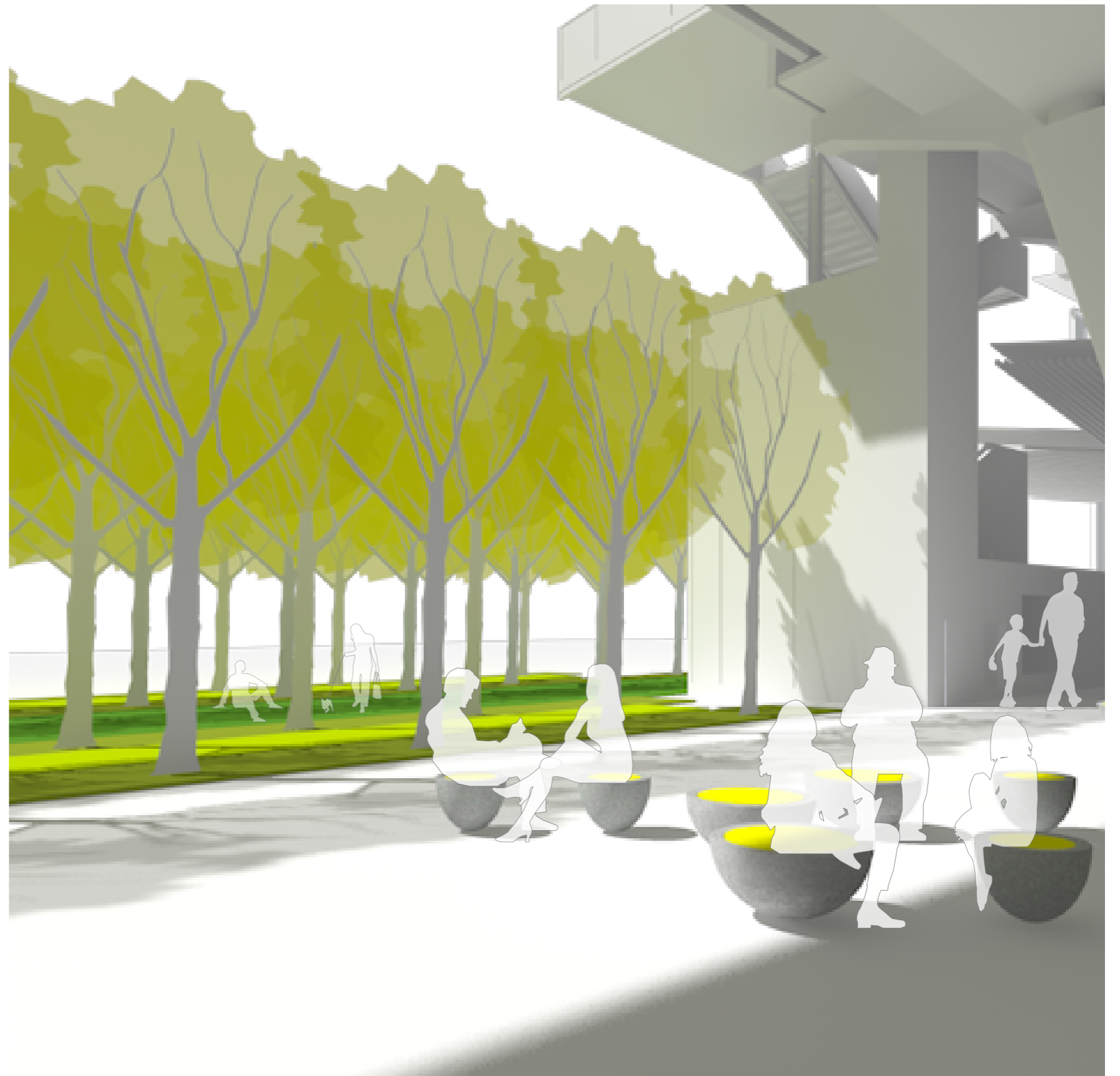


# 03

## Precinct Plans

### Kellyville Station and Precinct



Kellyville Station Social Square Visualisation. Source: MWA with T+C Studio.



### 3.1 Precinct Context

Kellyville Station is within the North West Growth Centre and is in the centre of a new commercial core with strong links to the existing business park to the south and the surrounding residential development. The existing landscape is essentially rural/rural residential in character with open pasture/grassland and a number of large stands of remnant Cumberland Plain Woodland.

The Kellyville Station public domain responds to aspects of the station's immediate context and provides a high quality setting for the station architecture. The Kellyville structure plan and intended future developments inform the precinct and architectural design.

The station's patronage will be generated from the precinct's interchange bus/rail function as well as the existing and future walk up patronage. Multimodal transport requirements are met within the precinct streets and the existing T-way on Old Windsor Road. Commuter parking is located in multi level car parking to the west of the station and at grade car parking to the east

The station is simple in its program and intuitive for the customer. Entries are provided from precinct streets and the shared path to a well organised concourse and even distribution of passengers to platform level.

The Kellyville precinct and station will deliver:

- A network of socially connected civic spaces
- Seamless interchange from cycle, bus, taxi, and kiss and ride
- Socially active secondary and primary station plazas
- Visible, comfortable, protected and safe station entrances
- High quality proactive customer service within a modern, uplifting station environment
- An intuitive journey to the train.

#### 3.1.1 Purpose and Scope

This section of the Sydney Metro Northwest Urban Design and Landscape Corridor Plan (UDCLP) provides an overall description of the built elements, their context and the design drivers for the precinct at Kellyville Station. This section should be read in conjunction with other sections of the UDCLP to gain an appreciation of the strategic context, design vision and system wide componentry of the project.

This section establishes the following for Kellyville Station:

- Sets out the project context and vision
- Describes the local context
- Describes the urban design, landscaping and architectural design approach
- Describes and details the proposed buildings within the precinct site
- Outlines the key relevant issues
- Describes all key inputs and outputs of the design.

The proposed station facilities include:

- Precinct Streets A, B, C, D & F
- On grade and multi level commuter parking spaces
- One primary plaza
- One station entry
- One covered paid concourse
- Two platforms
- Single level servicing buildings
- Pedestrian footbridge
- Transport interchange facilities
- Precinct soft and hard landscaping.

**3.1.2 Overview of Precinct Project**

Kellyville Station is located between Rouse Hill Station and Bella Vista Station. The platform is orientated on a north south axis with Old Windsor Road to the west, Precinct Street A to the east, Samantha Riley Drive to the north, and Precinct Street F to the south. The station footprint is constrained between the T-way and Precinct Street A. Secondary plazas continue to the north and south beyond the station and under the viaduct.

Other components of the station include a northern at grade car park, southern multi level carpark and pedestrian foot bridge.

Refer Figure 3.1\_Kellyville Station Precinct Plan and Figure 3.2\_Aerial View of Kellyville Station.

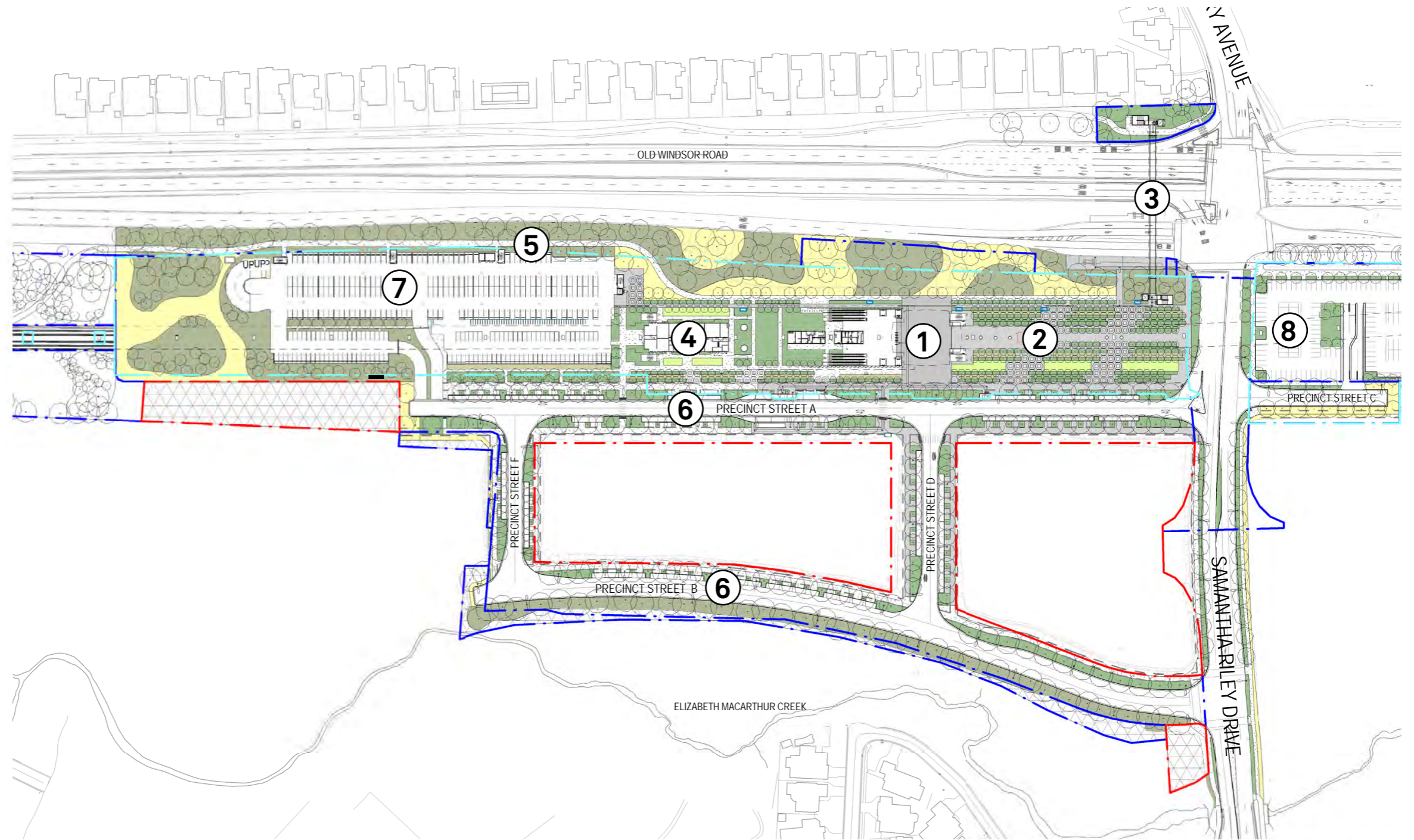
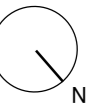


Figure 3.1 KVE\_Plan View Kellyville Station. Source: HASSELL.



The following are the key components of Kellyville Station:

1. Primary Plaza and Station Entry
2. Secondary Plaza
3. Pedestrian Bridge
4. Service Buildings
5. Shared User Path
6. Precinct Streets
7. Southern Car Park
8. Northern Car Park

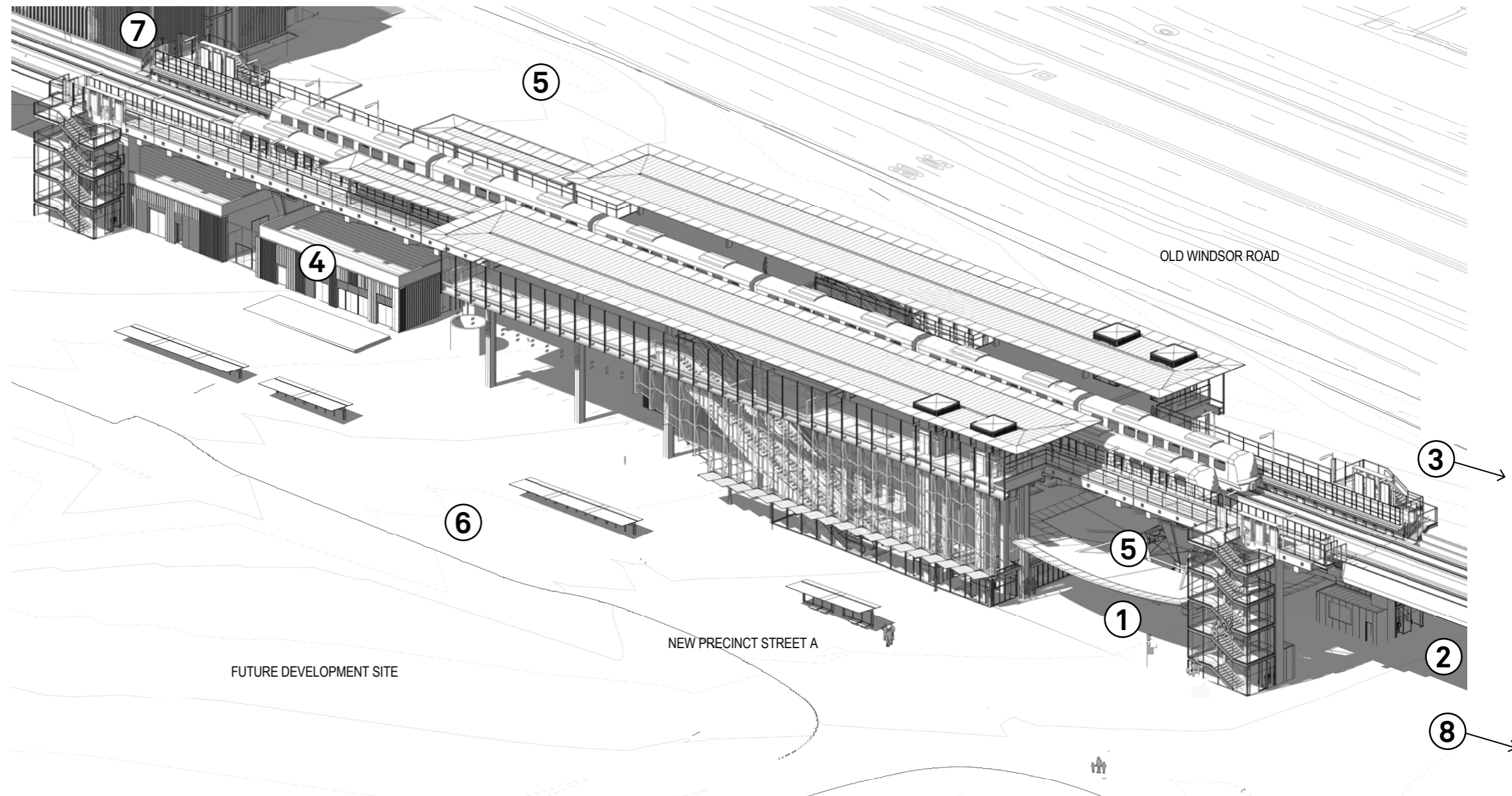


Figure 3.2 KVE\_Aerial View of Kellyville Station. Source: HASSELL.

### 3.1.3 Location

#### Regional Location

Kellyville Station is in the Hills Shire Council. It is located between Bella Vista and Rouse Hill Stations at the corner of Old Windsor Road and Samantha Riley Drive, east of the Riley T-way Station and car park.

Kellyville is 34 km north west of Sydney CBD.

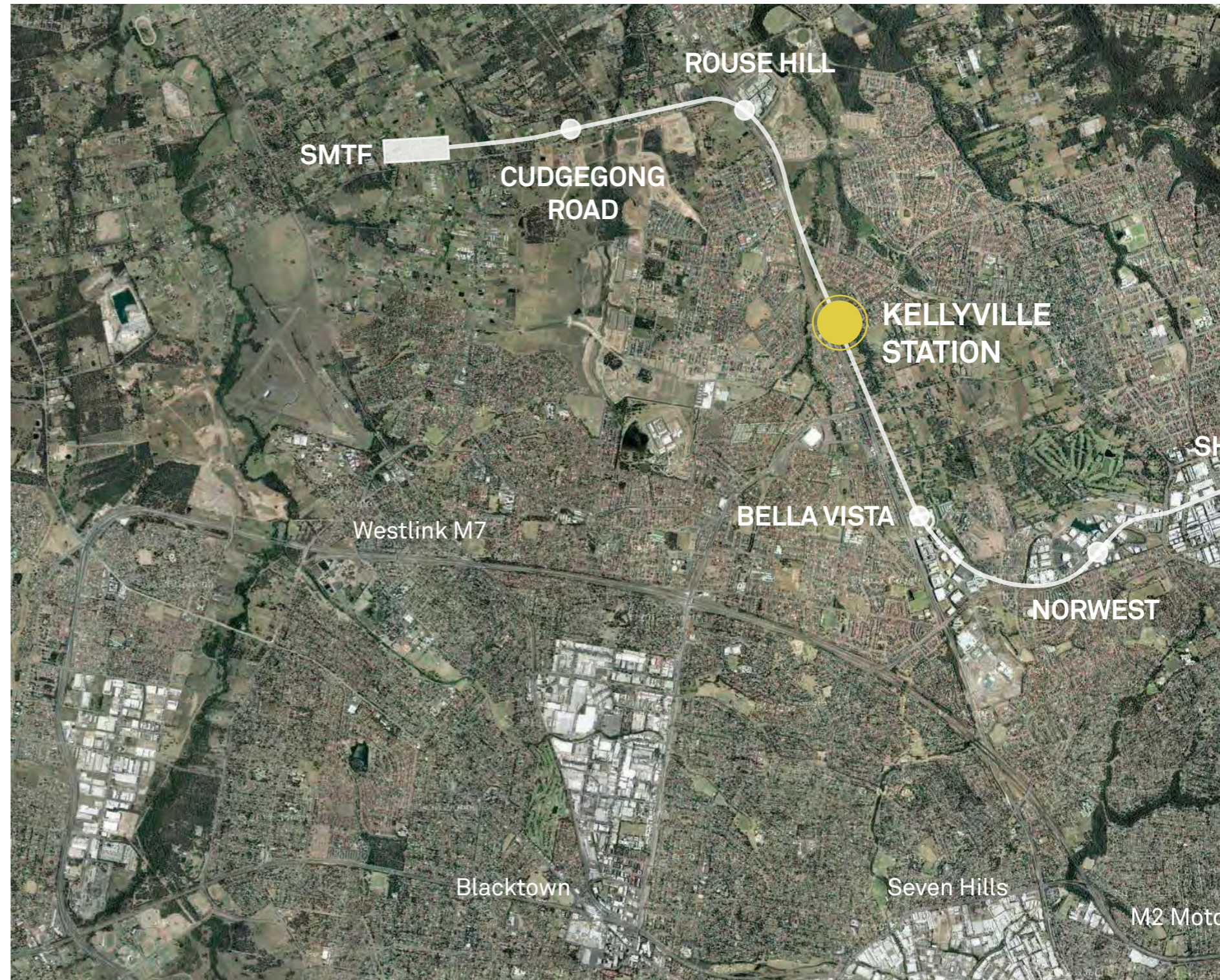
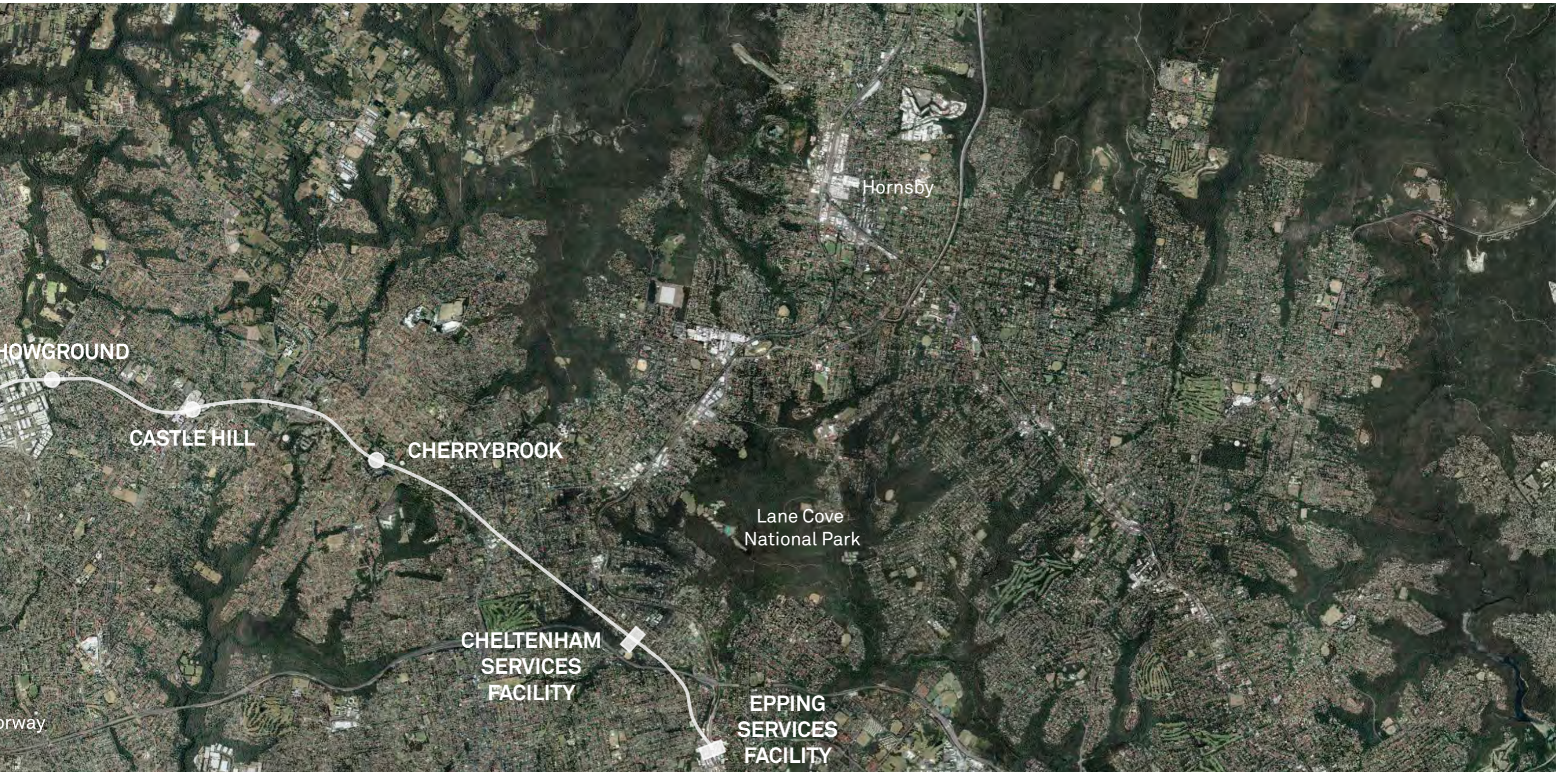


Figure 3.3 KVE\_Location of Kellyville Station. Source: Google Maps.



**Local Context**

Figure 3.4 is taken from the Kellyville Structure Plan in the North West Rail Link Corridor Strategy (NSW Planning 2013) and illustrates the Kellyville Station precinct's location within the structure plan study area and existing surrounding land uses. The legend outlines the key existing local places of note.

Kellyville Station will work together with the adjacent T-way to service the public transport needs of this growing residential area.



Figure 3.4 KVE\_Kellyville Station Aerial. Source: Planning NSW 2013.



### 3.1.4 Statutory Context

The Structure Plan prepared by the NSW Department of Planning and Environment proposes a new local centre adjacent to the station to provide for the growing retail needs of the existing and future residents of the Kellyville precinct

Suitable locations for high density residential, of between 7 to 12 storeys, have been identified within close proximity to the station and along open space corridors where there is greater amenity and direct access to the buses, rail transport interchange and local services. Medium density living, comprising of 3-6 storey apartments, will be located within an easy 10 minute walk of the station or major bus route.

New links are proposed in locations where they will increase connectivity and permeability. These links could be either pedestrian or vehicular connections. Drawing on existing significant vegetation, existing parks and riparian corridors, green links are proposed along Elizabeth Macarthur and Strangers Creeks. These will become significant pedestrian and cycle links between Kellyville and Rouse Hill, Bella Vista and Norwest

Refer Figure 3.5\_ Kellyville Structure Plan.

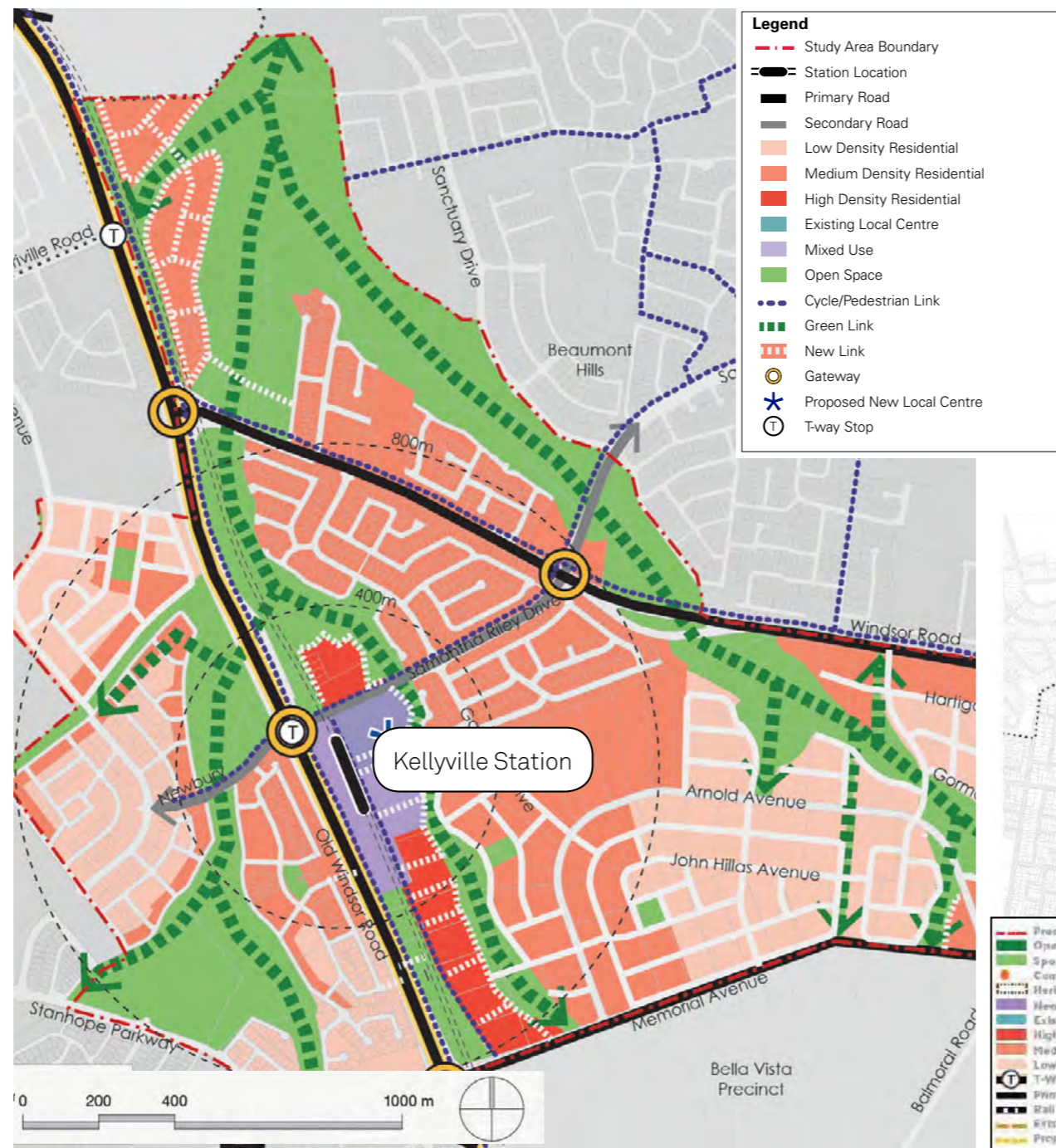


Figure 3.5 KVE\_Kellyville Structure Plan. Source: DP & E 2015.

### Kellyville Precinct Plan

Figure 3.6 outlines the priority precinct proposal for Kellyville prepared by DP&E in December 2015 to illustrate the potential future development within the station's environs. While the scheme is indicative, the proposals contained within it were used to formulate design proposals for the station interfaces, so that options for future development were optimised.



Figure 3.6 KVE\_Kellyville Indicative Precinct Layout December 2015. Source: DP&E priority Precinct Proposal December 2015.

**3.1.5 Precinct Access**

The Station Access Plans describe traffic requirements and related pedestrian movement for the precinct. These have informed the precinct kerbside provisions for bus, taxi, kiss and ride and on-street parking.

The Sydney Metro Northwest Pedestrian-Cycle Network and Facilities Strategy contains recommendations for district cycle and pedestrian access requirements for the precinct. Kellyville Station precinct integrates off road cycle access routes to and from the station. Locations for provision of bicycle parking are consistent with these recommendations.

**3.1.6 Parking, Pedestrian, Cyclist Access Arrangements and Facilities**

The parking, pedestrian and cyclist access arrangements and facilities are consistent with the requirements of the station access plans and pedestrian cyclist strategy. They are illustrated in summary form in Figure 3.7.

- Bus stands are located on Precinct Street A to the south of the gateline.
- Taxi ranks are located on Precinct Street A close to the pedestrian crossing point near the Precinct Street D intersection.
- Kiss and Ride bays are located on Precinct Street A.

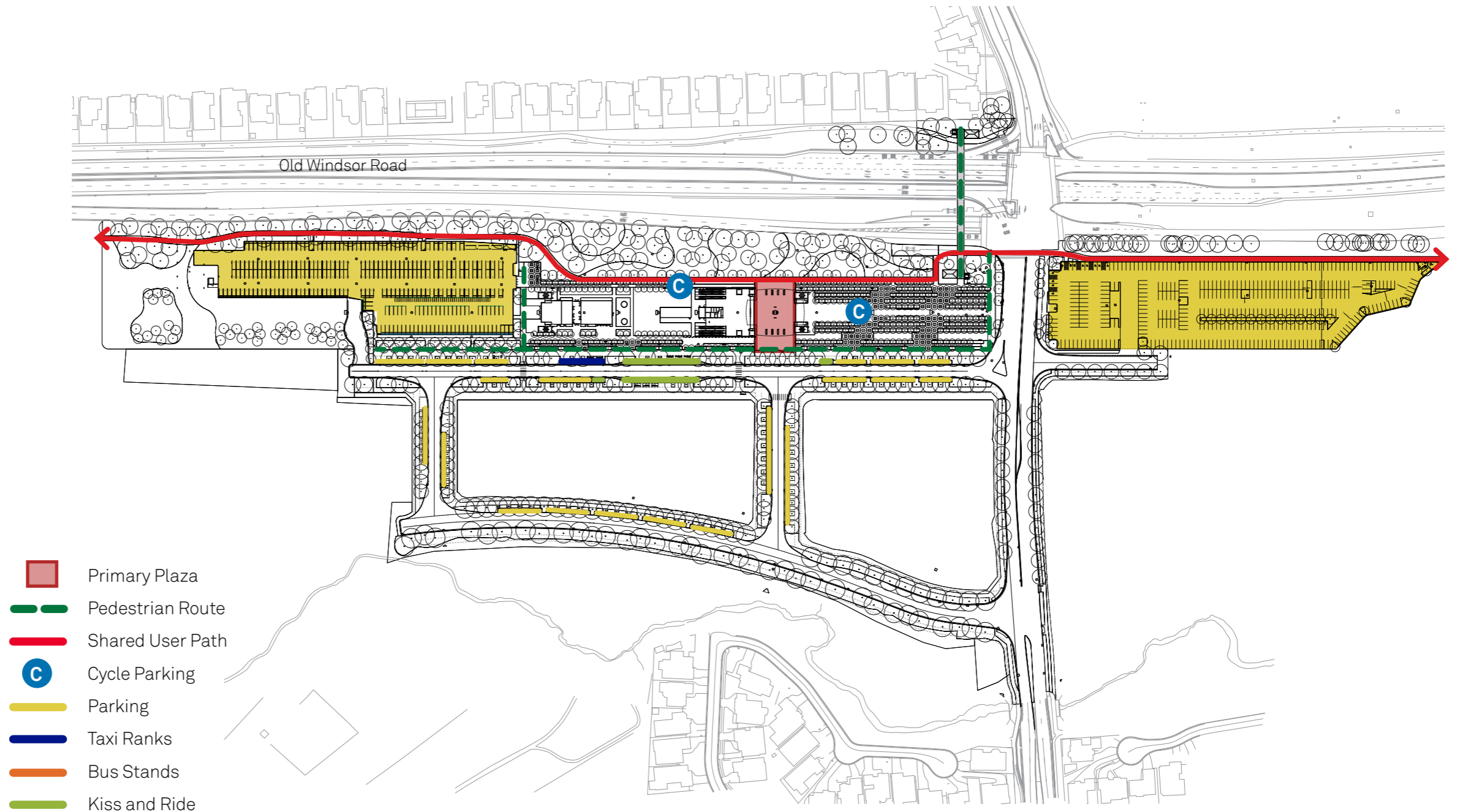


Figure 3.7 KVE\_Kellyville Station Plan showing Parking, Pedestrian, Cyclist Access Arrangements and Facilities. Source: HASSELL.



### 3.1.7 Precinct Planning and Design Issues

The key issues identified at Kellyville Station are summarised below.

- **Public Domain**\_character and function of the public space immediately around the station defined
- **Activity**\_levels of activity around the primary plaza and secondary areas of public domain
- **Walk to Car Parks**\_public domain needs to accommodate for the safety and amenity of primary pedestrian routes from car parking
- **Multi Level Car Park**\_massing and character of the building in relation to the viaduct
- **Precinct Street D**\_character and width of the street
- **Community Use**\_character and extent of community facility and function
- **Landscape Edge**\_width and nature of the landscape strip between the station and Old Windsor Road

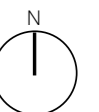
### 3.1.8 Precinct Design Principles

The key precinct principles developed for Kellyville Station are summarised below:

- Primary plaza centred on Precinct Street D
- Local centre focused along Precinct Street D
- Active station edge to Precinct Street A
- Landscape edge to Old Windsor Road



Figure 3.8 KVE\_Kellyville Precinct Principles Plan. Source: HASSELL.



**Station Type**  
Elevated

**Height Above Concourse**  
13 m

**Centre Type**  
Major neighborhood  
centre

**Catchment**  
Residential

**Local Government Area**  
Hills Shire Council

### 3.1.9 Key Design Drivers

The key design drivers informing Kellyville Station precinct are summarised below:

- Generous and convenient public domain for seamless interchange
- Precinct designed to act as a catalyst for a community hub including opportunity for an integrated social and art program such as a social square
- Station that is instantly recognisable to form the heart of the Kellyville precinct
- Safe, reliable, convenient and comfortable environment to attract customers to the Sydney Metro Northwest as first choice over other forms of transport
- Highly visible precinct where views throughout are enhanced for safety, community connection and engagement
- Station layout that is intuitive to navigate with minimal travel distances
- Concept that is attuned to the Kellyville environment in the context of the Cumberland Plain Woodland

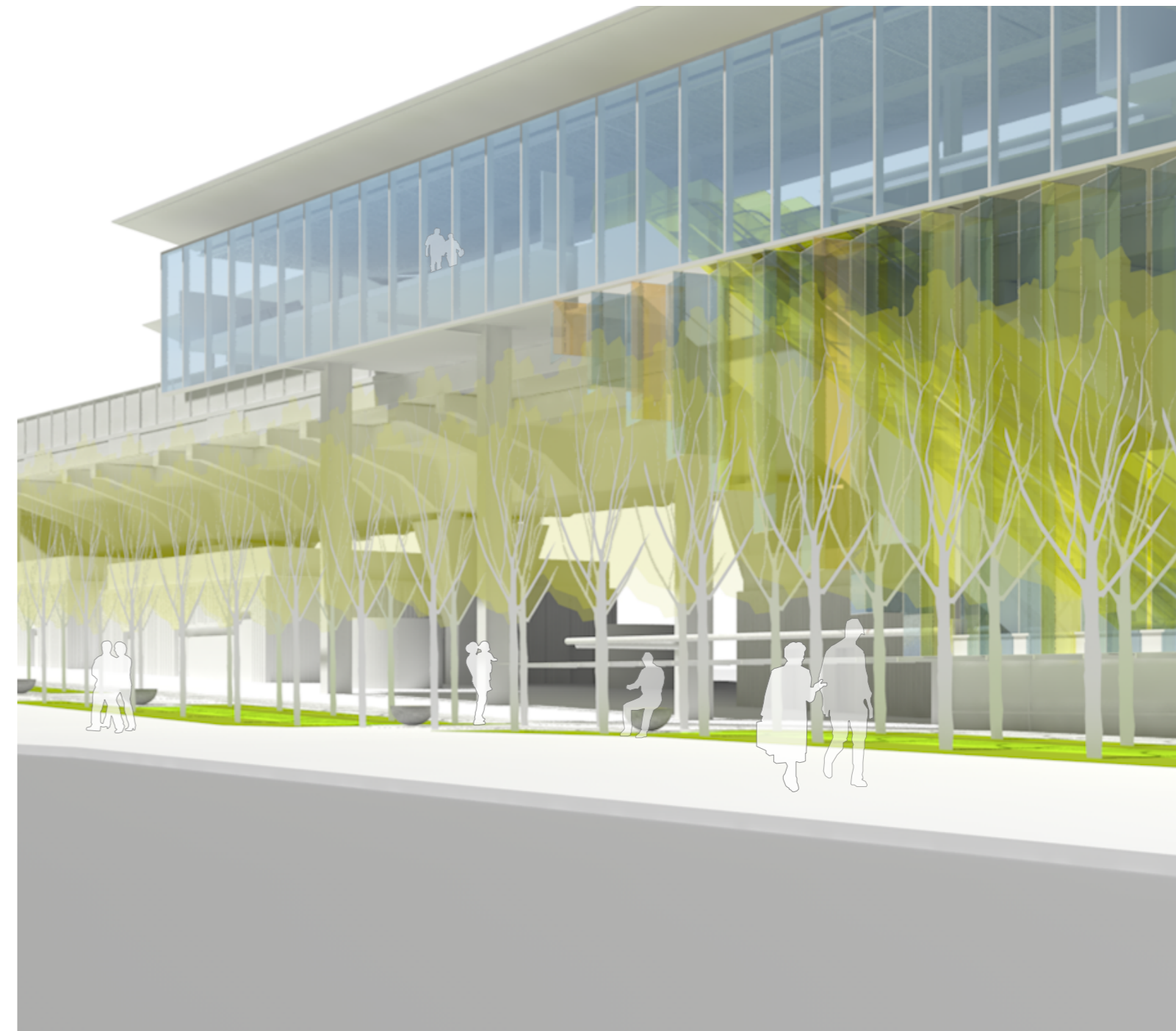
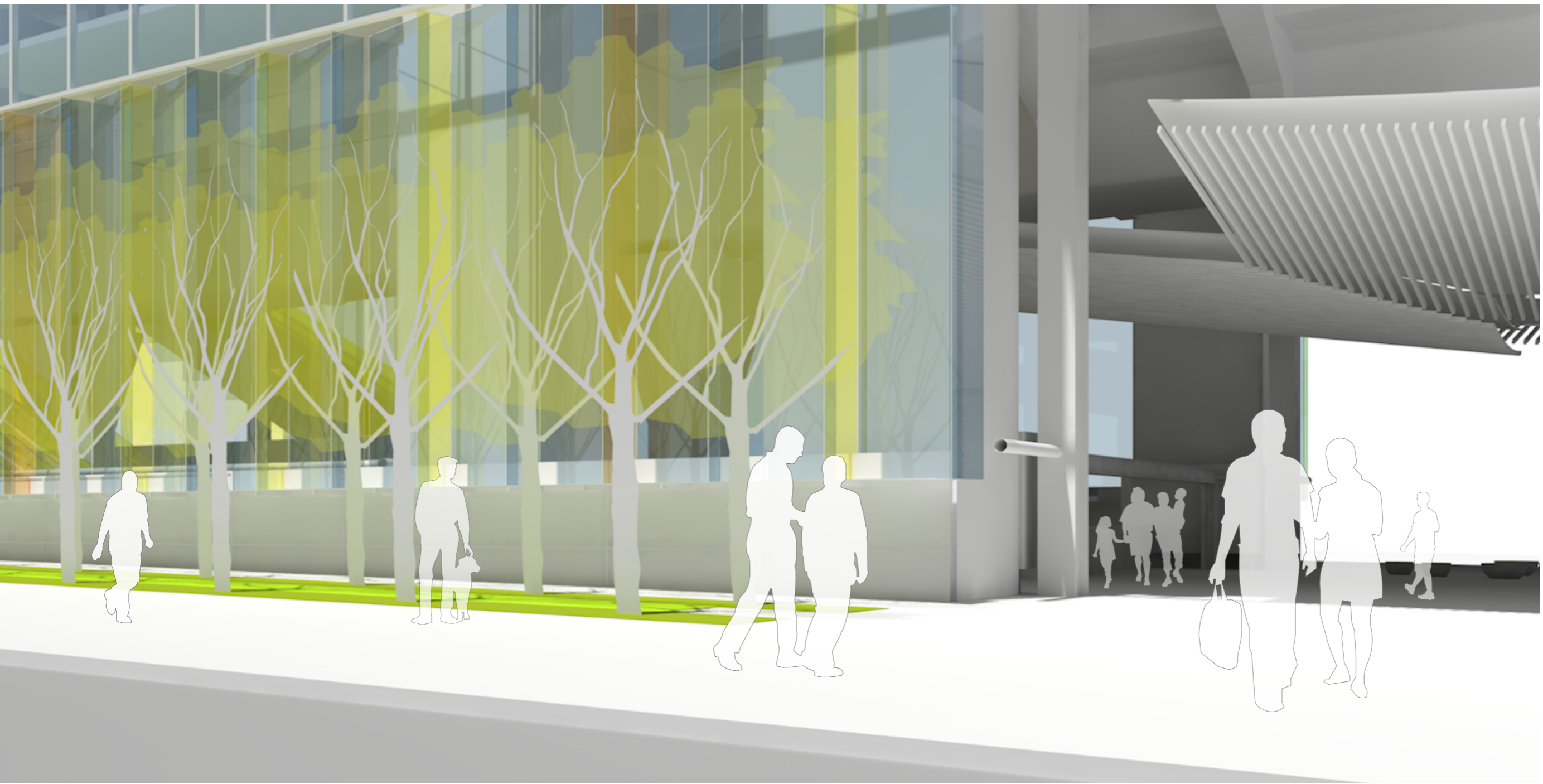


Figure 3.9 KVE\_Kellyville Station Visualisation. Source: MWA with T+C Studio.



### 3.2 Urban Design and Landscape Plan

This section contains descriptions of the proposals for the urban and landscape design of Kellyville station and its immediate surrounds in the context of the wider precinct. Plans have been prepared in accordance with the strategies documented in Section 2 of this UDCLP using the componentry documented in Section 4 of this UDCLP.

#### 3.2.1 Site Interactions

Interactions between the Kellyville station and its immediate surrounds that have informed the design are summarised in the adjacent diagrams. Refer Figures 3.10 to 3.12.

##### 1. Encapsulate Station in Woodland Setting

Opportunity to provide a station located within a Cumberland Plain Woodland setting that enhances the original character of Kellyville.

##### 2. Permeable Station Public Domain

Opportunity to create connections between the shared cycle path and Precinct Street A plus a new bridge connection over Old Windsor Road.

##### 3. Future Developments

Opportunity for the precinct to respond to the future developments surrounding the station.

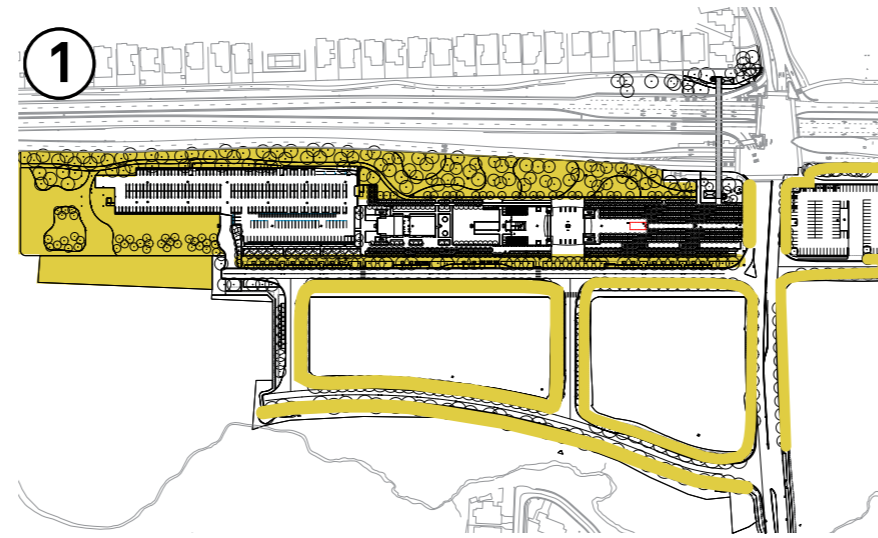


Figure 3.10 KVE\_Cumberland Plain Woodland Setting. Source: HASSELL.

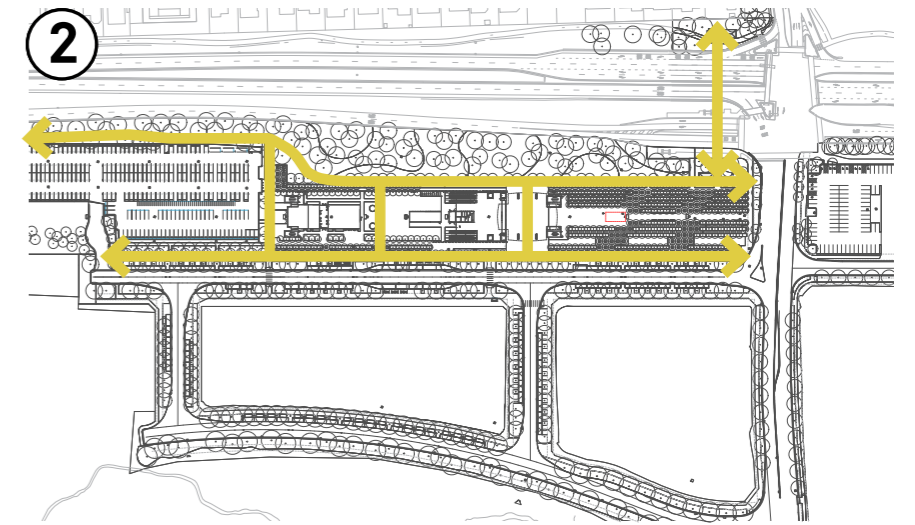


Figure 3.11 KVE\_Permeable Station Public Domain. Source: HASSELL.

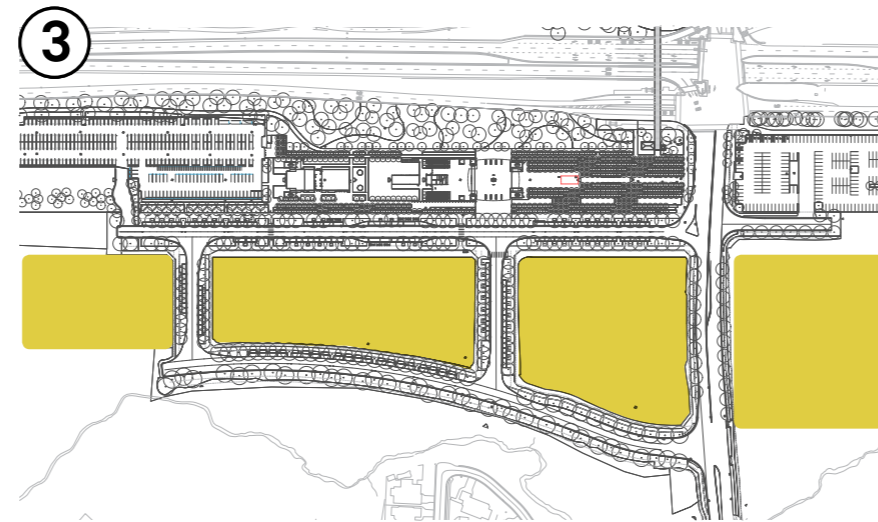


Figure 3.12 KVE\_Future Developments. Source: HASSELL.



### 3.2.2 Design Opportunities

The design opportunities informing the design of the Kellyville station and its immediate surrounds that have informed the precinct are summarised in the adjacent diagrams. Refer Figures 3.13 to 3.15.

#### 1. Station Identity

Opportunity to create an easily identifiable station through the design of the Station Canopy and the Public Art. Key views down streets, around the station and to the station canopy are maximised.

#### 2. Shady Public Domain

Opportunity to create shady public spaces for pedestrians by maximising tree planting under and adjacent to the sky train viaduct.

#### 3. Activation and Connections

Opportunity to provide a setting for a variety of active and passive activities with two distinct but interconnected public spaces. The use of the public spaces is discussed in further detail in Section 3.2.3.

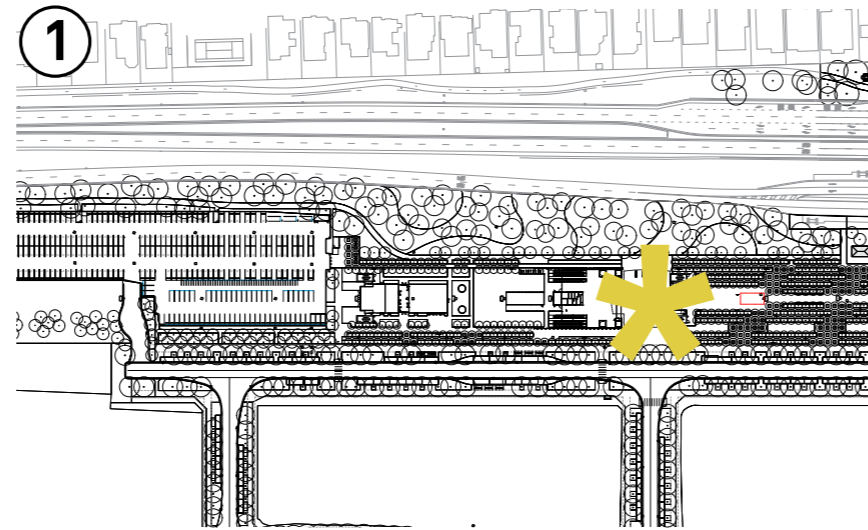


Figure 3.13 KVE\_Station Identity. Source: HASSELL.

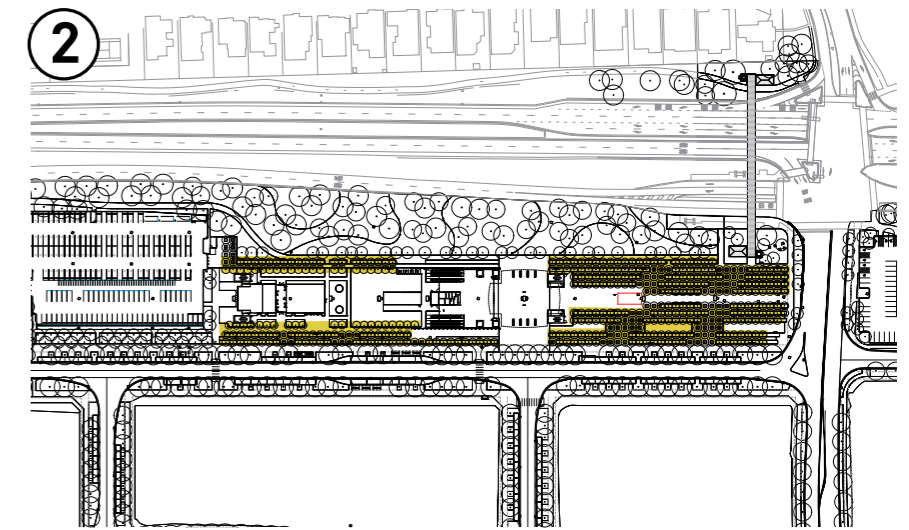


Figure 3.14 KVE\_Shady Public Domain. Source: HASSELL.

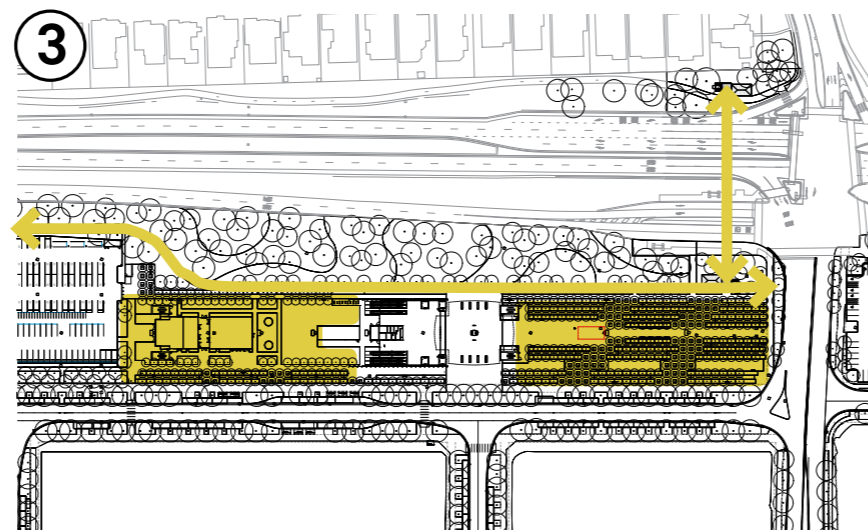


Figure 3.15 KVE\_Activation and Connections. Source: HASSELL.



**3.2.3 Landscape Site Plan and Precinct Elements**

The main precinct landscape elements at Kellyville Station are located on the following plan and discussed in further detail below. Refer Figure 3.16\_Kellyville Station Landscape Elements Location Plan.

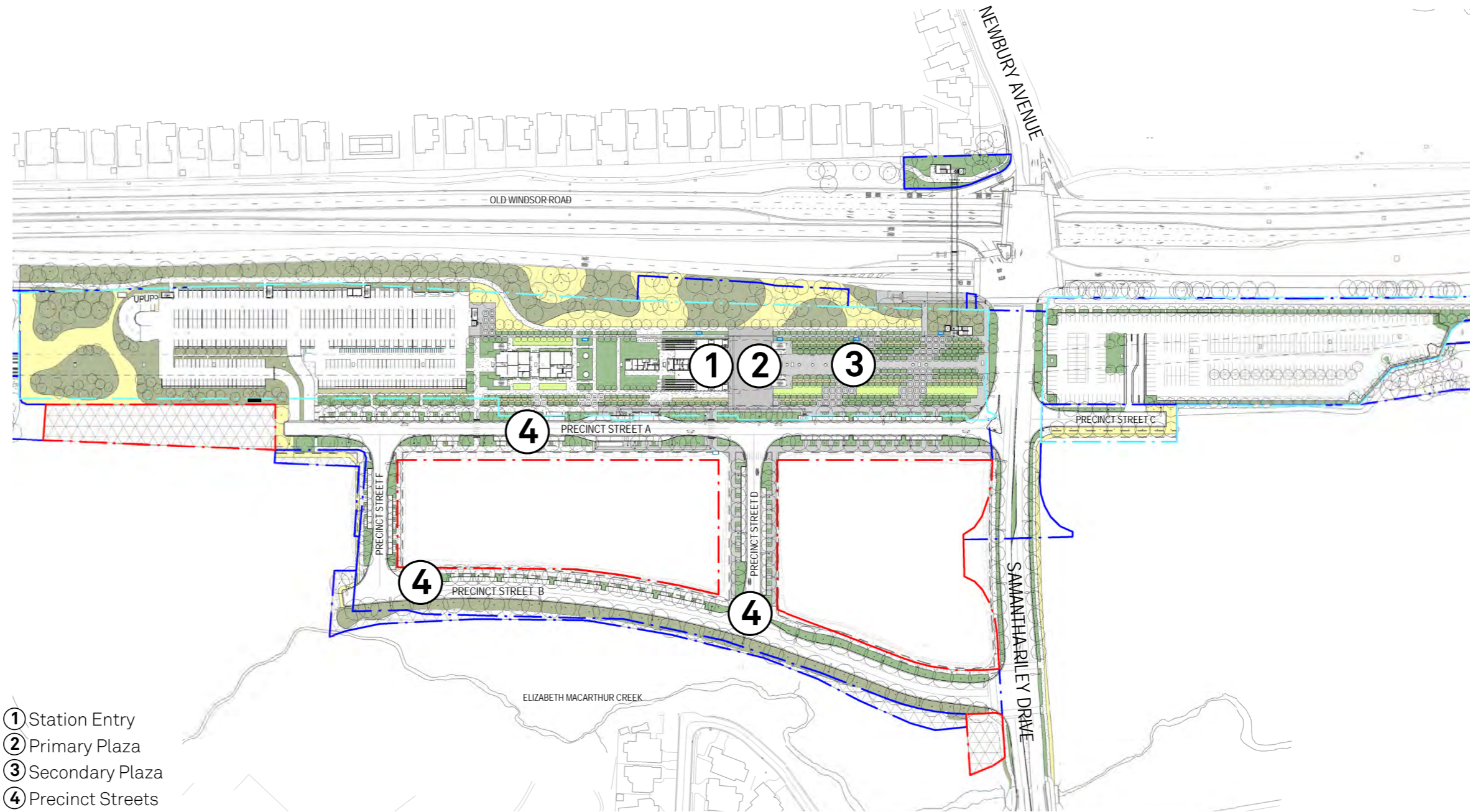
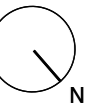


Figure 3.16 KVE\_Kellyville Station Landscape Elements Location Plan. Source: HASSELL.





**Plaza Spaces**

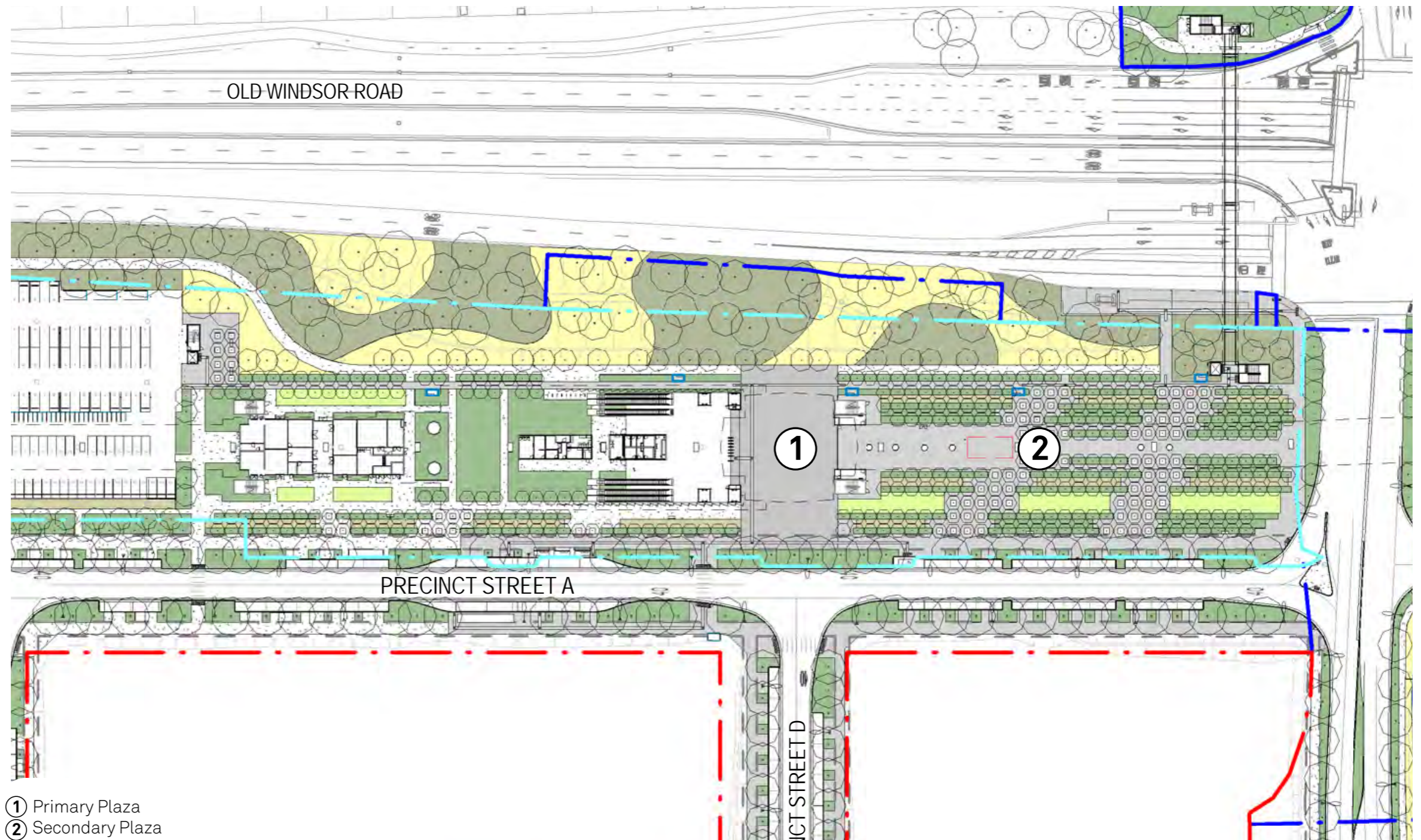
*Primary Plaza*

The primary plaza space is located to the northwest of the station entry point. It is a generous space that allows for ease of pedestrian movement while providing opportunities for gathering.

*Secondary Plaza*

The areas under the viaduct are programmed spaces that incorporate public art. The areas are graded gently to create ease of access and transition between each space. These spaces are surrounded by a grove of trees which are clear stemmed to allow for pedestrian access under the canopy and create clear site lines. These trees create a comfortable microclimate and are chosen for their physical attributes, the colour of their flower reinforcing the colour of Kellyville station, as part of the public art *Light Line Social Square* strategy.

The secondary plaza spans the majority of the length of Precinct Street A. Expansive open space allows free flowing circulation. Trees are planted as groups and in ground to minimise obstacles in the space.



- ① Primary Plaza
- ② Secondary Plaza

Figure 3.17 KVE\_Landscape Plaza Plan. Source: HASSELL.

### Precinct Streets

Precinct streets have a formal boulevard character with street trees and generous footpaths. Future street junctions are safeguarded.

The streets have been designed as urban places with a high level of pedestrian amenity and inherent traffic calming measures. The design of the streets encourage lower traffic speeds through tree placement, activity around kerbside stopping, drop off locations and crossings. The streetscape design has been developed in response to future development sites and future road locations adjacent to the station.

Precinct Street A will become a through route and busy boulevard and will be heavily used in the future. Plenty of shade and amenity has been provided to encourage users to use the street frontages and congregate.

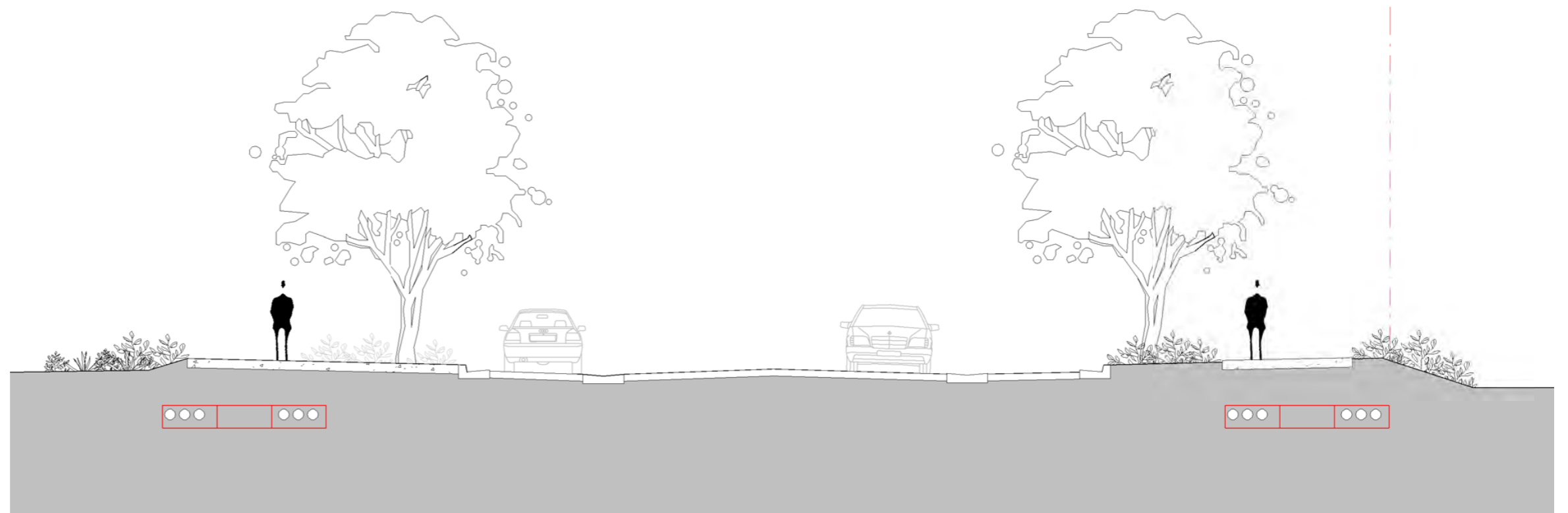


Figure 3.18 KVE\_Section of Precinct Street D. Source: HASSELL.

### 3.2.4 Sustainable Landscape Design

The key sustainable landscape design strategies used at Kellyville Station include the following:

- Generous planted areas are provided to maximise permeable surfaces
- Trees close to the station provide shade beyond built canopy. Light coloured surfaces are used where possible to reduce urban heat island effect
- Cumberland Plain woodland species used in vegetation buffers around the precinct provides connected planted corridors
- Incorporation of continuous massed planting beds along streetscapes where footpath and plaza requirements allow
- Bio swales within and around the main carpark at Kellyville. Footpaths are graded where possible to allow run off to be directed to planting beds
- Where the design allows, permeable tree grates will be provided at the base of trees.

Refer to Section 4.5 of this UDCLP for further detail on the project wide Sustainable Design and Maintenance initiatives.

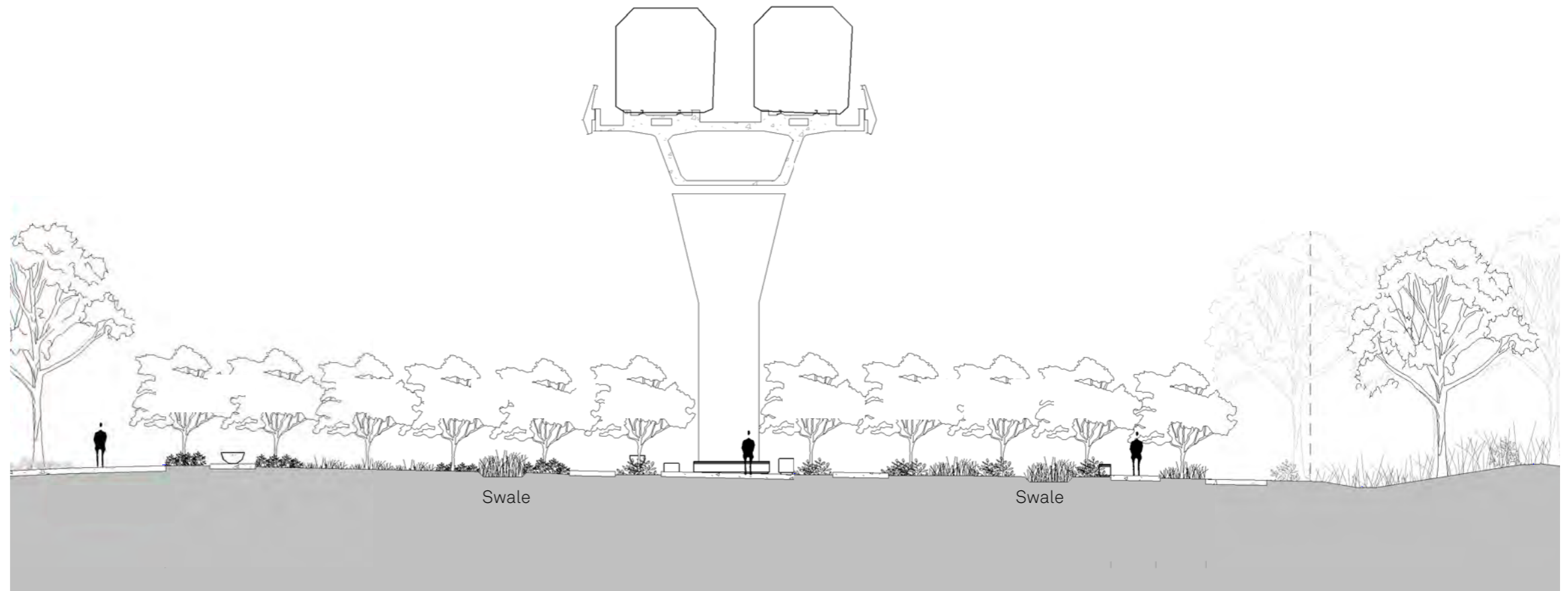


Figure 3.19 KVE\_Kellyville Station Section showing Swales in Secondary Plaza. Source: HASSELL.

**3.2.5 Heritage Interpretation, Interactions and Public Art**

**Heritage Interpretation**

Heritage interpretation will be in accordance with the guidelines in Section 4.10 of this UDCLP.

**Heritage Interactions**

The heritage trees on Old Windsor Road will be retained and protected. Refer Figure 3.21.

**Public Art**

Public art for Kellyville Station will be in accordance with the public art plan *Light Line Social Square*. Refer Sections 2.5.4 and 4.11 of this UDCLP. Elements are arranged and orchestrated to make a cohesive composition fusing together architecture, landscape, engineering, lighting, science and art.

Figure 3.20 shows the layout and organisation of *Light Line Social Square* elements at Kellyville Station. They comprise:

1. *Urban Grove*- Cultural Trees
2. *Social Spheres*- Sculptural + Play Elements
3. *Incidental Play*- Ground Play Elements
4. *Cloud Room*- Mist Installation
5. *Light Screens*- Vertical Station Facade Element
6. *Light Line*- Platform and Threshold

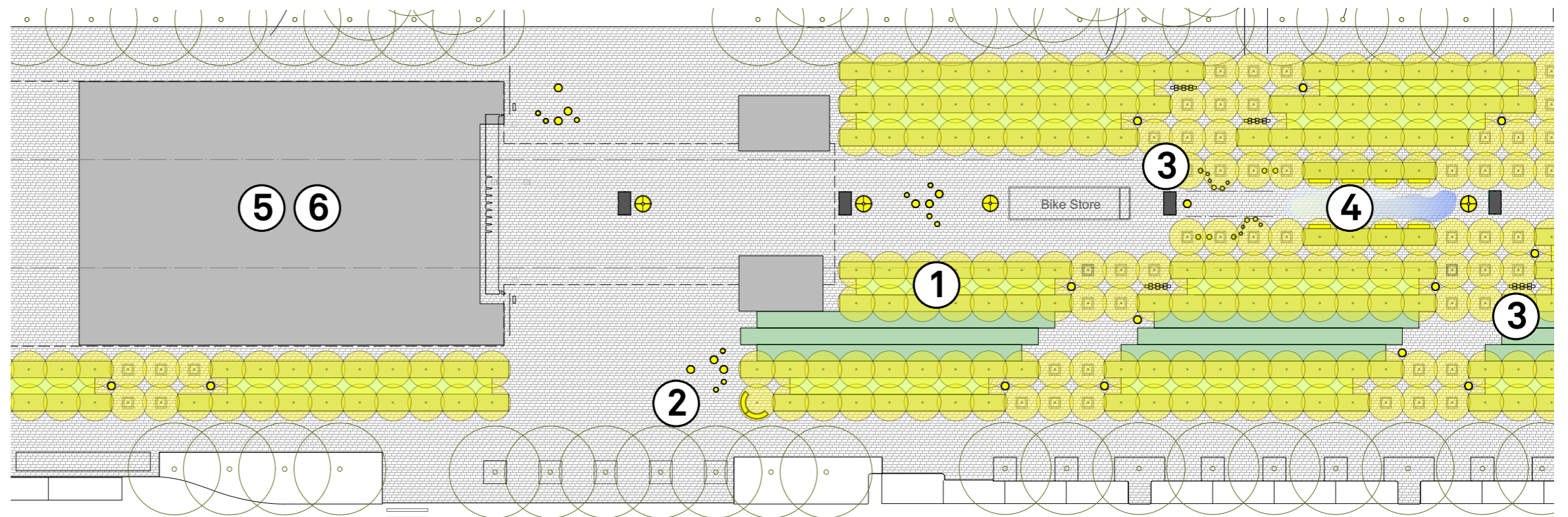
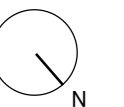


Figure 3.20 KVE\_Location of Public Art Elements at Kellyville Station. Source: MWA with T+C Studio.



### 3.2.6 Planting Design

Planting design for Kellyville Station is consistent with the project wide design themes discussed in Section 2.5 Urban and Landscape Design Values and Approach and Section 4.3 Landscaping Measures to Minimise, Mitigate and /or Offset the Impacts of the SSI, of this UDCLP. Different vegetation characters responding to functional requirements and situation, are proposed throughout the precinct. These are summarised below and illustrated on the adjacent figure.

#### Plaza Trees

Provide trees within the plaza areas which define the station entry areas and assist with orientation and location of the station access points from wider public domain.

#### Street Trees

Rows of street trees throughout the broader public domain beyond the station areas provide shade and character.

#### Bushland Trees

The bushland planting will emulate the Cumberland Plain Woodland community and tie into the language of the bushland corridor.

#### Heritage Trees

There are a number of existing heritage trees that are located adjacent to the T-way near the corner of Old Windsor Road and Samantha Riley Drive. The heritage trees on Old Windsor Road will be retained and protected. Refer Figure 3.21.

Refer to Section 4.3.7 Planting Design for further detail on the project wide tree and understorey planting strategies.

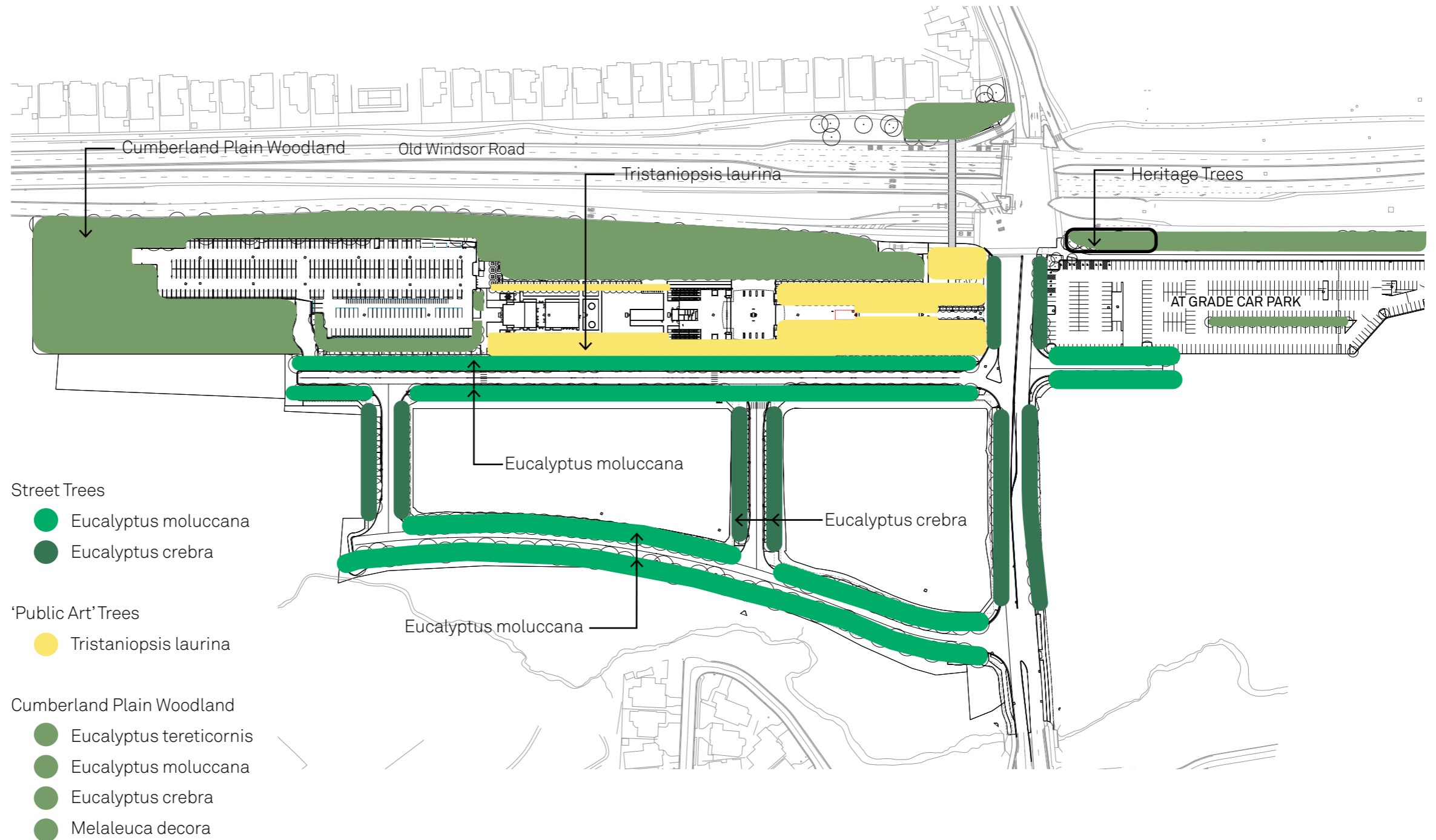


Figure 3.21 KVE\_Kellyville Station Precinct Planting Plan. Source: HASSELL.

### 3.3 Architectural Design

Kellyville Station design is based on a suite of components and systems that form part of the system wide approach to design, as well as site specific responses to the precinct. Refer to Section 4.12 for further detail on the design of these elements.

There are eight new stations along Sydney Metro Northwest with three station typologies. Kellyville is part of the elevated typology.

#### 3.3.1 Built Elements Typology - Elevated

Key aspects of the elevated typology at Kellyville Station include:

- The station is arranged over two primary levels: a ground level concourse, and the platform level set 13 m above ground
- The primary plaza is adjacent to Precinct Street A and aligned with Precinct Street D, and defines the entrance to the station via a curved entry canopy
- Station services buildings are grouped at the southern end of the station away from primary customer activity
- A glazed facade shelters the paid concourse and sheltered portion of the platform.

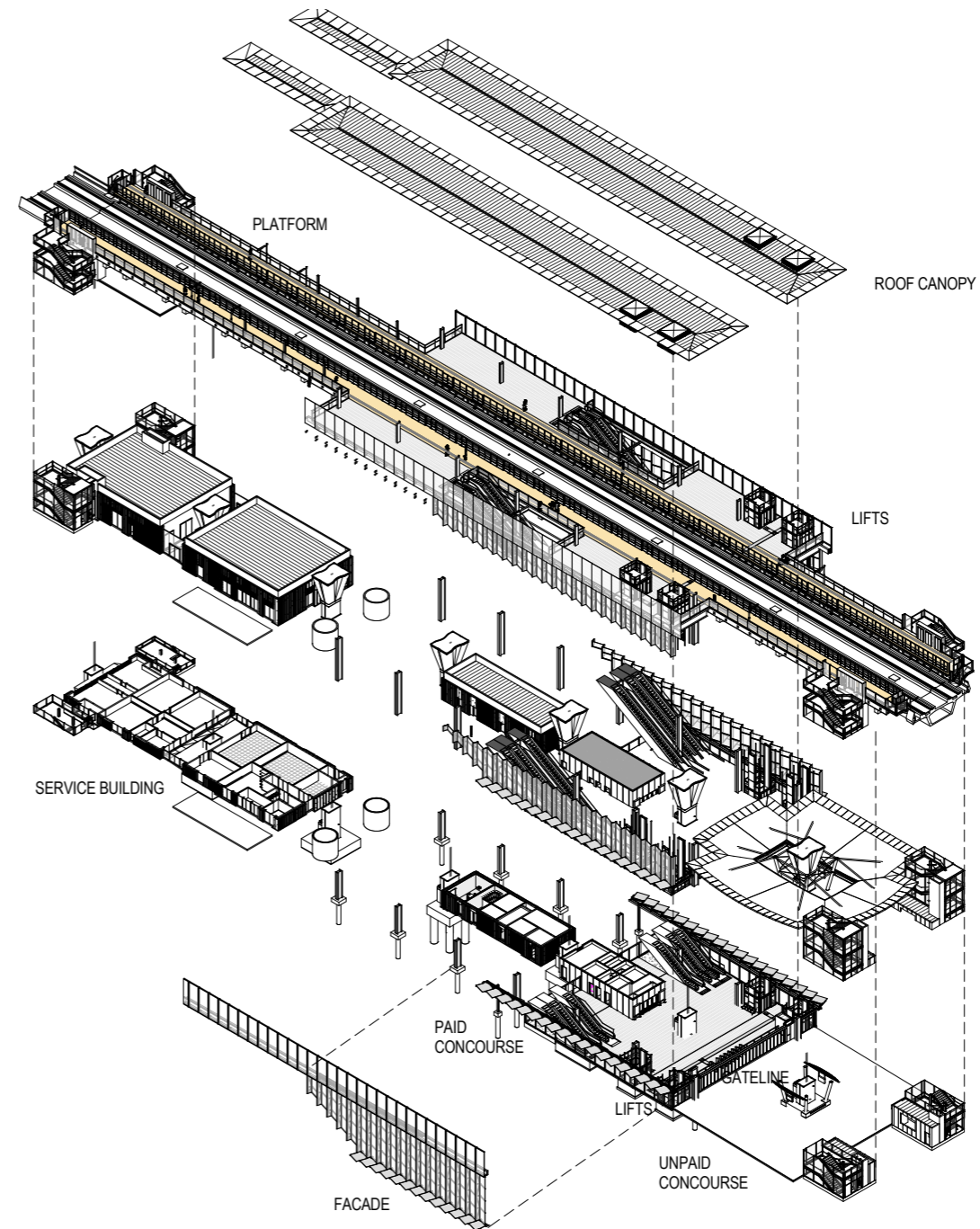


Figure 3.22 KVE\_Kellyville Station Exploded Diagram showing Station Configuration. Source: HASSELL.

### 3.3.2 Built Elements Design Opportunities

The arrangement of the built elements at Kellyville station delivers an integrated station and precinct that optimises transport interchange and community amenity and has the following features:

#### 1. Entry Canopy and Plaza

The primary plaza is aligned at the junction of Precinct Street A and Precinct Street D, and defines the entrance to the station via a curved entry canopy located beneath the viaduct.

#### 2. Station Gateline and Concourse

Station gateline and open concourse located within the station building at ground level, free from obstruction.

#### 3. Service Buildings

Collected at the southern end of the station away from customer movement. Grouped into two single level buildings located between the viaduct piers.

#### 4. Vertical Transport Arrangement

Westbound escalators from ground level concourse to platform, Eastbound escalators from ground level concourse to platform, with provision for additional escalators in the future. Lifts service each platform. The platform is approximately 9.5m wide at the head of the escalators and lifts to accommodate platform customer movement.

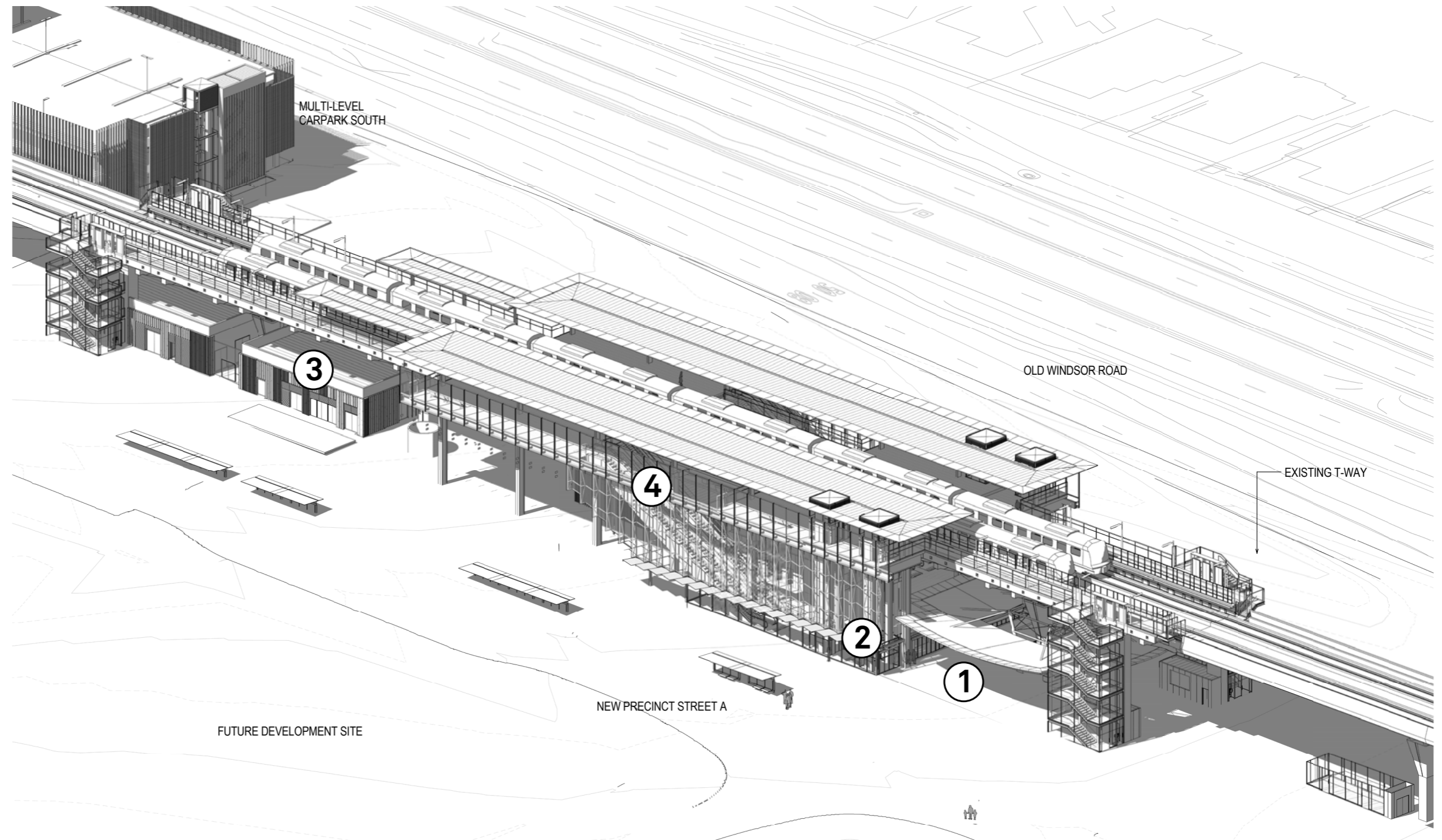


Figure 3.23 KVE\_Kellyville Station Diagram. Source: HASSELL.

**3.3.3 Built Elements Design**

Kellyville Station's key built elements are summarised below and illustrated on the adjacent figures.

**Station Configuration**

- The station footprint is constrained between the T-way and Precinct Street A.
- Secondary plazas continue to the north and south beyond the station and under the viaduct.
- The platform is located approximately 13m above ground.
- The façade to the station building is glazed providing cross-precinct views through the paid concourse and ensuring plenty of natural light.
- The entry canopy is adjacent to Precinct Street A and aligns with Precinct Street D to address primary pedestrian routes and provides a welcoming and identifying address to the station.
- Future development is foreseen along Precinct Street A in the form of retail and social activation

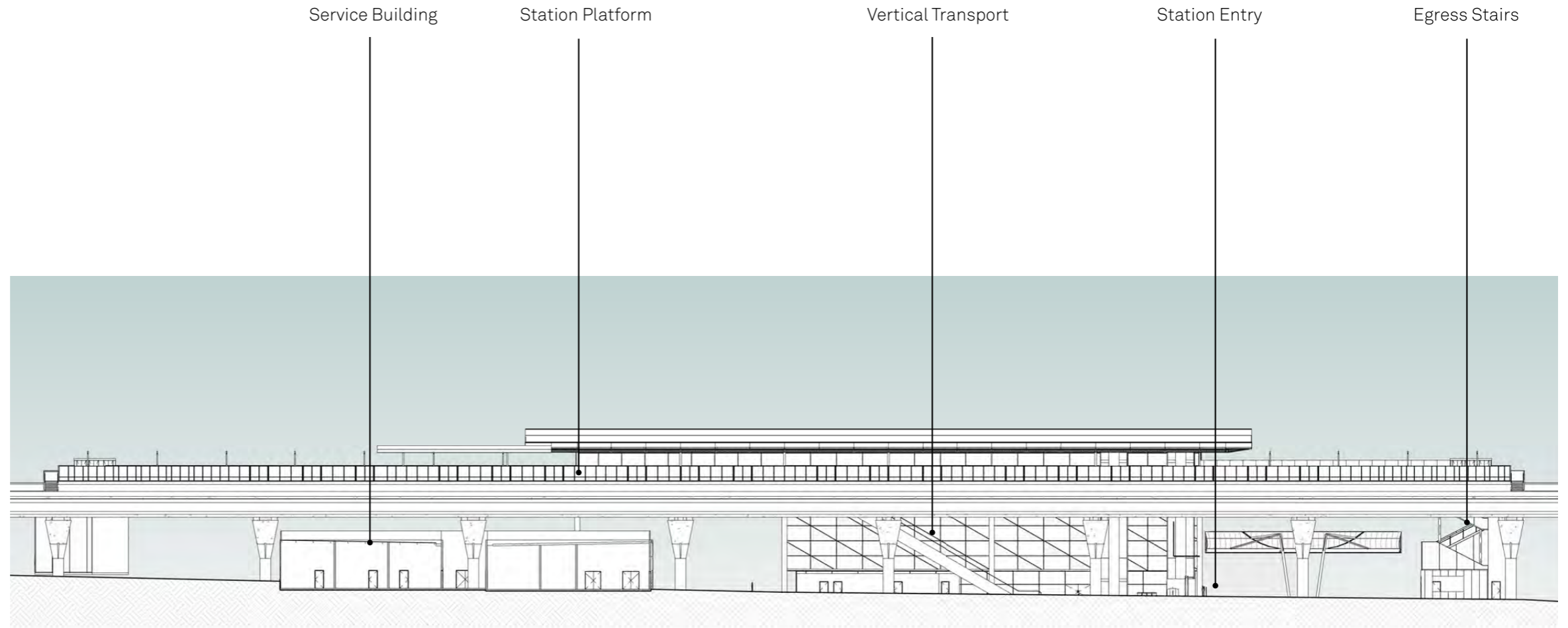


Figure 3.24 KVE\_Kellyville Station Diagram showing Station Configuration. Source: HASSELL.



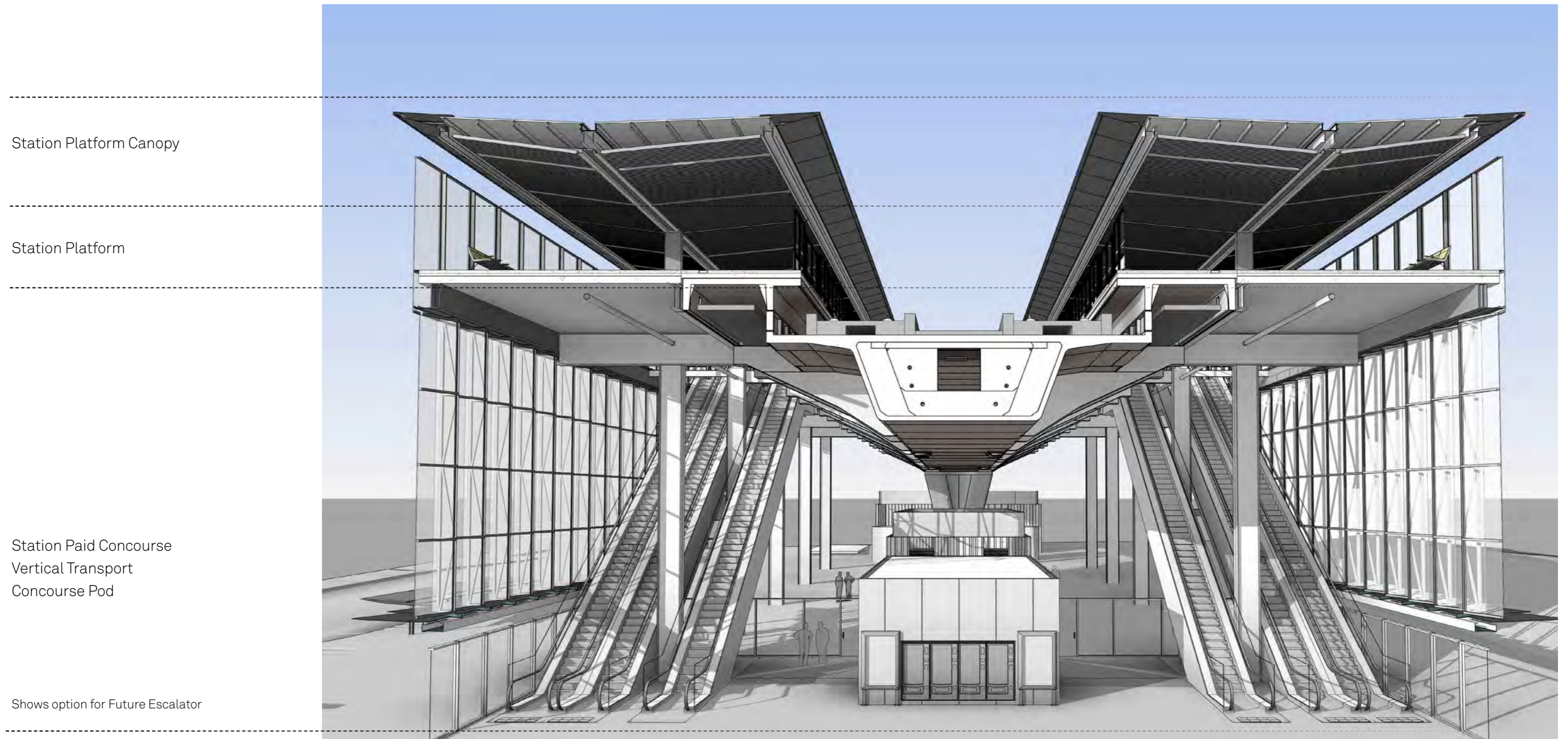


Figure 3.25 KVE\_Section of Kellyville Station highlighting ground level concourse, elevated platform and platform canopies . Source: HASSELL.

**Station Entry**

The station is accessed via the primary plaza, where the station entry canopy provides a welcoming, legible and identifiable feature. It orients the customer and provides direct and legible wayfinding to the station entry and ticket gate line, and also provides additional weather protection beyond the viaduct structure above.

Customer information, ticketing and associated facilities are provided on the front face of the egress stairs and future retail pods.

The paid concourse is fully weather protected by the viaduct and platforms over, as well as by the entry canopy. Lifts are located behind the gateline and station managers room along with customer toilets located neatly under the escalators.

The paid concourse area has been kept clear of fixed elements to maximise customer sightlines, and to ensure that the environment is as open and transparent as possible.

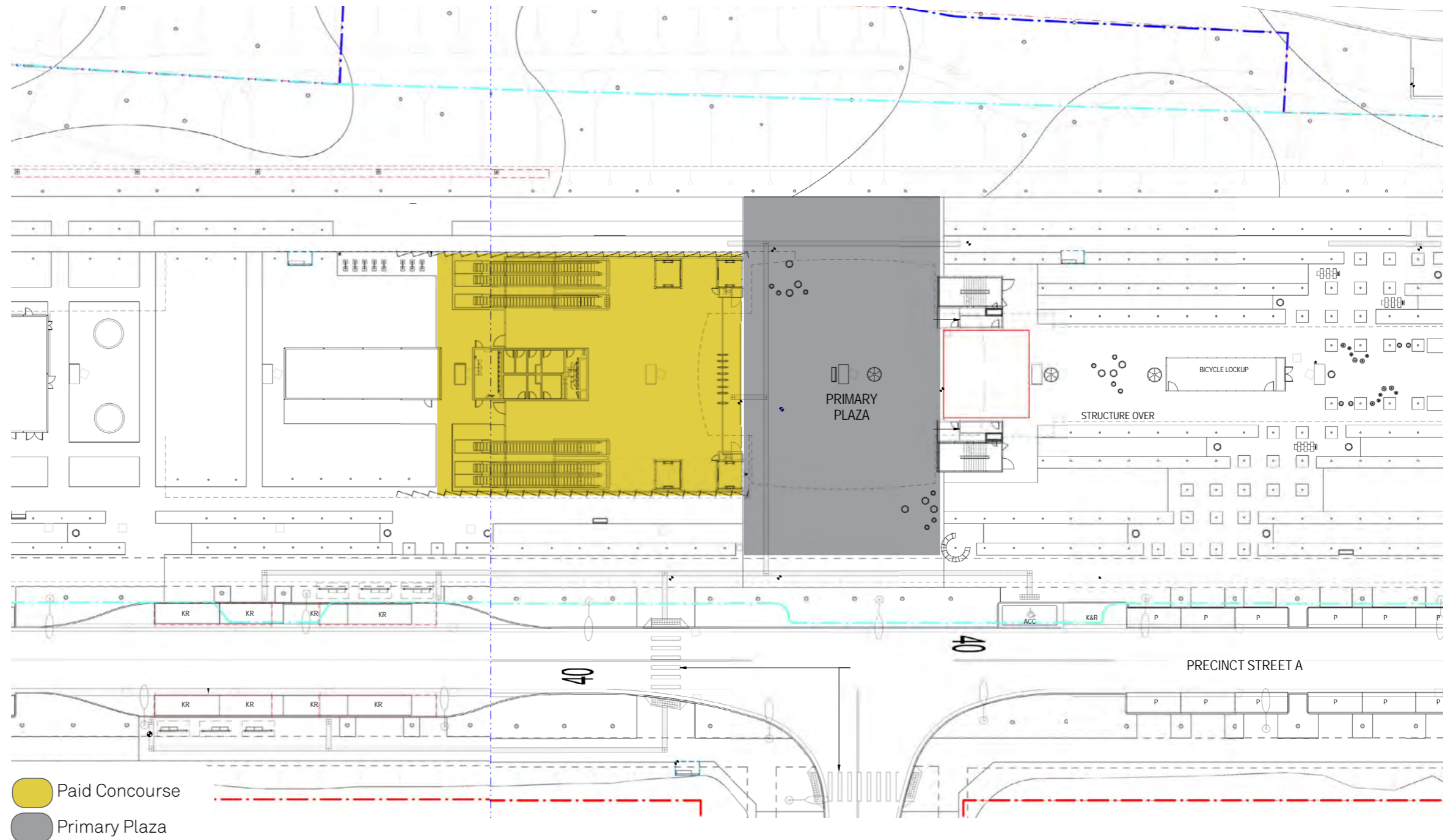


Figure 3.26 KVE\_Kellyville Station Paid and Concourse. Source: HASSELL.

### Car Parking

Combined multi level and at grade car parking with two entry and exit access points are located at each end of the station.

Parking types are gathered together for ease of use. Priority of car types nearest the station are accessible parking/ future electric recharge spaces/ compact cars and standard cars. Motorcycle parking is located in the car park for efficient circulation.

The distance from the car park to the gate line is under 400m. The car park entry plaza signifies the main entry to the car park and provides opportunities for seating.

Refer Section 4.12.15 Multi Level Car Park.

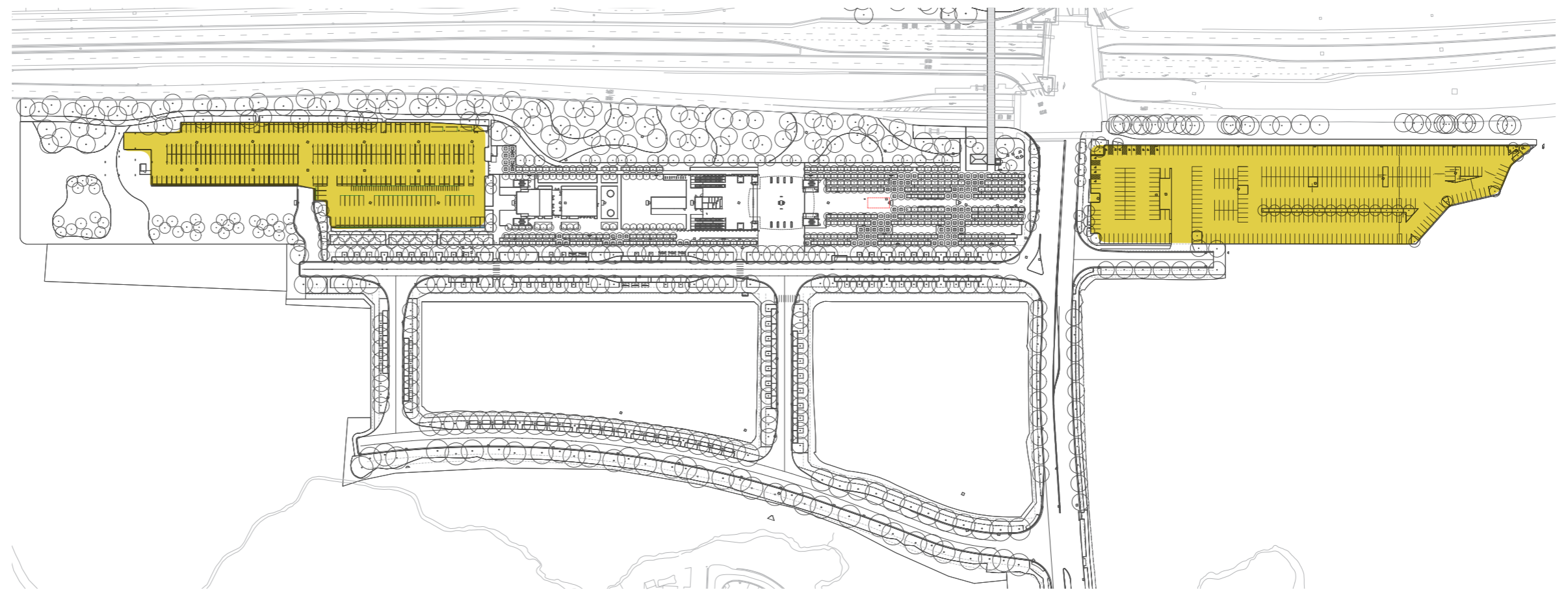


Figure 3.27 KVE\_Location of Multi Level and At Grade Car Park at Kellyville Station. Source: HASSELL.

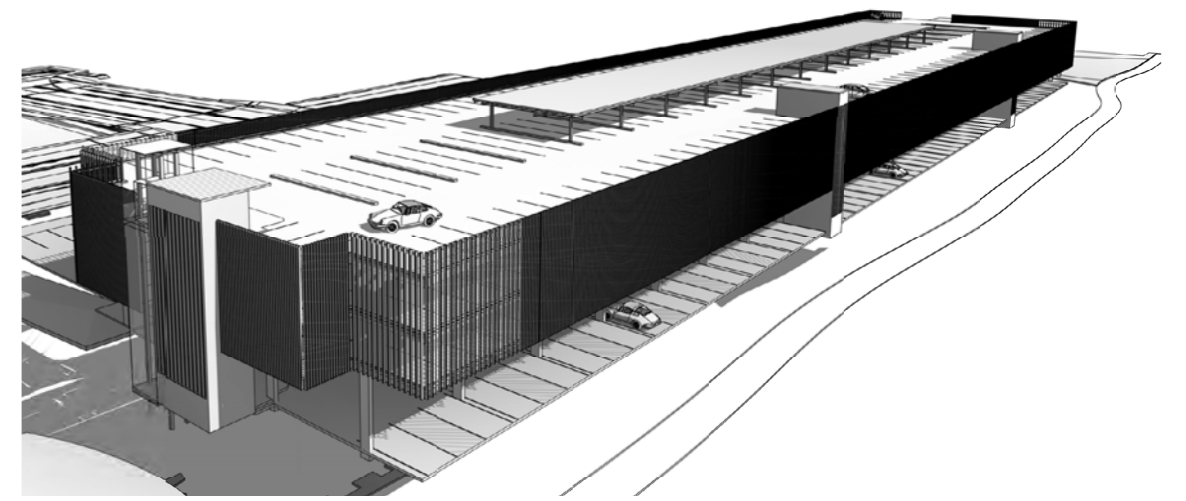


Figure 3.28 KVE\_Southern Car Park at Kellyville Station. Source: HASSELL.

### Pedestrian Bridge

A new bridge provides a safe connection over Old Windsor Road to the station precinct. The alignment of the future road and shared use path provide strong sightlines and way finding into the primary plaza. A lift and stairs provide access to the pedestrian bridge. There are no other stairs required within the wider precinct at Kellyville Station.

Refer to Section 4.12.16 Pedestrian Bridge.

### Service Buildings

The service buildings have been grouped at the city end of the station to enable a more efficient services strategy.

The service buildings are connected by perforated metal screens. Buildings have been set back from viaduct piers allowing for a maintenance access way for visual inspection of the piers. Less frequent maintenance to the piers will be via an articulated boom, parked adjacent the service buildings. Service buildings have been planned to minimise the extents of external doors, to maximise future flexibility and enable the possibility for retail tenancies to be built alongside the service buildings in the future.

Staff services are located centrally to enable staff direct access to the paid concourse.

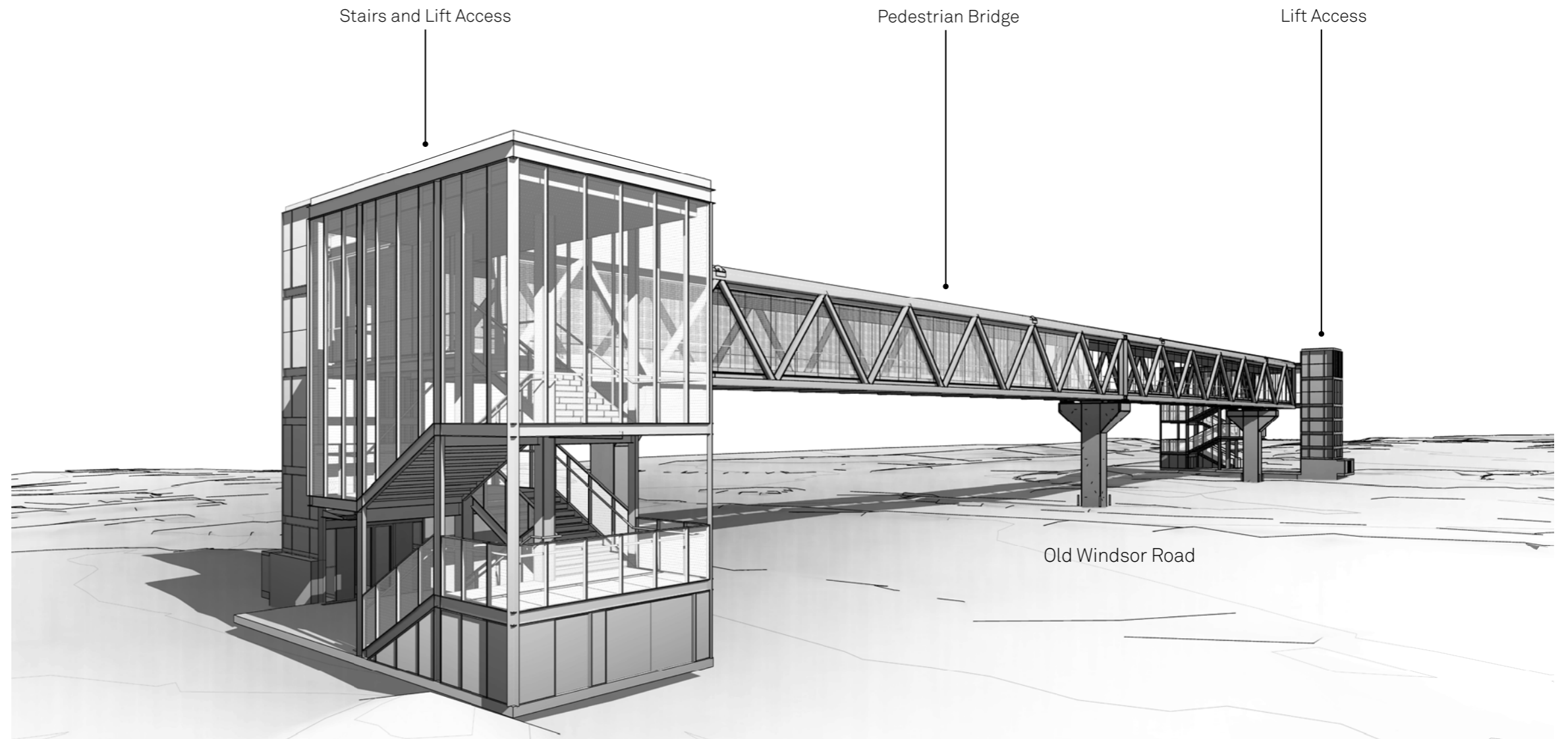


Figure 3.29 KVE\_Kellyville Station Pedestrian Bridge crossing Old Windsor Road. Source: HASSELL.

### Station Accommodation Strategy

The station accommodation has been categorised into four types and has been integrated with the precinct and station design to enhance the customer experience both physically and visually.

#### 1. Customer Facilities

Located at the base of the northern egress stairs flanking the northern side of the Primary Plaza so that queuing for the customer facilities is well clear of gateline movement. Customer toilets are located below the escalator and stair within the paid concourse.

#### 2. Staff Facilities

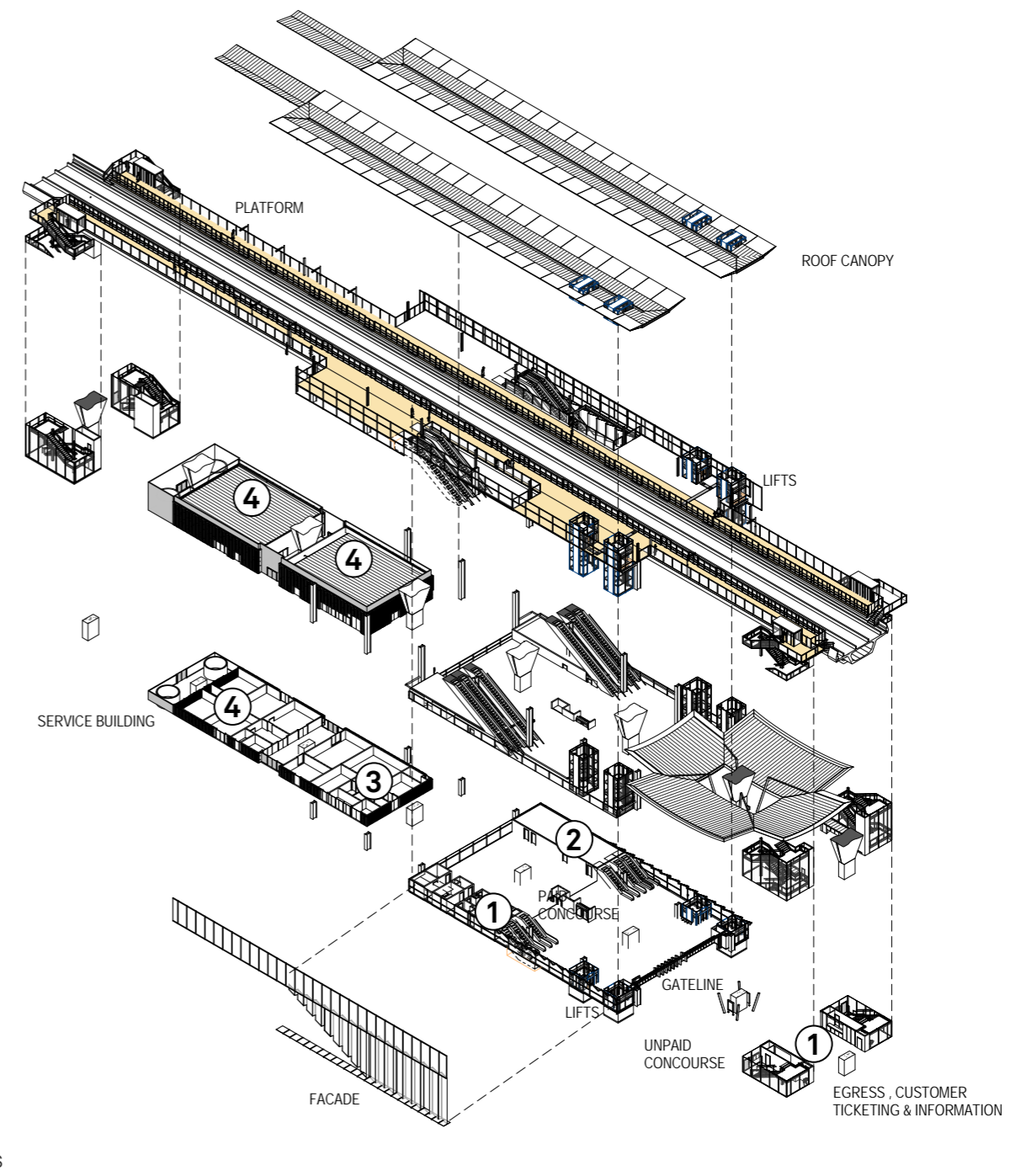
The Station Manager's room has been placed on the western edge of the paid concourse to maximise accessibility without impairing customer flow through the station.

#### 3. Back Of House

Staff back of house and maintenance rooms have been placed at street level, south of the paid concourse. They are located in the northern most pod in a collection of pods that are visually and physically disconnected from the station building. This maximises openness and visual legibility within the paid concourse.

#### 4. Station Service Buildings

Station services accommodation are located in the southern most pod of the back of house buildings situated at the southern end of the building. The pods are connected by recessive perforated metal screens which provide secure access to the buildings and also allow the overall form to be articulated as two smaller elements rather than one large building.



- ① Customer Facilities
- ② Staff Facilities
- ③ Back of House
- ④ Station Service Buildings

Figure 3.30 KVE\_Kellyville Station Accommodation Strategy. Source: HASSELL.

**3.3.4 Future Opportunities**

Potential opportunities for the location of commercial facilities and the expansion of station facilities (where appropriate) have been identified in the design refinement of the stations. In the future, retail opportunities will be developed at this station and potential locations where such uses are likely to be suitable are identified on Figure 3.31. The final locations will be subject to further design and viability assessment.

An objective for commercial activity is that it should contribute positively to customer journeys. The following commercial opportunities have been identified.

**Retail**

Provision has been made to safeguard future retail tenancies at the base of each fire egress stair at the northern side of the primary plaza. Capped services will be provided to this area.

**Vending Machines**

Provision has been made for vending machines within the paid concourse. This space has been integrated with concourse pods to provide seamless integration with the architecture and not adversely impact customer circulation or wayfinding.

**Advertising**

Advertising will be integrated to the station architectural design and finishes in the future.

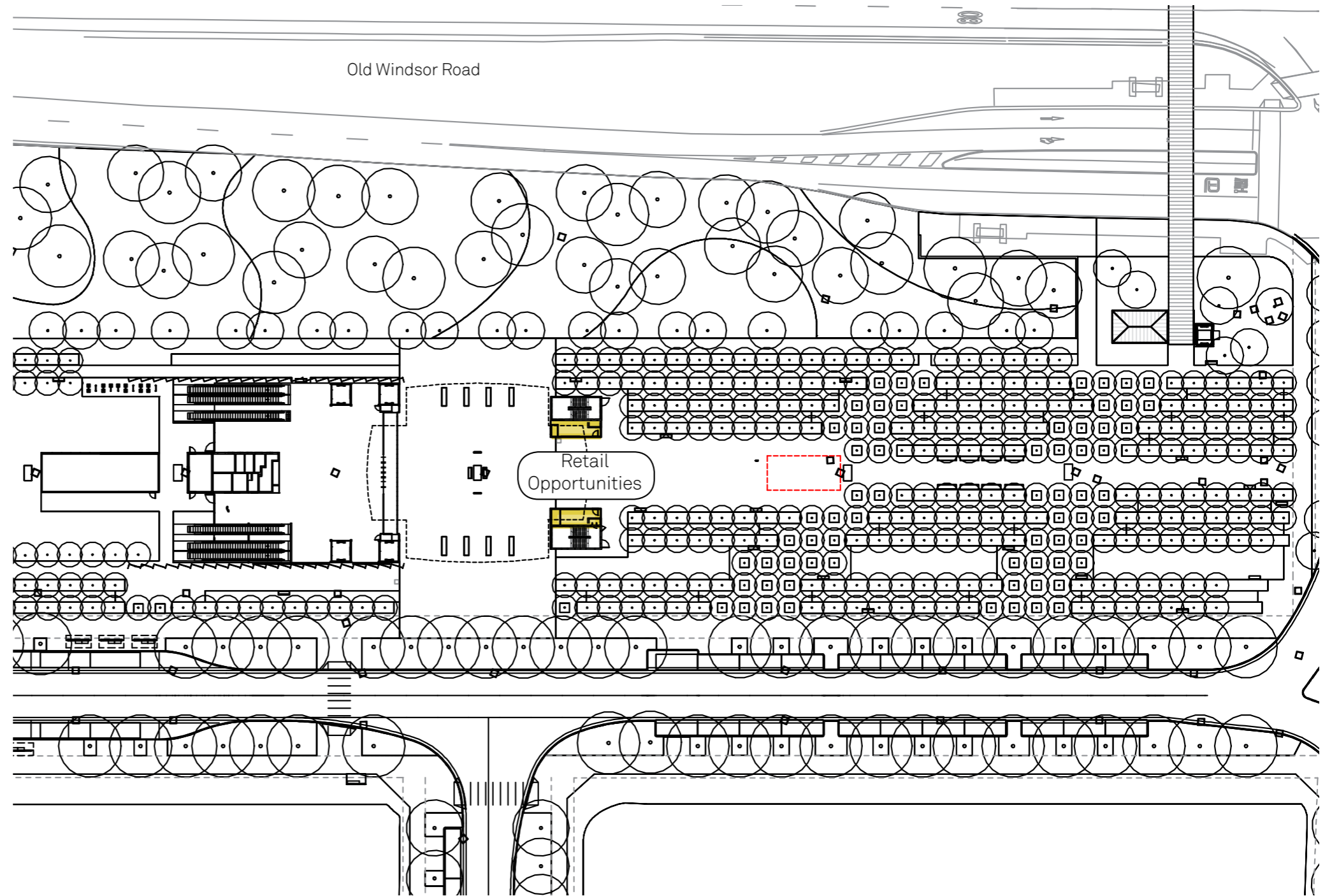
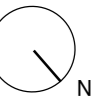


Figure 3.31 KVE\_Commercial Opportunities Plan. Source HASSELL.



### 3.3.5 Signage and Wayfinding

The Sydney Metro Northwest stations are designed to be open, accessible and intuitively navigable with a wayfinding and signage strategy that will enhance these features. The wayfinding strategy provides directional and location information through simple clear signage, messages and use of pictograms, and aims to provide the right information at the right location so customers can navigate their way around the stations safely and efficiently.

The wayfinding strategy and associated signage is designed to optimise the navigation experience inside and around the stations. This will be integrated with the station architecture, consistent with the principles currently being tested and established by TfNSW and tailored for a modern single line, rapid transit system. The current signing products as established by TfNSW will be specified for use in the stations.

The right location for signing is usually at a decision point and at the Station, there are primarily two key decision points. One of these is on entering the concourse where the correct platform has to be selected for travelling in the right direction and the other is when exiting the concourse to access the local area and other transport interchange modes. The other main choice customers face is the mode of vertical transportation and whether to use the lifts or escalators to travel between the station concourse and platform below.

The signage and customer information details shown are indicative only and will be developed in greater detail during subsequent design stages.

### Precinct Signing

The area immediately outside of the station buildings will vary in scale and function depending on location. At Kellyville Station precinct, the initial provision is for a 'park and ride' function plus small-scale transport interchange.

Multi level car parking is provided plus secure bike storage, bus stands and a taxi rank. Precinct signing will identify and direct passengers to these facilities, primarily through the use of a standard TfNSW finger-post sign and to a lesser extent the use of a TfNSW blade sign.

The blade sign has the added advantage of incorporating poster information which can take the form of local area maps.

Precinct signs will include station totem signs, using the two (different sized) standard post mounted products developed by TfNSW.

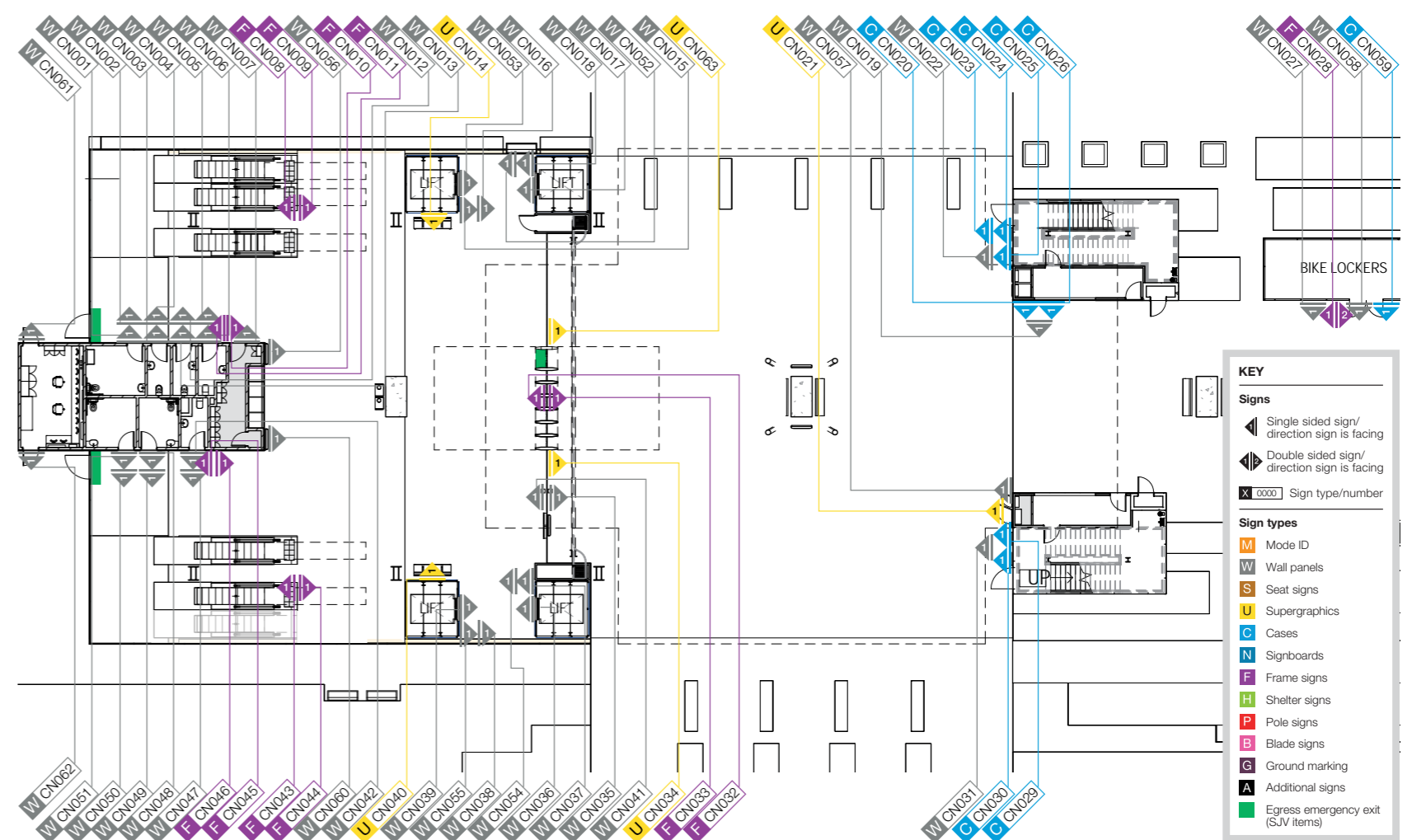


Figure 3.32 KVE\_Extract of Precinct Signing in Primary Plaza. Source: Blue Sky.

